

SOUTH KOLB ROAD SPECIFIC PLAN AMENDMENT

Co23-08-01

South Kolb Road Specific Plan Amendment

East of Kolb Road, North and South of Valencia Road
Pima County, Arizona

Submitted to:

**Pima County
Development Services Department**
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Board of Supervisors Approved Submittal
November 2008

F. ANN RODRIGUEZ, RECORDER
RECORDED BY: LAM
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ORDINANCE 2009- 1

AN ORDINANCE OF THE BOARD OF SUPERVISORS OF PIMA COUNTY, ARIZONA; RELATING TO ZONING; REZONING PROPERTY (PARCEL CODES 141-06-005B, 141-06-012B, 141-06-012C, 141-06-013B, 141-06-013C, 141-07-0090, 141-10-0020) FROM RH (AE) (RURAL HOMESTEAD) (AIRPORT ENVIRONS AND FACILITIES), SR (AE) (SUBURBAN RANCH) (AIRPORT ENVIRONS AND FACILITIES), AND CI-1 (LIGHT INDUSTRIAL/WAREHOUSING ZONE) TO SP (AE) (SPECIFIC PLANS, SOUTH KOLB ROAD SPECIFIC PLAN (FIRST AMENDMENT)) (CO23-08-01) (AIRPORT ENVIRONS AND FACILITIES) LOCATED EAST OF SOUTH KOLB ROAD AND ON THE NORTH AND SOUTH SIDES OF WEST VALENCIA ROAD; AMENDING PIMA COUNTY ZONING MAP NOS. 31 AND 58.

BE IT ORDAINED BY THE BOARD OF SUPERVISORS OF PIMA COUNTY, ARIZONA:

Section 1. The 83.4 acres, located east of South Kolb Road and on the north and south sides of West Valencia Road, illustrated by the shaded area on the attached rezoning ordinance map (Exhibit "A"), which amends Pima County Zoning Map Nos. 31 and 58 is hereby rezoned from RH (AE) (RURAL HOMESTEAD) (AIRPORT ENVIRONS AND FACILITIES), SR (AE) (SUBURBAN RANCH) (AIRPORT ENVIRONS AND FACILITIES), AND CI-1 (LIGHT INDUSTRIAL/WAREHOUSING ZONE) TO SP (AE) (SPECIFIC PLANS) (AIRPORT ENVIRONS AND FACILITIES).

Section 2. Rezoning Conditions.

1. Not more than 60 days after the Board of Supervisors approves the Specific Plan, the owner(s) / developer(s) shall submit to the Planning Director the specific plan document, including any necessary revisions of the specific plan document reflecting the final

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actions of the Board of Supervisors, and the specific plan text and exhibits in an electronic format acceptable to the Planning Division.

2. Submittal of a development plan, or acceptable site development plan, if determined necessary by the appropriate County agencies.
3. Recording of a covenant holding Pima County harmless in the event of flooding.
4. Recording of the necessary development related covenants as determined appropriate by the various County agencies.
5. Provision of development related assurances as required by the appropriate agencies.
6. Prior to the preparation of the development related covenants and any required dedication, a title report (current to within 60 days) evidencing ownership of the property shall be submitted to the Development Services Department, Document Services.
7. There shall be no subdividing or lot splitting without the written approval of the Board of Supervisors.
8. In the event of a conflict between two or more requirements in this specific plan, or conflicts between the requirements of this specific plan and another Pima County regulation not listed in Section 18.90.050B3, the more restrictive requirement shall apply.
9. This specific plan shall adhere to all applicable Pima County regulations that are not explicitly addressed within this specific plan. The specific plan's design guidelines shall be interpreted to implement the specific plan or relevant Pima County regulations.
10. Prior to the issuance of any permits, this specific plan amendment site is subject to the approval of the Subdivision Review Committee.

A. Subdivision Block Plat: The subdivision block plats shall reference all dedications (including roads, sewer, drainage, trails and open space), unless otherwise specified in a development agreement, and the plats shall identify all necessary improvements. Upon submittal of the block plats, the studies, reports, information required by these specific plan conditions and the specific plan document itself, shall be provided for review and approval of the applicable Pima County department or departments. Subsequent site development requires submittal of development plans/site development plans prepared in accordance with the subdivision block plats.

B. Development Plans/Site Development Plans: The plans shall identify all necessary improvements. Upon submittal of the development plans, the studies, reports, information required by these specific plan conditions and the specific plan document

itself, shall be provided for review and approval of the applicable Pima County department or departments. Subsequent site development shall require amendment of the applicable development plan/site development plan.

11. No building permits shall be issued until all applicable specific plan requirements for or affecting the site are satisfied and the Planning Director issues a Certificate of Rezoning Compliance.

12. Regional Flood Control District requirements:

A. Drainage shall not be altered, disturbed or obstructed without the written approval of the Flood Control District.

B. The property owner(s) shall obtain a Floodplain Use Permit for any development on the subject property.

C. A drainage study shall be submitted for review and approval, which addresses the impacts of development to the federally mapped floodplain and local area drainage.

D. The property owner(s) shall provide necessary on-site and off-site drainage improvements as required by the Flood Control District.

E. All-weather access shall be provided to all lots to meet concurrency requirements.

F. A riparian mitigation plan shall be required for development in designated riparian areas.

G. The annual improvements implementation and maintenance report, is to be submitted to PCRFCDD. This report shall include information pertaining to the status of floodplain changes and improvements, riparian mitigation and maintenance.

13. Wastewater Reclamation Department requirements:

A. The owner / developer shall construe no action by Pima County as a commitment to provide sewer service to any new development within the rezoning area until Pima County executes an agreement with the owner / developer to that effect.

B. The owner / developer shall obtain written documentation from the PCRWRD that treatment and conveyance capacity is available for any new development within the rezoning area, no more than 90 days before submitting any tentative plat, development plan, sewer improvement plan or request for building permit for review. Should treatment and / or conveyance capacity not be available at that time, the owner / developer shall have the option of funding, designing and constructing the necessary

improvements to Pima County's public sewerage system at his or her sole expense or cooperatively with other affected parties. All such improvements shall be designed and constructed as directed by the PCRWRD.

C. The owner / developer must secure approval from the Pima County Department of Environmental Quality to use on-site sewage disposal systems within the rezoning area allowing on-site sewage disposal systems at the time a tentative plat, development plan or request for building permit is submitted for review.

D. The owner / developer shall time all new development within the rezoning area to coincide with the availability of treatment and conveyance capacity in the downstream public sewerage system.

E. The owner / developer shall fund, design and construct all off-site and on-site sewers necessary to serve the rezoning area, in the manner specified at the time of review of the tentative plat, development plan, sewer construction plan or request for building permit.

F. The owner / developer shall design and construct the off-site and on-site sewers to accommodate flow-through from any properties adjacent and up-gradient to the rezoning area that do not have adequate access to Pima County's public sewer system, in the manner specified at the time of review of the tentative plat, development plan, sewer construction plan or request for building permit.

G. The owner / developer shall also design and construct any necessary off-site sewers to accommodate the anticipated wastewater flow from any properties down-gradient from the rezoning area that can reasonably be served by those sewers, in the manner specified at the time of review of the tentative plat, development plan, sewer construction plan or request for building permit.

H. The owner / developer shall complete the construction of all necessary public and/or private sewerage facilities as required by all applicable agreements with Pima County, and all applicable regulations, including the Clean Water Act and those promulgated by ADEQ, before treatment and conveyance capacity in the downstream public sewerage system will be permanently committed for any new development within the rezoning area.

I. The owner / developer shall connect all development within the rezoning area to Pima County's public sewer system at the location and in the manner specified by the PCRWRD in its capacity response letter and as specified by the Development Services Department at the time of review of the tentative plat, development plan, sewer construction plan, or request for building permit.

14. Department of Environmental Quality requirement: If on-site wastewater disposal is

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proposed, and public sewer is within 200 feet of the property line, a request for waiver of connection to public sewer requirements shall be made and must be approved prior to the issuance of the Construction Authorization Certificate.

15. Cultural Resources requirements:

A. As documented in the cultural resources survey reports submitted by Westland Resources, Inc., proposed development in the Amendment area of 83.4 acres will have no impact on any known cultural resources; therefore, no mitigative actions are necessary. There are known cultural resources, however, located within the area defined in the existing South Kolb Road Specific Plan. Therefore, a cultural resources mitigation plan for any identified archaeological and historic sites on the subject property shall be submitted to Pima County at the time of, or prior to, the submittal of any tentative plan or development plan. Following rezoning approval, any subsequent development requiring a Type II grading permit will be reviewed for compliance with Pima County's cultural resources requirements under Chapter 18.81 of the Pima County Zoning Code.

B. In the event that cultural resources are revealed during ground-disturbing activities, all construction shall cease, and consultation shall be initiated with ASM to assess the potential significance of any unearthed materials (ARS §41-841). If human skeletal remains or funerary objects are discovered, ASM will be contacted immediately (ARS §41-865 & §41-844).

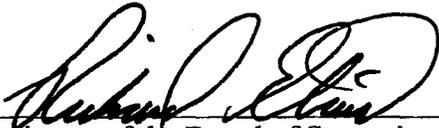
16. In the event the subject property is annexed, the owner(s) / developer(s) shall adhere to all applicable rezoning conditions, including, but not limited to, development conditions which require financial contributions to, or construction of infrastructure, including without limitation, transportation, flood control, or sewer facilities.

17. The property owner shall execute and record the following disclaimer regarding Prop 207 rights. "Property Owner acknowledges that neither the rezoning of the Property nor the conditions of rezoning give Property Owner any rights, claims or causes of action under the Private Property Rights Protection Act (Arizona Revised Statutes Title 12, chapter 8, article 2.1). To the extent that the rezoning or conditions of rezoning may be construed to give Property Owner any rights or claims under the Private Property Rights Protection Act, Property Owner hereby waives any and all such rights and/or claims pursuant to A.R.S. § 12-1134(I)."

18. Adherence to the specific plan document as approved at the Board of Supervisor's public hearing.

Section 8. The effective date of this Ordinance shall be 30 days after adoption of this Ordinance.

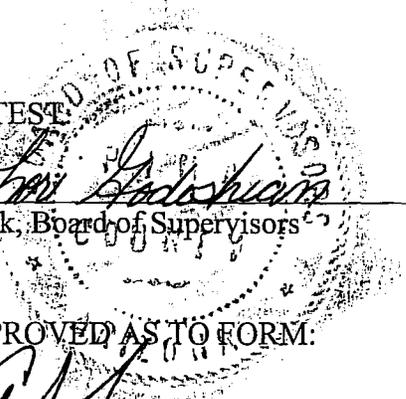
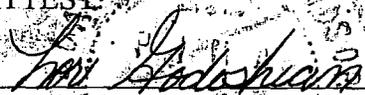
Passed and adopted by the Board of Supervisors of Pima County, Arizona, this 6th day of January _____, 2009.



Chairman of the Board of Supervisors

JAN 06 2009

ATTEST

Clerk, Board of Supervisors

APPROVED AS TO FORM:



Deputy County Attorney

ANDREW FLAGG

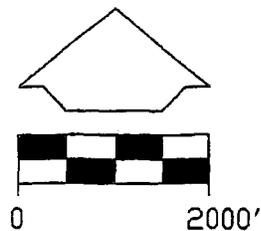


Executive Secretary,
Planning and Zoning Commission

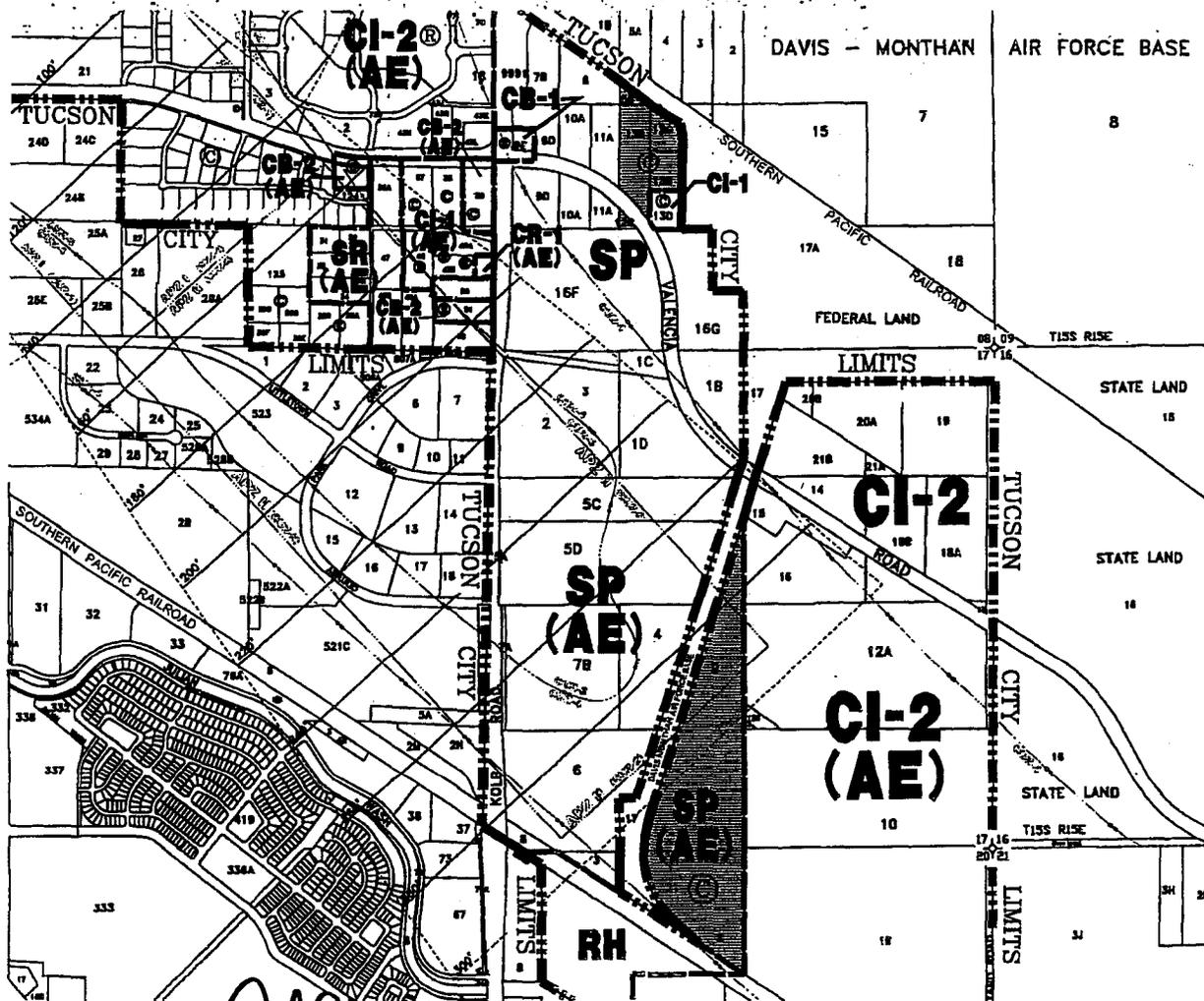
ANDREW FLAGG

EXHIBIT A

AMENDMENT NO's. 21 & 23 BY ORDINANCE NO. 2009-1
 TO PIMA COUNTY ZONING MAP NO's. 31,58 TUCSON, ARIZONA.
 PARCEL 5B BEING A PART OF THE SE 1/4 NW 1/4 OF SEC 08, T15S, R15E; PARCELS 12B, 12C, 13C & 13B BEING A PART OF THE NE 1/4 SW 1/4 OF SEC 08, T15S, R15E; PARCEL 9 BEING A PART OF THE SE 1/4 NW 1/4 AND E 1/2 SW 1/4 OF SEC.17, T15S, R15E; AND PARCEL 2 BEING A PART OF THE NE 1/4 NW 1/4 OF SEC 20, T15S, R15E.



ADOPTED JANUARY 6, 2009 EFFECTIVE FEBRUARY 6, 2009



Carl Shallick

EXECUTIVE SECRETARY PIMA COUNTY PLANNING AND ZONING COMMISSION

© NO BUILDING PERMITS WITHOUT CERTIFICATE OF COMPLIANCE
 FROM SR 55.1 ac±, CI-1 6.2 ac±, RH 22.1 AC±
 PJ-DECEMBER 02, 2008

Co23-08-1
 Co7-00-20
 Co23-88-1
 MULTIPLE TAX
 PARCEL CODES

LAWSON ENGINEERING

Exhibit B is the South Kolb Road Specific Plan Amendment document approved by the Pima County Board of Supervisors on November 18, 2008.

The document is not recorded but may be viewed at the office of the Pima County Development Services Department, Planning Division, at the following address:

County-City Public Works Building
201 N. Stone Avenue, 2nd Floor
Tucson, Arizona

020012 07431

Exhibit C – Legal Description

SOUTH KOLB ROAD SPECIFIC PLAN AMENDMENT LEGAL DESCRIPTION

NORTH BLOCK

A portion of the Southwest Quarter of Section 8, Township 15 South, Range 15 East of the Gila and Salt River Meridian, Pima County, Arizona.

The above described parcel of land contains 15.8 acres, more or less.

SOUTH BLOCK

A portion of the West Half of Section 17, Township 15 South, Range 15 East of the Gila and Salt River Meridian, Pima County, Arizona.

The above described parcel of land contains 45.5 acres, more or less.

A portion of the Northwest Quarter of Section 20, Township 15 South, Range 15 East of the Gila and Salt River Meridian, Pima County, Arizona.

The above described parcel of land contains 22.1 acres, more or less.

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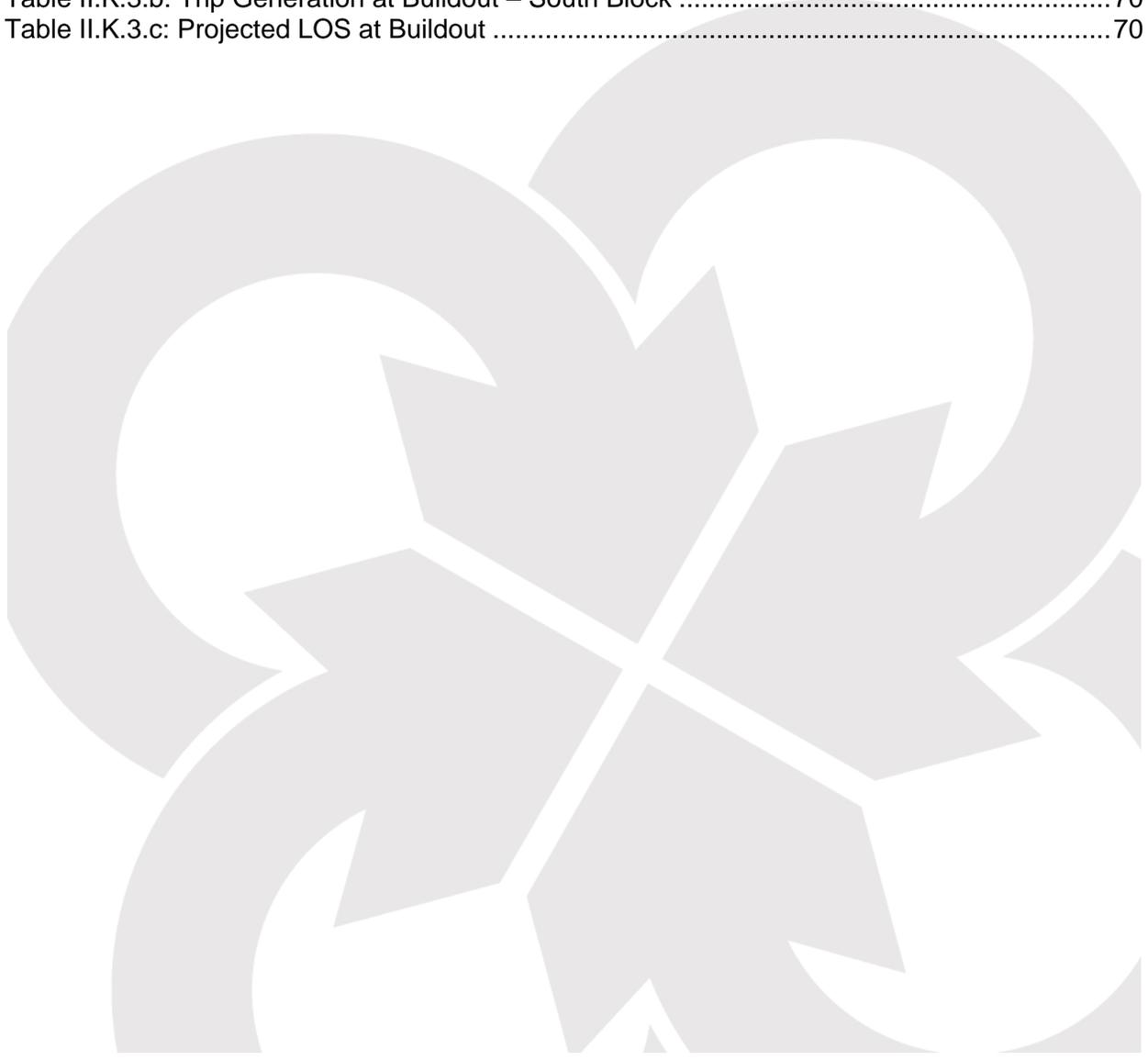
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Introduction

The South Kolb Road Specific Plan was originally approved by Pima County in 1988 as CO23-88-01. The plan included approximately 348 acres of land located east of Kolb Road, south of Davis-Monthan Air Force Base and north of the Union Pacific Railroad. The Specific Plan proposes industrial use for the site, with some limited areas for commercial uses. The Specific Plan was amended once in 2006, with the Board of Supervisors approving a non-substantial change of rezoning condition that eliminated the distinction between “Primary Uses and “Secondary Uses.”

The current owners of the Specific Plan have purchased additional parcels of land adjacent to the Specific Plan boundary. The purpose of this amendment is to incorporate those parcels into the Specific Plan, adopting the same land uses and development regulations to guide their development in a cohesive manner.

This document was created to act as an addendum to the original South Kolb Road Specific Plan. A supplementary Site Inventory is included to address the existing conditions of the parcels to be added to the Specific Plan. This is followed by a revised Land Use Proposal section that designates land uses for these parcels and discusses the planning considerations for those proposed uses. The Development Regulations, Design Guidelines, Implementation and Administration sections have been duplicated to create a “stand-alone” document, but no changes are proposed to these sections from the approved South Kolb Road Specific Plan.

In addition to the rezoning process, the entire property (existing Specific Plan area plus the amendment parcels) is a pilot “Shovel Ready” project. The Shovel Ready Program is being introduced by Tucson Regional Economic Opportunities Inc. (TREO) to address the needs of targeted industries for certified, high quality information about sites and for assured quick turnaround on facility permitting. The program is intended to increase the region's ability to compete for high-skill/high-wage companies and is intended to be a strong selling point for companies to relocate to Tucson. Under the Shovel Ready Program for this project, a tentative plat/development plan was submitted to Pima County Development Services Department. Once a tentative plat is approved, a grading plan will be submitted for approval to allow for land clearing and plant relocation in preparation for channel and detention basin construction. A paving plan for the private roads will also be submitted with the grading plan. Currently, sewer and water plans are in review, and Southwest Gas, Tucson Electric Power and Qwest are preparing utility plans. Once the final plat, grading plan, paving plan and utility plans are approved, an application will be made to TREO to certify "shovel ready" status. Additional information on the Shovel Ready Program and how it relates to this project has been included in Part IV of this document.



Site Inventory Divider

Part I Site Inventory



A. Existing Land Uses

1. Location and Regional Context

The proposed Amendment is divided into two separate blocks north and south of Valencia Road. The north block, which is approximately 15.8 acres, is located northeast of the existing South Kolb Road Specific Plan and is adjacent to and south of Davis-Monthan Air Force Base (DMAFB). The block consists of five separate Assessor’s parcels (141-06-005B, 141-06-012B, 141-06-012C, 141-06-013B and 141-06-013C), four of which are north of Valencia Road and one south of Valencia. The south block, which is approximately 67.6 acres, is southeast of the existing South Kolb Road Specific Plan and is north of the Union Pacific rail line and east of the abandoned DMAFB rail spur. The block consists of two separate Assessor’s parcels (141-07-0090 and 141-10-0020) (*see Exhibit I.A.1: Location and Vicinity*).

The existing South Kolb Road Specific Plan and proposed Amendment are located within portions of Sections 8, 17 and 20, Township 15 South, Range 15 East, Pima County, Arizona. Both blocks are within the Rincon Southeast/Santa Rita Subregion of Pima County Comprehensive Plan and are represented by Supervisor Ray Carroll in District 4.

2. Existing On-site Land Uses

Both blocks are vacant; however, a portion of the south block has been graded under a railroad exception (refer to A.R.S. Section 11-830 and Pima County Code § 18.01.030-C.1). The graded area is a 300-foot strip along the eastern and southern boundaries and is not visible on the most recent aerial for the site. The railroad use for this site is an extension of the Port of Tucson. The railroad will provide locations for users to have direct access to the rail line from sidings at warehouses, manufacturing facilities and commodity distribution centers. A portion of the south block is also graded for the El Paso Natural Gas line that crosses the northern portion of the block (*see Exhibit I.A.2.a: Existing On-site Land Uses – North Block and Exhibit I.A.2.b: Existing On-site Land Uses – South Block*).

3. Existing Land Uses within One-Quarter-Mile Radius

- a. Existing On-site and Off-site Zoning

Table 1.A.3.a: Existing Zoning

North Block	SR, CI-1
North	SR (COT)
South	SP
East	SR (COT)



West	SP, CB-1
-------------	----------

South Block	RH, SR
North	SR (COT)
South	CI-2, RH
East	CI-2
West	SP

(See Exhibit I.A.3.a: Existing Zoning.)

b. Existing Off-site Land Uses

Table 1.A.3.b: Existing Off-site Land Uses

North Block

North	DMAFB
South	Valencia Road, Vacant
East	Vacant
West	Vacant, Diamond Shamrock

South Block

North	Vacant
South	UA Science & Tech Park
East	Goodwill Donation Center, Hansen Pipe and Products, Vacant
West	Vacant

(See Exhibit I.A.3.b: Existing Off-site Land Uses.)

c. Number of Stories of Off-site Existing Structures

Table 1.A.3.c: Number of Stories of Off-site Existing Structures

North Block

North	No structures
South	No structures
East	No structures
West	1-story



South Block

North	No structures
South	No structures
East	Approximately 3-story, 1-story
West	No structures

(See Exhibit I.A.3.b: Existing Off-site Land Uses.)

- d. Pending or Conditionally-Approved Rezonings and Subdivisions and/or Development Plans Under Review

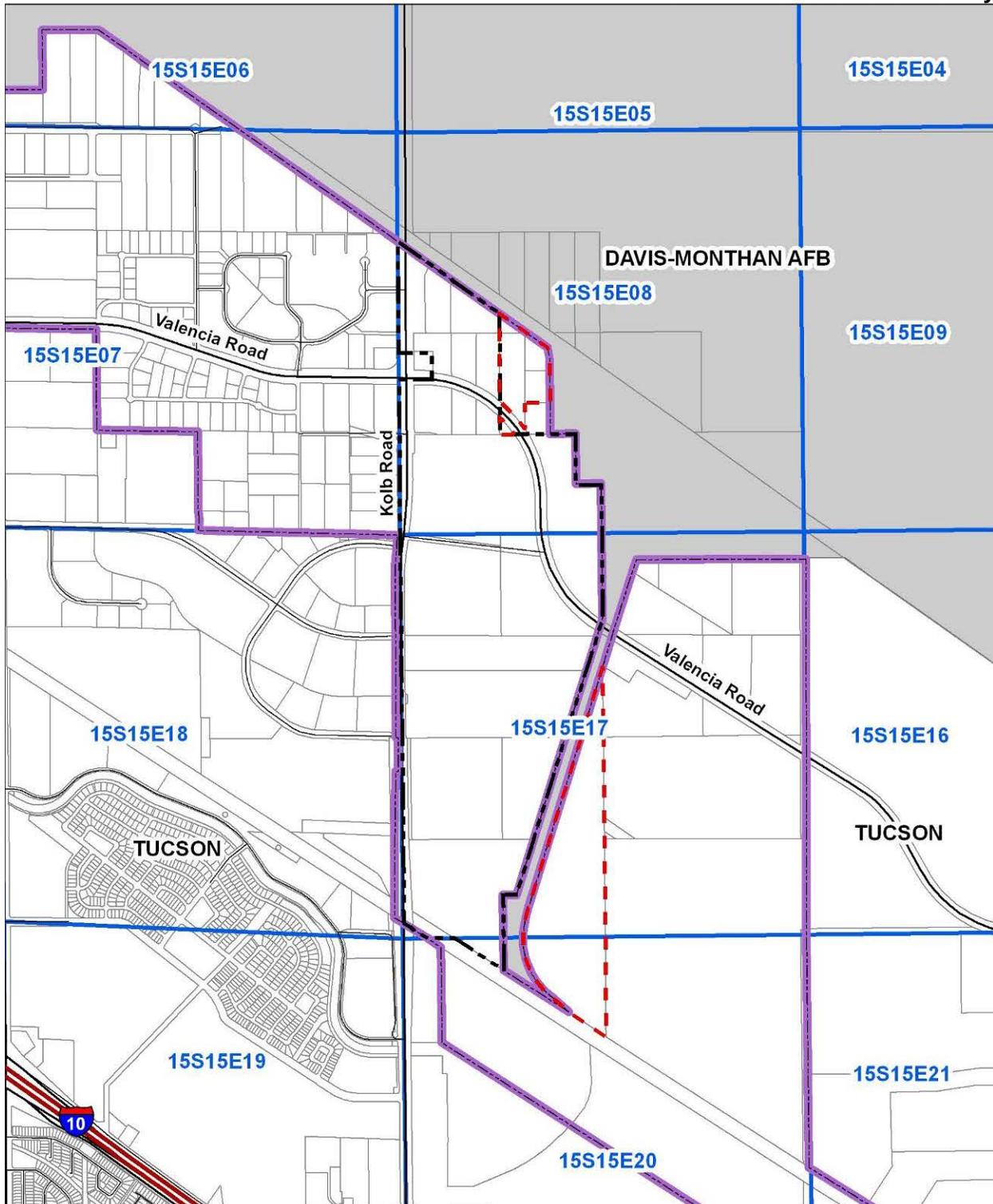
There is a tentative plat submitted for approximately 150 acres under Three B S Properties, LLC directly east of the south block. There are no other pending rezonings, conditionally-approved zonings and subdivisions nor development plans under review near or for the project site.

4. Wells or Well Sites Within 100 Feet

There is only one well located within 100 feet of the Amendment. According to the Arizona Department of Water Resources (ADWR), Wells-55 Registry, there are two registered wells, but only one drilled well within Assessor’s Parcel #141-06-013D southeast of the north block. Well # 535554 was applied for in 1992, but has never been drilled. Well #543693 has been drilled and is an exempt well used for domestic stock. Both wells are registered under Chuck McCain of Vail, Arizona. There are no other wells within 100 feet of the north block or the south block *(see Exhibit I.A.4: Well Locations)*.



Exhibit I.A.1: Location and Vicinity



LEGEND

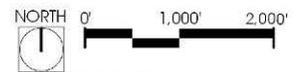
-  South Kolb Specific Plan
-  Amendment Boundary
-  Township, Range & Section
-  Jurisdiction Boundary

Notes:

Project Site is located at:
Township 13S, Range 13E, and Sections 8, 17 & 20

Acreage: Approx. 83.4 AC

Parcel ID #s:
141-06-005B, 141-06-012B, 141-06-012C, 141-06-013B,
141-06-013C, 141-07-0090, 141-10-0020



FILE NAME: LLR-01_location.mxd
SOURCE: Pima County DOT GIS, 2007



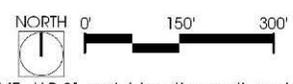
Exhibit I.A.2.a: Existing On-site Land Uses – North Block



Imagery: Aerials Express, May 2007

LEGEND

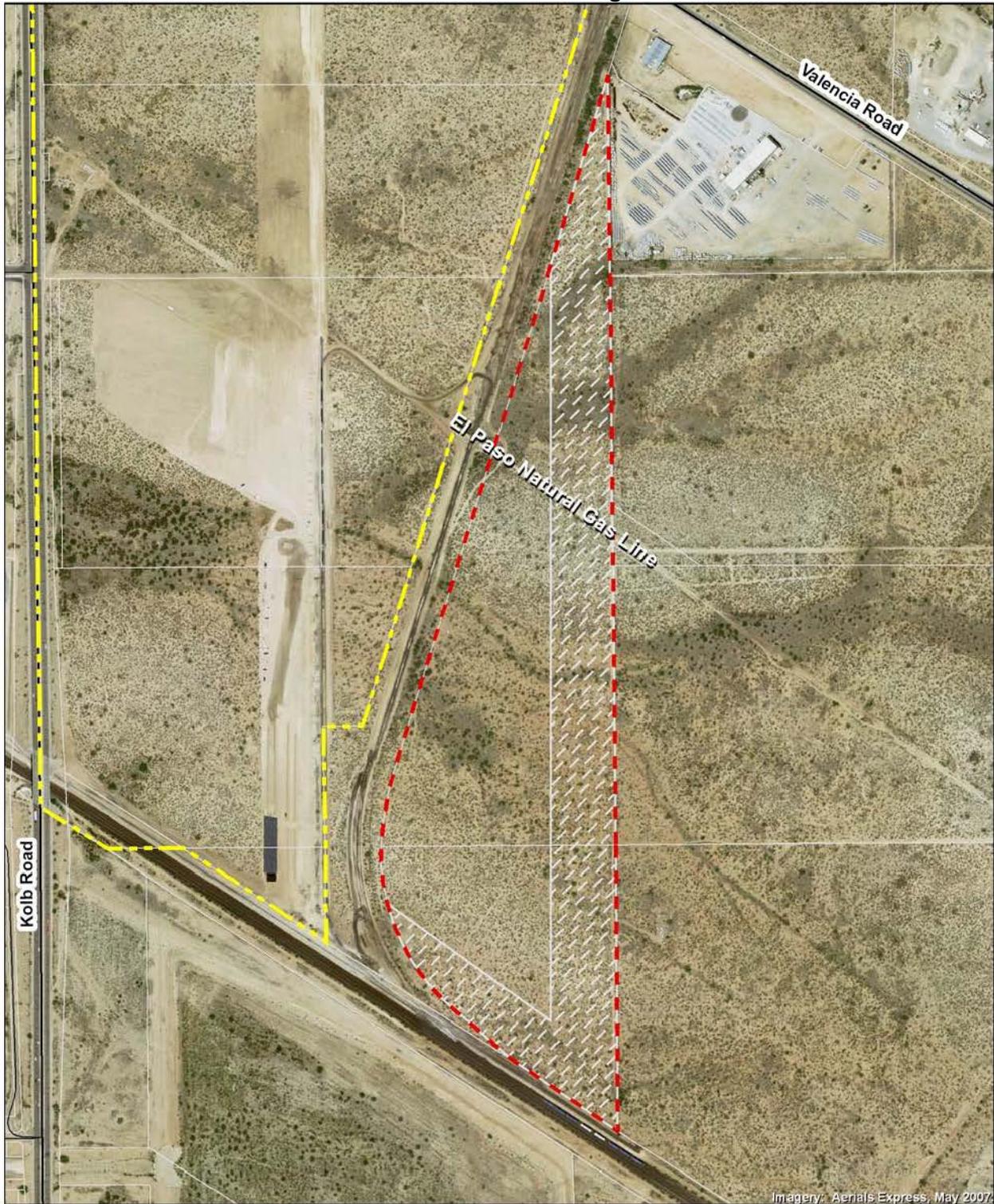
-  South Kolb Specific Plan
-  Amendment Boundary



FILE NAME: LLR-01_aerial_location_north.mxd
SOURCE: Pima County DOT GIS, 2007



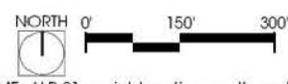
Exhibit I.A.2.b: Existing On-site Land Uses – South Block



Imagery: Aerials Express, May 2007

LEGEND

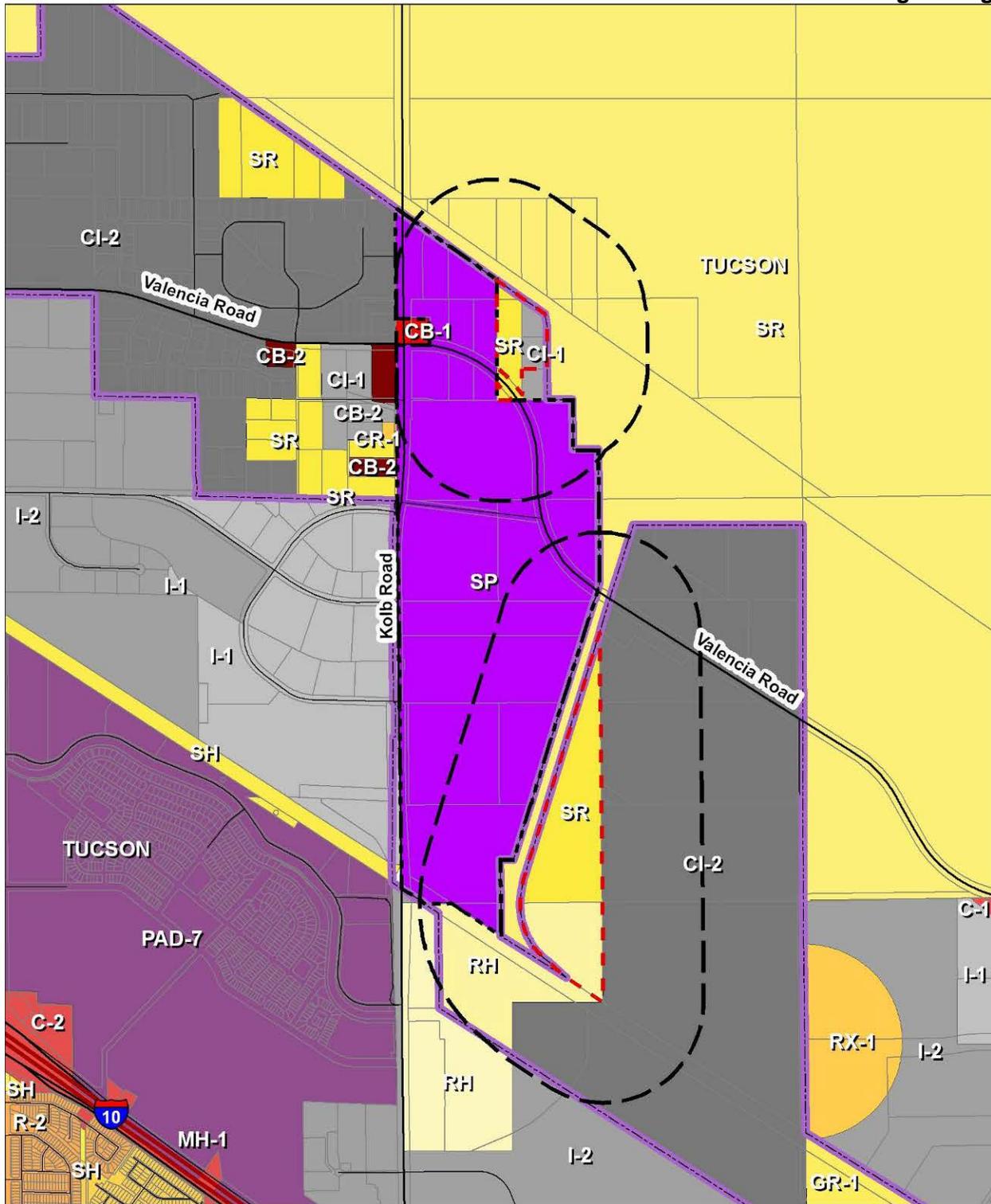
- South Kolb Specific Plan
- Amendment Boundary
- Approximate Area Graded for Railroad Exception



FILE NAME: LLR-01_aerial_location_south.mxd
SOURCE: Pima County DOT GIS, 2007

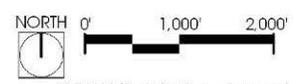


Exhibit I.A.3.a: Existing Zoning



LEGEND

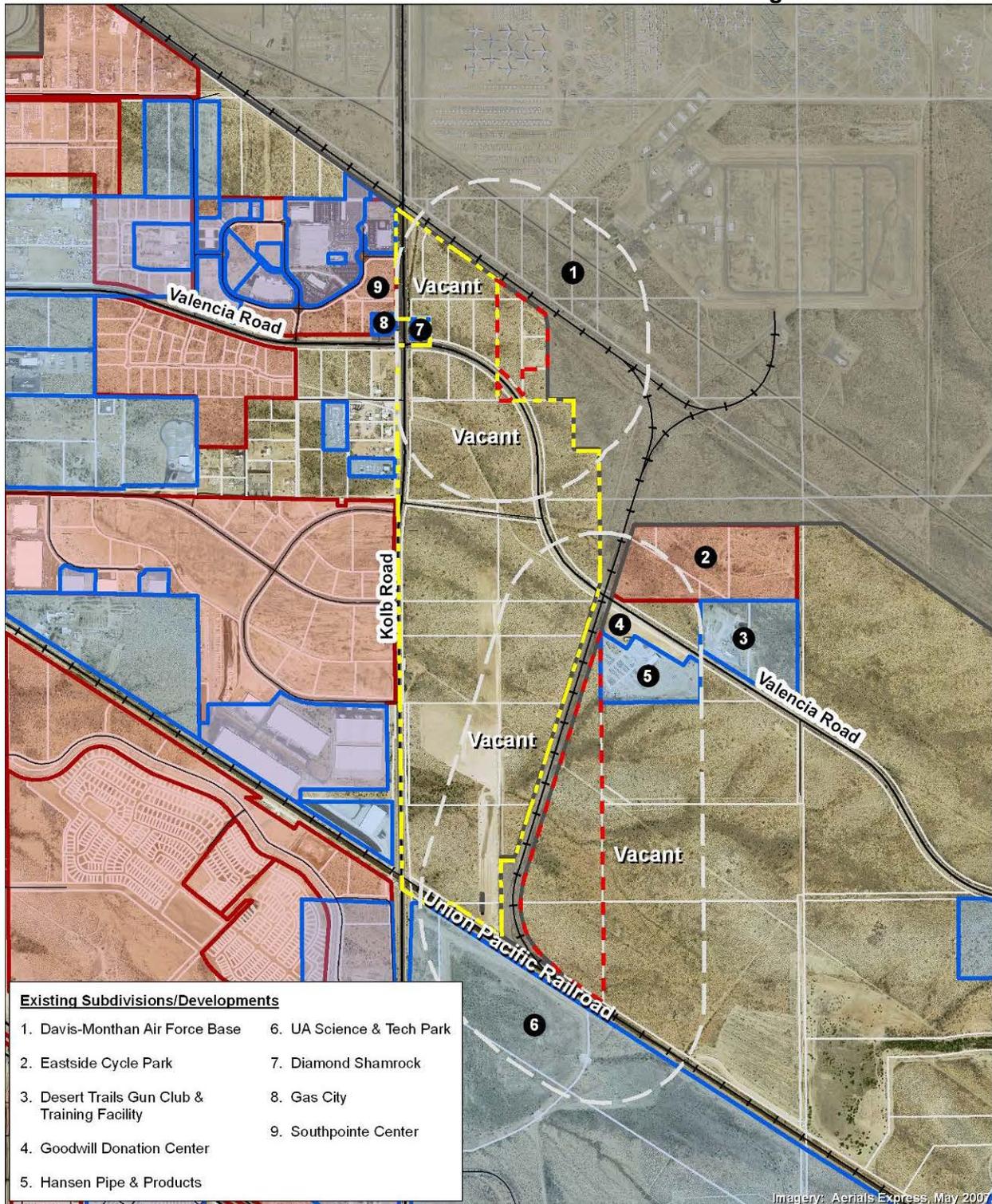
- South Kolb Specific Plan
- Amendment Boundary
- Jurisdiction Boundary
- Quarter Mile Radius



FILE NAME: LLR-01_zoning.mxd
SOURCE: Pima County DOT GIS, 2007



Exhibit I.A.3.b: Existing Off-site Land Uses



LEGEND

- South Kolb Specific Plan
- Amendment Boundary
- Approved Subdivision Plan
- Approved Development Plan
- Davis-Monthan Air Force Base
- Quarter Mile Radius

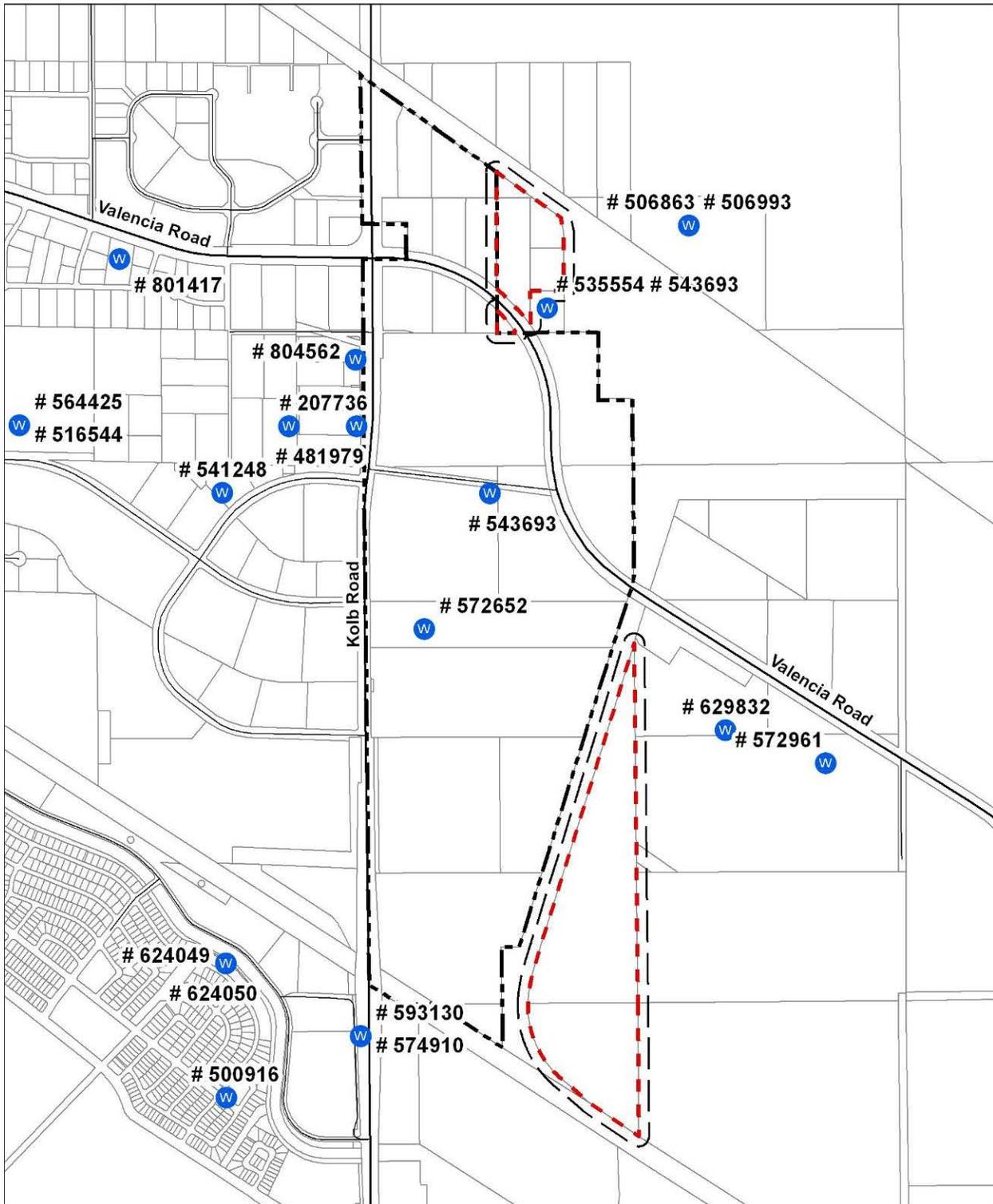
NORTH

 FILE NAME: LLR-01_existinglanduses.mxd

 SOURCE: Pima County DOT GIS, 2007

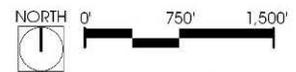


Exhibit I.A.4: Well Locations



LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  100-Foot Radius
-  Wells (Registered ID # Labeled)



FILE NAME: LLR-01_wells.mxd
SOURCE: Pima County DOT GIS, 2007



B. Topography

1. Topographic Characteristics of the Site

The north and south blocks are relatively flat with no significant topographic features. The north block slopes slightly from the southeast to the northwest with a high elevation of 2,804 feet and a low elevation of 2,792 feet. The south block slopes slightly from the east to west in the northern half of the block and southeast to northwest in the southern half of the block, with a high elevation of 2,822 feet and a low elevation of 2,806 feet (see *Exhibit I.B.1: Topography*).

- a. Restricted Peaks and Ridges
There are no restricted peaks or ridges on-site.
- b. Rock Outcrops
There are no rock outcrops on-site.
- c. Slopes of 15% or Greater
There are no slopes greater than 15%.
- d. Any Other Significant Topographic Features
There are no other significant topographic features on-site.

2. Pre-development Average Cross Slope Calculations

The pre-development cross slope is 2.26% per the formula taken from Chapter 18.61.051 of the Pima County Zoning Code.

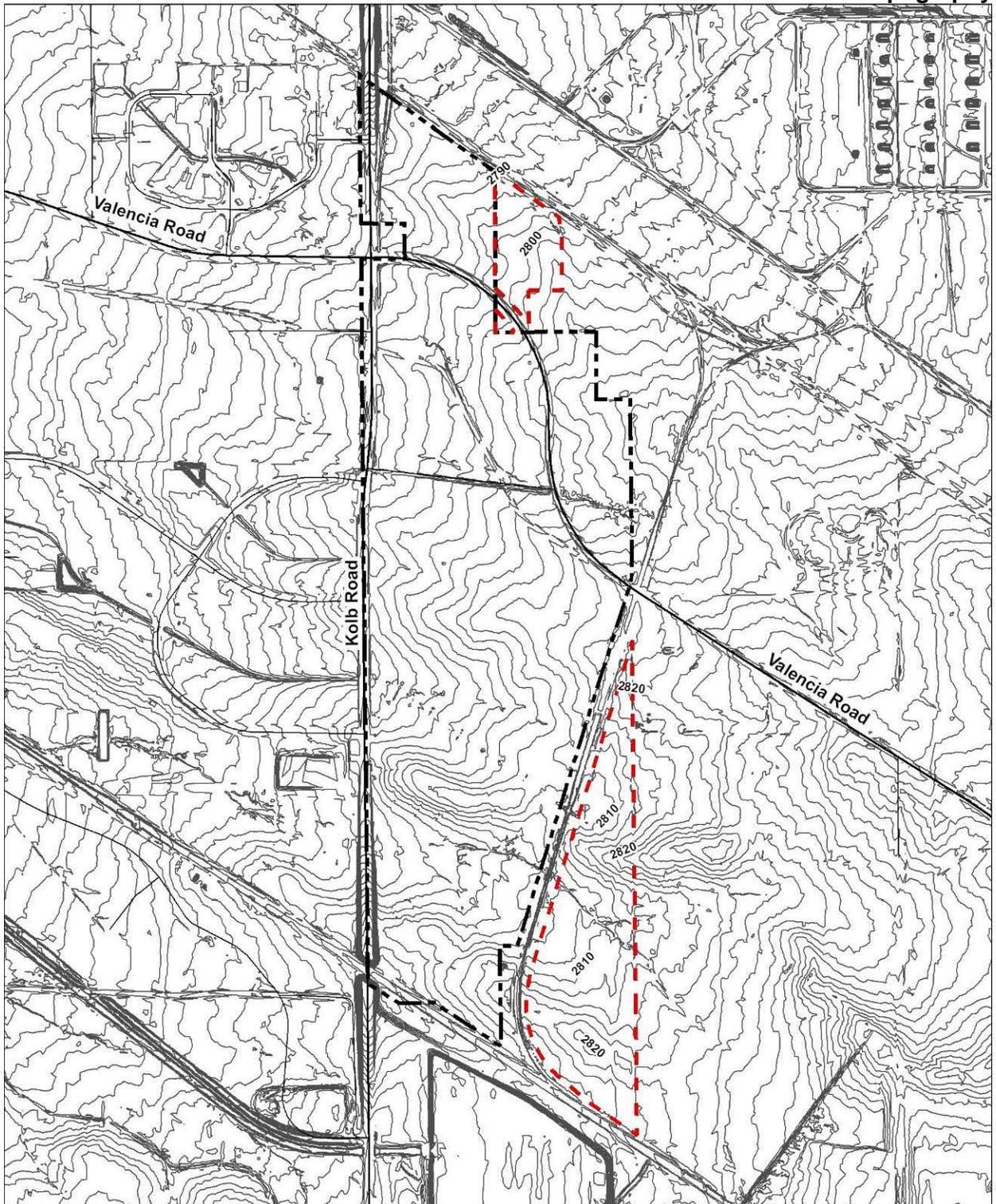
$$\text{Average cross slope} = \frac{(I \times L \times 0.0023)}{A} = \frac{(2 \times 40972 \times 0.0023)}{83.4} = 2.26\%$$

Where:

- I = contour interval (2')
- L = total length of contours (40,972)
- 0.0023 = conversion of "square feet" into "acres x 100"
- A = total site area in acres (83.4)



Exhibit I.B.1: Topography



LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  2' Contour

Note: There are no slopes greater than 15%



FILE NAME: LLR-01_topo_slope.mxd
SOURCE: Pima County DOT GIS, 2007



C. Hydrology

A drainage report was produced by Baker and Associates Engineering Inc. for the northern portion of the existing South Kolb Road Specific Plan, which includes the north block of the Amendment area. A drainage report was produced by PSOMAS for the southern portion of the existing South Kolb Road Specific Plan, which includes the south block of the Amendment area. Information from both reports has been condensed to address the Amendment area and is included in this section. Both reports have been submitted under a separate cover for review as part of the tentative plat/development plan.

1. Off-site Watersheds

North Block

Existing-condition off-site watersheds affecting the north block are shown in Exhibit I.C.1.a: Off-Site Watersheds – North Block. The site is characterized by predominantly desert brush on gently sloping terrain with low to moderate drainage definition. There are two watersheds that drain into the north block of the Amendment. Drainage Area A is approximately 497 acres and has a regulatory flow of 1,053 cfs. Drainage Area C is approximately 157 acres and has a regulatory flow of 404 cfs. Under existing conditions, off-site drainage generally flows from east to west. Regulatory flows for off-site Drainage Areas A and C were modeled using HEC-RAS. The resulting flood prone lines are illustrated on Exhibit I.C.4.a: Existing On-Site Hydrology. The site lies within a balanced drainage basin as listed by Pima County, which requires detention under developed conditions.

South Block

Existing-condition off-site watersheds affecting the south block are shown in Exhibit I.C.1.a: Off-Site Watersheds – South Block. There are 3 major natural drainage ways that flows southeast to northwest across the south block of the Amendment and draining towards the Santa Cruz River west of the site. Table I.C.4: Summary of Hydrologic Analysis (Existing On-Site and Off-Site Conditions) – South Block provides information regarding existing conditions for off-site watersheds affecting the property.

2. Off-site Natural or Man-made Features

North Block

There are no off-site natural or man-made features that affect the north block.

South Block

There are 5-60" aluminum culverts onsite beneath the Railroad Spur crossing located in watershed Concentration Point E6 east of the concentration point. These culverts will be kept in the developed conditions site plan. Off-site at Kolb Road there is a 100-year access culvert cross-drainage. In addition to the



existing culverts there is one existing channel at the northern boundary of the site.

3. Upstream Off-site Watersheds with 100-Year Discharges Greater than 100 cfs

North Block

Both watersheds have a discharge greater than 100 cfs as noted above.

South Block

Two of the three watersheds have a discharge greater than 100 cfs as noted in Table I.C.4.

4. On-site Hydrology

North Block

Existing-condition on-site runoff was computed and labeled on Exhibit I.C.4.a: Existing On-Site Hydrology. A FEMA special flood hazard area (Map #04019C2265, February 8, 1999) Zone "A" exists in Planning Area C; however it does not encroach upon the areas proposed for amendment. There is existing 100-year floodplain along the northern boundary of the north block which has been identified on Exhibit I.C.4.a. The site is predominately characterized by sheetflow, which generally flows from east to west.

South Block

Per the Federal Emergency Management Agency (FEMA), the south block of the Amendment is located within Zone X (unshaded) (FIRM Panels 04019C2265K and 04019C2855K). Per the FIRM Panel legend, Zone X (unshaded) denotes areas outside of the 500-year floodplain.

A HEC-RAS model was developed to delineate the 100-year floodplain of all watersheds with discharges greater than 100 cfs. The calculations are provided in the PSOMAS drainage report. The HEC-RAS models were developed in the following manner:

- 1) Cross sections were cut at varied intervals along reaches through the site.
- 2) Manning's values were estimated, contraction/expansion coefficients entered, and reach and bank lengths were entered.
- 3) Boundary conditions were set using normal depth water surface elevations per hydraulic calculation using the cross-section geometry.
- 4) A supercritical flow regime analysis was run using the HEC-RAS program.



5) The 100-year floodplain was established by interpolating the water surface elevations between each cross section.

The floodplain for the northern wash has been referenced from the report developed by Baker Engineering for the northern portion of the existing South Kolb Road Specific Plan, which includes the north block of the Amendment. The erosion hazard setbacks (EHS) calculated per section 16.28.030 of the Pima County Drainage and Channel Design Standards for Local Drainage (Reference 5) are mapped on Exhibit I.C.4.b: On-Site Hydrology – South Block. The setback was determined using only the largest downstream flow value for each channel. The EHS is mapped from the 100-year WSEL.

Table I.C.4: Summary of Hydrologic Analysis – South Block

Concentration Point	Area (ac)	Length (ft)	Mean Slope (%)	Weighted Basin Factor (n_{bw100})	Tc (min)	Watershed Peak Discharge Q_{100} (cfs)	Contributing Watersheds	
							Concentration	$Q_{peak\ total}$
E1	47	3033	0.73	0.035	24.3	213	E1, E5, OS1*, OS2	1382
E2	85	2849	1.05	0.035	19.7	405	---	405
E3	34	2950	0.58	0.035	26.9	141	---	141
E4	17	1565	1.02	0.035	12.6	98	---	98
E5	5	1000	0.90	0.035	9.4	32	E5, OS1*	1085
E6	128	2881	0.76	0.035	22.9	592	E6, OS3, OS4	1768
OS1*	497	13000	0.88	0.035	49.6	1053	---	1053
OS2	18	1576	1.02	0.035	13.7	84	---	84
OS3	77	3706	0.94	0.035	26.6	280	---	280
OS4	321	7609	0.95	0.035	47.1	896	---	896

The north and south blocks of the Amendment have been determined to be non-jurisdictional pursuant to Section 404 of the Federal Water Pollution Control Act amendments of 1972, 33 USC 1334.

5. Existing Drainage Conditions along the Downstream Property Boundary

North Block

There is one on-site culvert crossing (Valencia Road) that cannot fully convey the existing condition $Q_{100} = 1,053$ cfs. The same is true downstream at Kolb Road. It is anticipated that the capacity of these structures will be addressed by others as a part of the proposed Valencia Road realignment project.

South Block

Downstream properties from the Amendment include the existing South Kolb Road Specific Plan, which are addressed by the drainage report.



Exhibit I.C.1.a: Off-site Watersheds – North Block

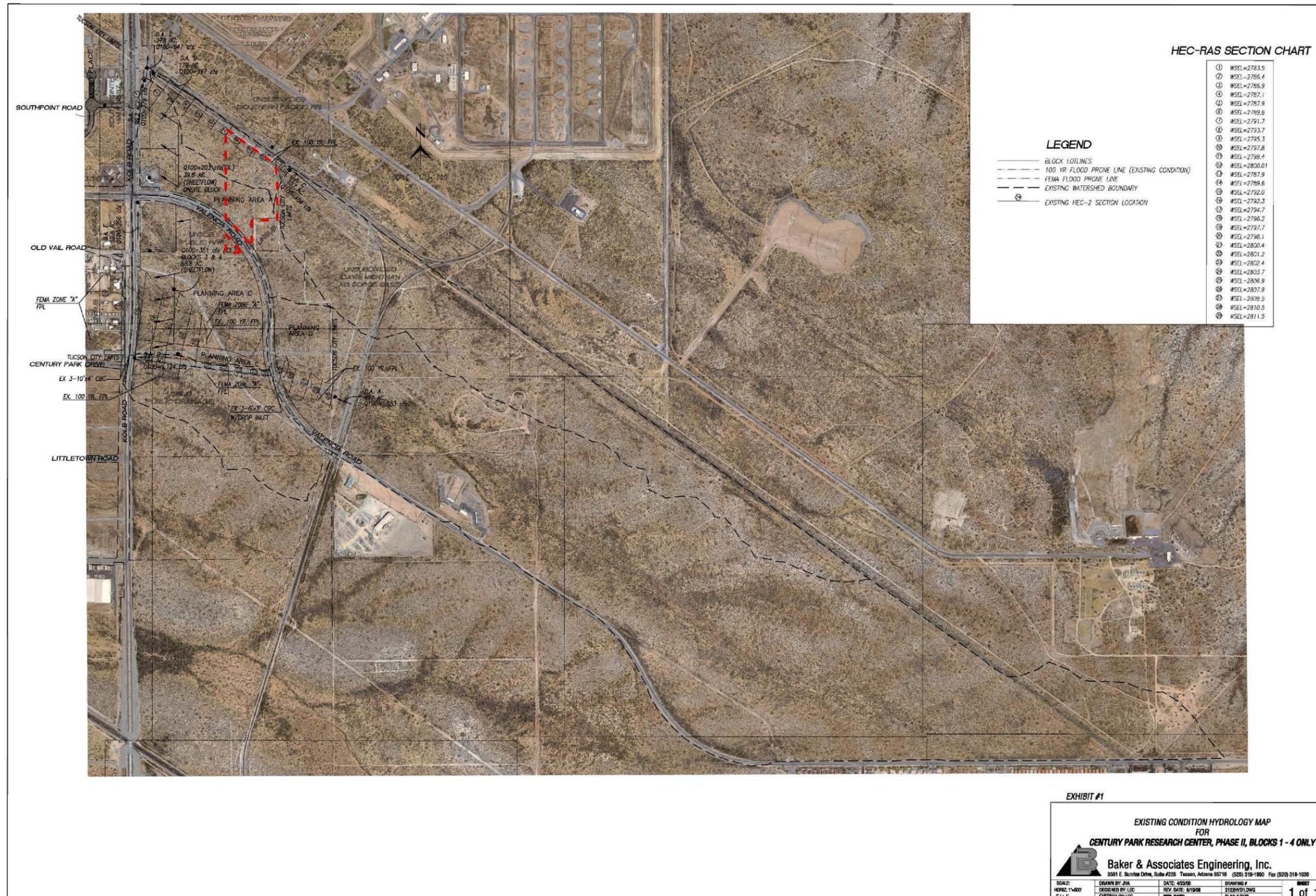


Exhibit I.C.4.a: On-site Hydrology – North Block

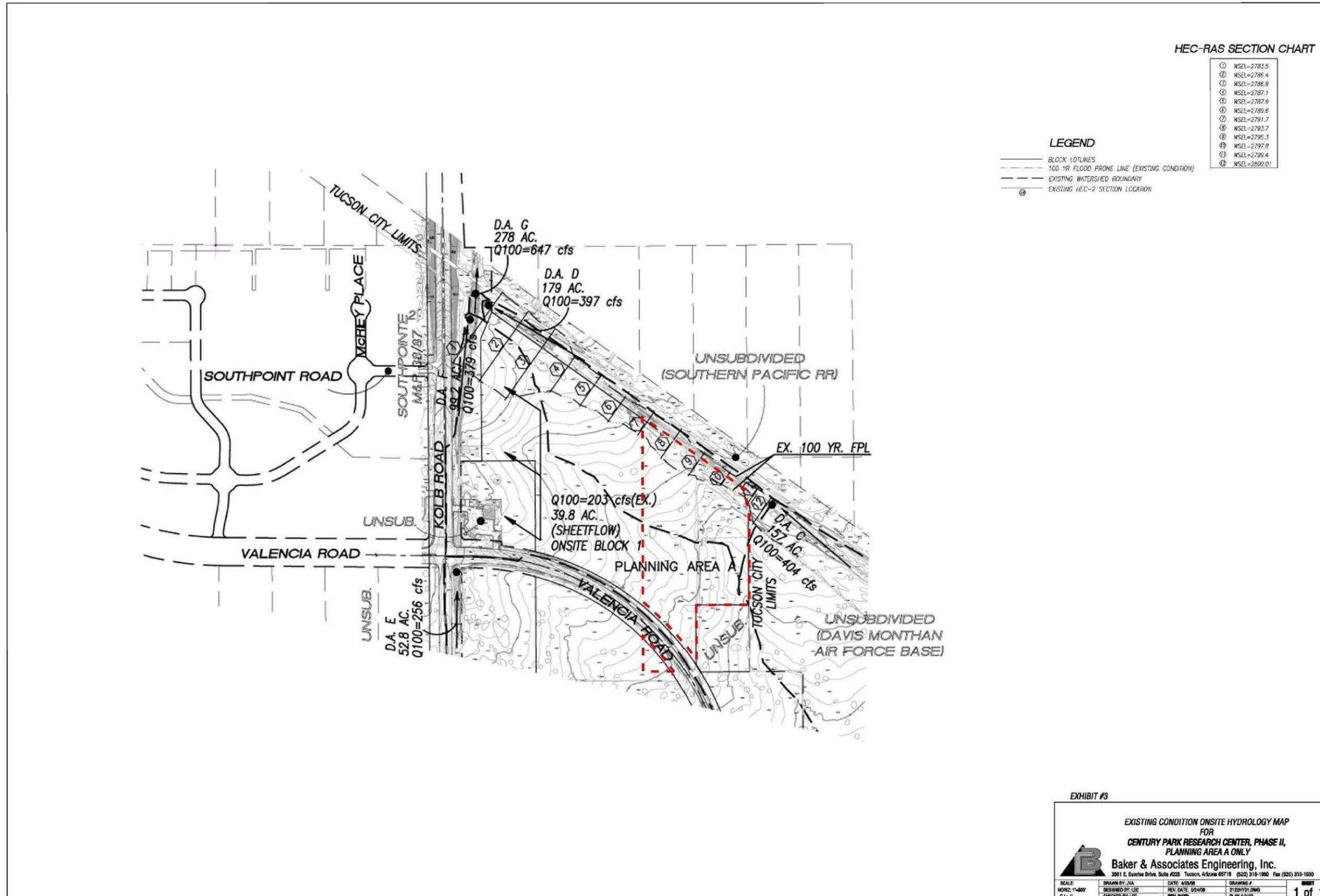


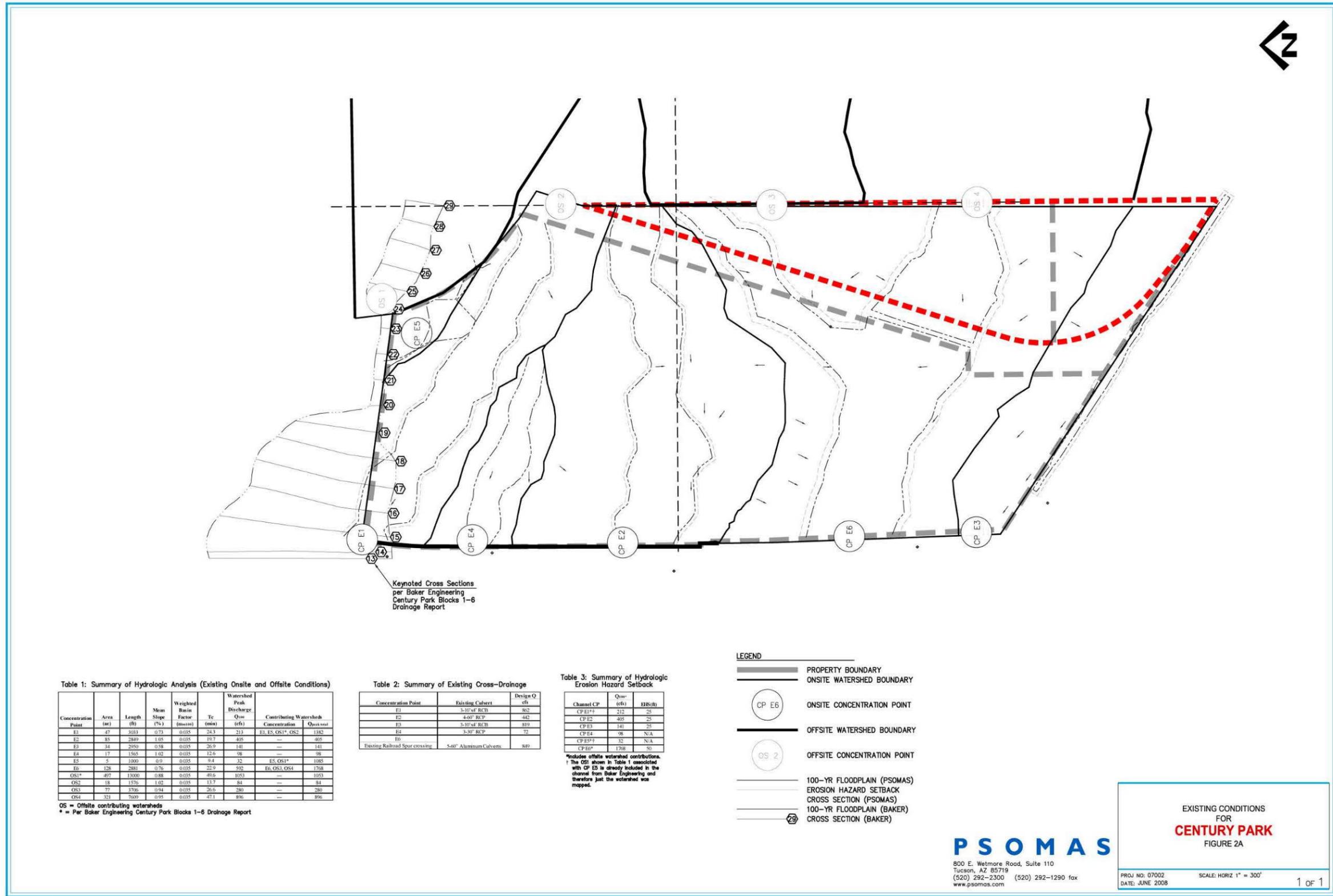
EXHIBIT #3

EXISTING CONDITION ONSITE HYDROLOGY MAP
FOR
CENTURY PARK RESEARCH CENTER, PHASE II,
PLANNING AREA A ONLY

Baker & Associates Engineering, Inc.
3641 E. Sunrise Drive, Suite #225, Tucson, Arizona 85718 (520) 318-1890 Fax (520) 318-1890

SCALE: HORZ: 1"=400' V: 1"=10'	DRAWN BY: JVA DESIGNED BY: LEC CHECKED BY: LEC	DATE: 4/23/08 REV. DATE: 5/24/08 REV. DATE:	DRAWING # 2122HYD1.DWG PLAN # 2122	SHEET 1 of 1
--------------------------------------	--	---	--	-----------------





D. Overlay Zones

1. Airport Environs Zone

The purpose of the Airport Environs Zone is to protect persons and property within the environs of DMAFB and other area airports, provide aircraft safety, and regulate land use to ensure compatibility with airports. The south block of the Amendment is impacted by DMAFB Height Overlay Zone and Accident Potential Zone 2. The Airport Height Overlay Zone limits building height to 300 feet and 280 feet for portions of the northern half of the south block. The Compatible Use Zone (CUZ-4), which covers the entire south block, limits interior noise levels of new noise-sensitive land uses, and accessory offices and indoor areas where the public is received, to forty-five Ldn. These zones do not impact the north block of the Amendment (see *Exhibit I.D.1: Airport Environs Zone Constraints*).

2. Joint Land Use Study

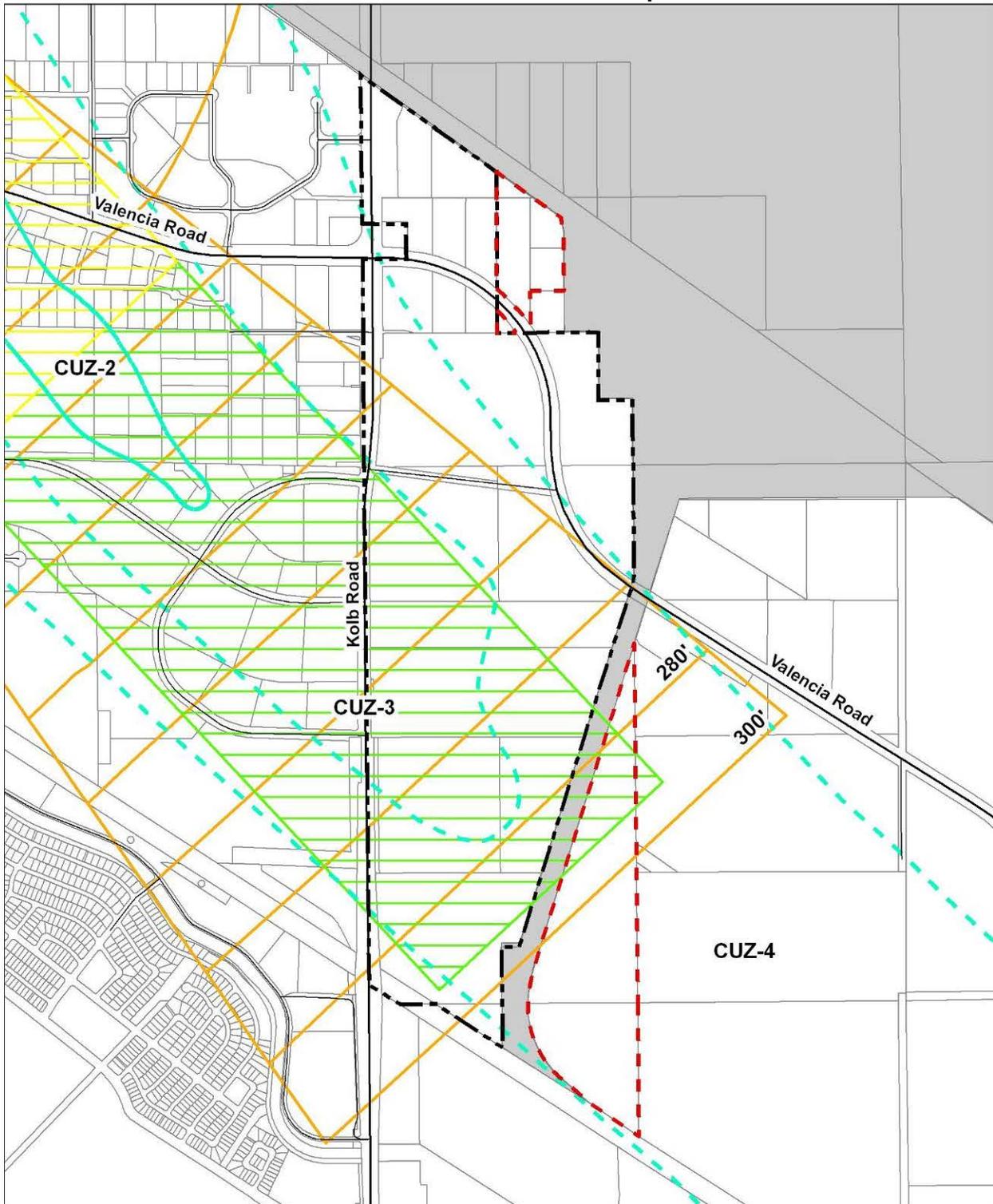
As an outcome of the DMAFB/Tucson/Pima County Joint Land Use Study (JLUS) Compatible Land Use Plan, the Comprehensive Plan proposes land use policies that will impact portions of the Amendment area due to its location near DMAFB. These land use policies regulate height and land use to ensure safe air approach and departure, avoid the concentration of population in potential accident areas, and reduce the harmful effect of noise exposure on humans and animals.

The north block is impacted by NCZ-B Noise Control Zone B (S-26) and NCZ-A Noise Control Zone A (S-27). The north half of the block is impacted by NCD-A, while the south half of the block is impacted by NCZ-B. Both noise districts require compliance with noise reduction standards. A list of compatible uses for both noise districts is included in the Pima County Comprehensive Plan Policies and Land Use Intensity Legend; however, both districts allow a variety of commercial and industrial uses that are proposed for the Amendment (see *Exhibit I.D.2: Joint Land Use Study Constraints*).

The entire south block is impacted by ADC-2 Approach-Departure Corridor 2/Accident Potential Zone (S-24) and Noise Control Zone B. ADC-2 recommends non residential uses, primarily industrial, that have a relatively low employment density. Planned development area and floor area ratio is also regulated in this corridor. A list of compatible uses and standards is included in the Pima County Comprehensive Plan Policies and Land Use Intensity Legend; however, proposed uses for this block will be compatible with the recommended uses (see *Exhibit I.D.2: Joint Land Use Study Constraints*).



Exhibit I.D.1: Airport Environs Zone Constraints

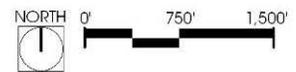


LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  Davis-Monthan Air Force Base

AEZ Constraints

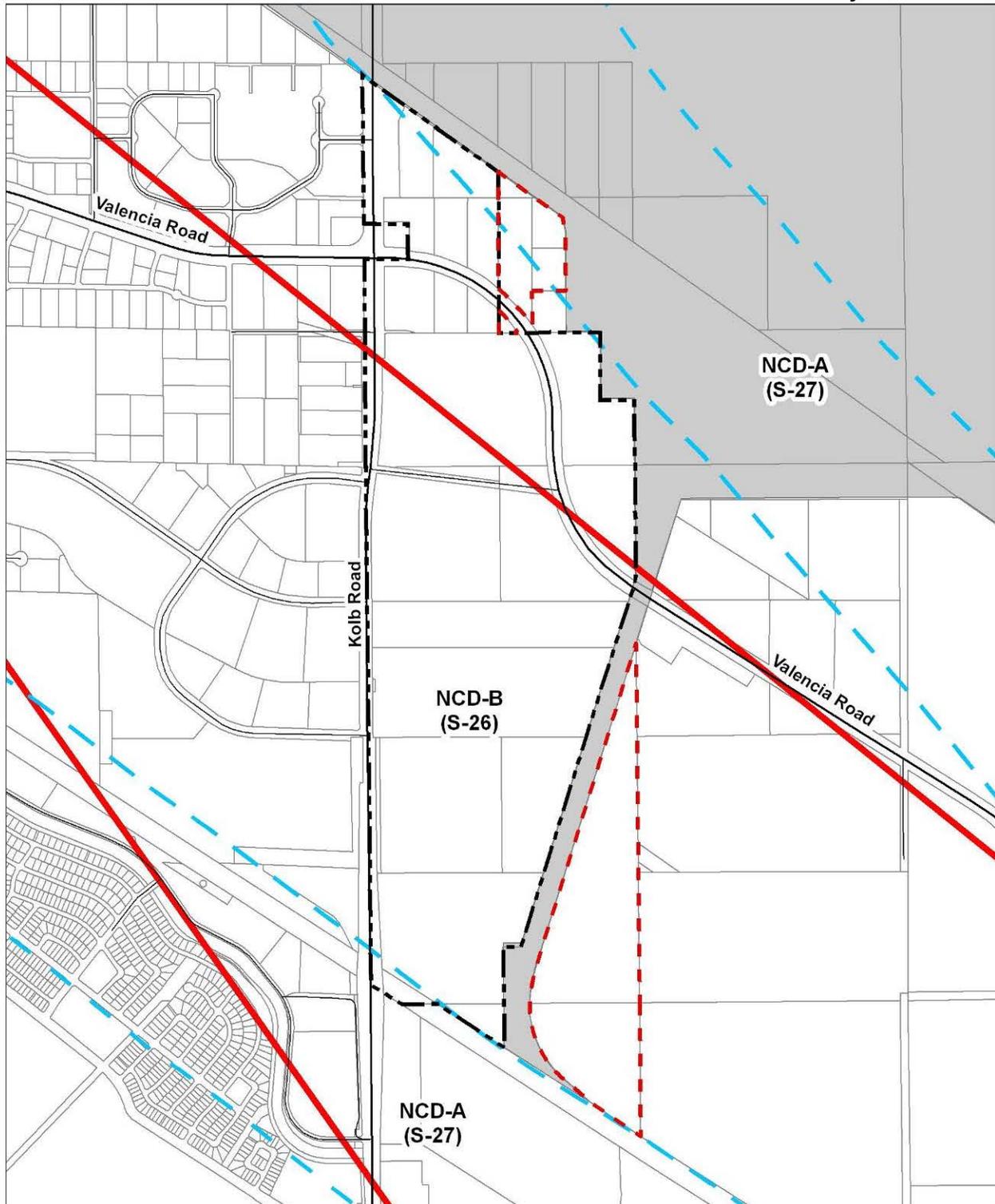
-  Accident Potential Zone 1
-  Accident Potential Zone 2
-  Airport Height Overlay Zone (labeled)
-  Compatible Use Zone (labeled)



FILE NAME: LLR-01_AEZ_constraints.mxd
 SOURCE: Pima County DOT GIS, 2007



Exhibit I.D.2: Joint Land Use Study Constraints

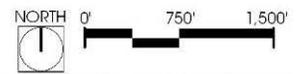


LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  Davis-Monthan Air Force Base

JLUS Constraints

-  DMAFB Approach Departure Corridor 2
-  Noise Control Districts (labeled)



FILE NAME: LLR-01_JLUS_constraints.mxd
SOURCE: Pima County DOT GIS, 2007



E. Vegetation

1. Inventory and Description

a. On-Site Vegetation Communities and Associates

Sonoran Desertscrub is the only vegetation community found on both blocks. This community is characterized by large stands of triangle leaf bursage, creosote bush, prickly pear, and cholla interspersed with assorted trees, cacti, shrubs, and annual and perennial forbes and grasses. Trees and shrubs consist of velvet mesquite, blue palo verde, foothills palo verde, and desert hackberry. Portions of the south block are identified as Xeroriparian Habitat Class C by Pima County (see *Exhibit I.E.1.a: Vegetation Communities*).

b. Federally-listed Threatened and Endangered Species, Saguaros, and other Prominent Cacti and Trees

Fishhook barrel cacti are present on-site. There are no saguaros or other prominent cacti or trees on-site.

c. Especially Important Vegetation

Due to the flat and generally open nature of the site, existing on-site vegetation has moderate value in terms of providing scenic value, screening and buffering; however, there are substantial portions of both the north and south blocks that have been previously disturbed due to grading or wildcat dumping. Existing on-site vegetation has good value in terms of providing soil stabilization.

2. Vegetative Densities

The density figures are based upon the ratio of canopy cover to bare ground (see *Exhibit I.E.2: Vegetation Densities*). The north block is primarily medium density vegetation, with some areas of low density vegetation. There has been some previous grading on the eastern portion of the site, which has left that area void of any vegetation. The south block has areas of high, moderate and low density vegetation. There is a 300-foot wide strip along the eastern border of the south block which has been graded under a railroad exception (*please refer to Section I.A.2*); this area amounts to approximately 33 acres and is considered low density. This is not evident on the aerial photograph used to examine the site, but rather from site visit verification. The area west of that strip is primarily moderate density; however, there is a small area of high density vegetation which is associated with existing on-site drainage. The vegetative densities on-site in terms of approximate percentages are categorized as follows:

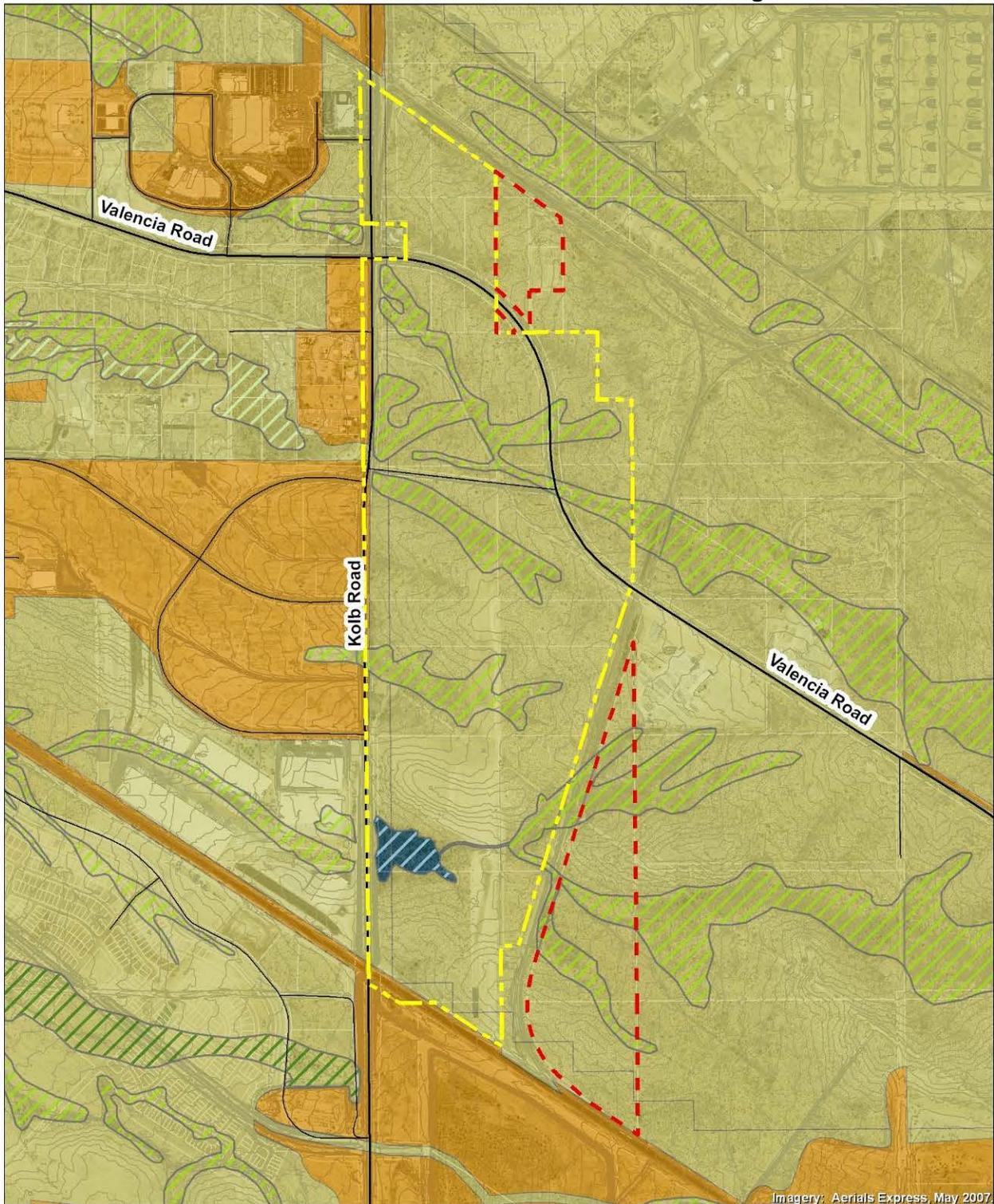


Table I.E.2: Density Table

High Density	Medium Density	Low Density
3%	52%	45%



Exhibit I.E.1.a: Vegetation Communities



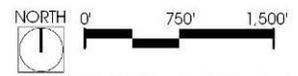
Imagery: Aerials Express, May 2007

LEGEND

- South Kolb Specific Plan
- Amendment Boundary
- 2' Contour
- Xeroriparian Habitat Class C

Vegetation - Composite

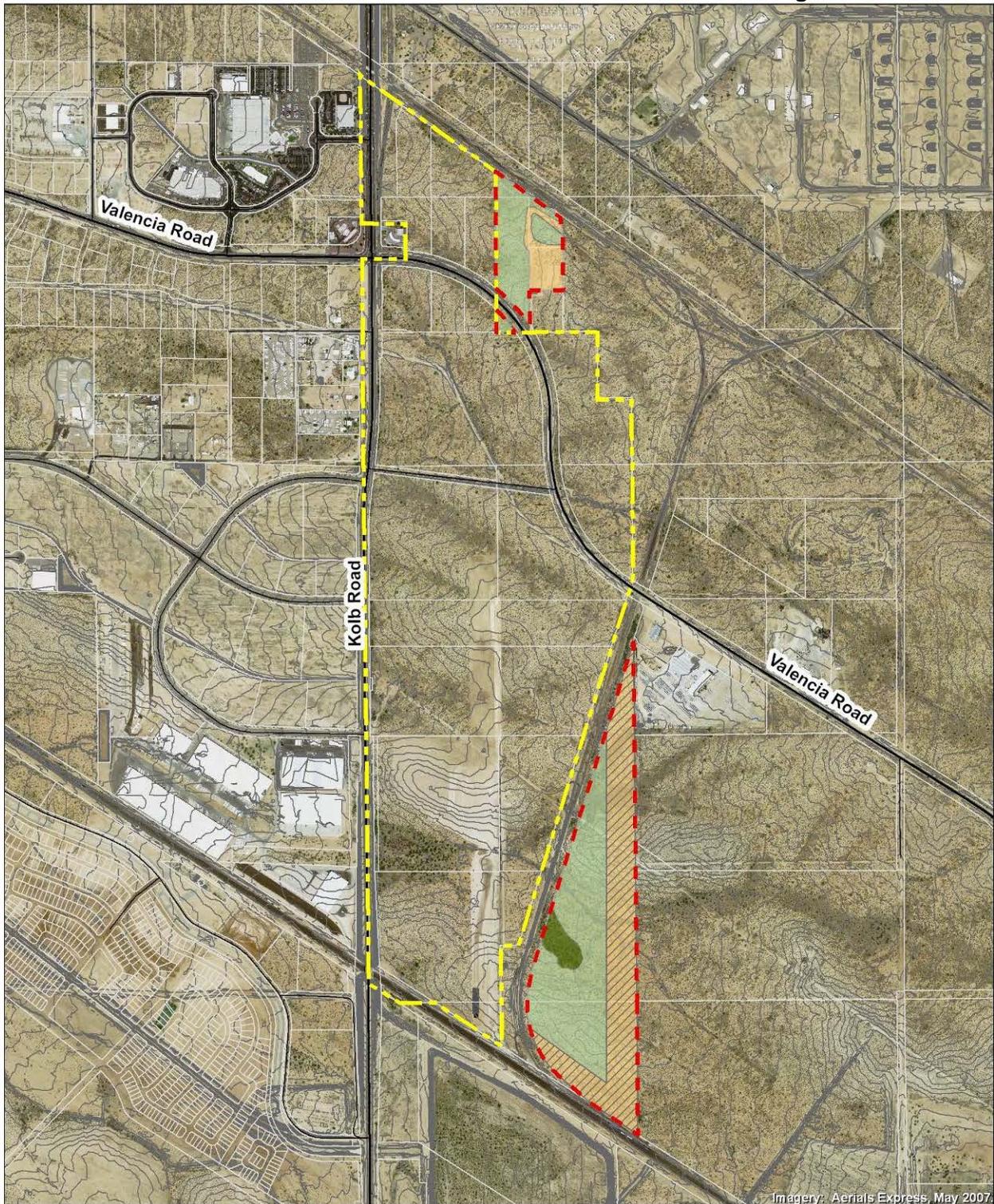
- Agriculture/Developed/Water/Bare Ground
- Sonoran Desertscrub
- Sonoran Riparian and Oases Forest



FILE NAME: LLR-01_vegetation.mxd
SOURCE: Pima County DOT GIS, 2007



Exhibit I.E.2: Vegetation Densities



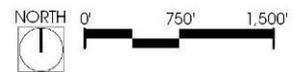
Imagery: Aerials Express, May 2007

LEGEND

- South Kolb Specific Plan
- Amendment Boundary
- 2' Contour

Vegetation Density

- Low
- Moderate
- High
- Area Graded for Railroad Exception



FILE NAME: LLR-01_vegetation_density.mxd
SOURCE: Pima County DOT GIS, 2007



F. Wildlife

1. Arizona Game and Fish Department Letter

A list of special status species within the project area is provided from the Arizona Game and Fish Department. The information was accessed using the Arizona Online Environmental Review Tool (see *Exhibit I.F.1.a: Arizona Game and Fish Department Online References – North Block* and *Exhibit I.F.1.b: Arizona Game and Fish Department Online References – South Block*).

a. State-listed Threatened or Endangered Species

The property does not fall into the Cactus Ferruginous Pygmy-Owl Critical Habitat or Habitat Zone 1. The property has not been surveyed for pygmy owls, and there are no surveys planned. According to the Arizona Game and Fish Department’s Heritage Data Management System (HDMS), there are two Special Status Species, Western Burrowing Owl (*Athene cunicularia hypugaea*) and Cave Myotis (*Myotis velifer*), known to occur within a 3-mile radius of the project site as noted in the table below:

Table I.F.1.a: Special Status Species

Common Name	Scientific name	Status
Western Burrowing Owl	<i>Athene cunicularia hypugaea</i>	SC, S
Cave Myotis	<i>Myotis velifer</i>	SC, S

Status Definitions: S - Sensitive
 SC - Species of Concern

b. High Densities of a Given Species’ Population

There is no information regarding on-site species densities.

c. Aquatic or Riparian Ecosystems

The project does not occur within the vicinity of any Proposed or Designated Critical Habitats according to the Arizona Game and Fish Department; however, portions of the south block have been identified as Xeroriparian Habitat Class C by Pima County.

d. Significant Habitat or Areas of Concern

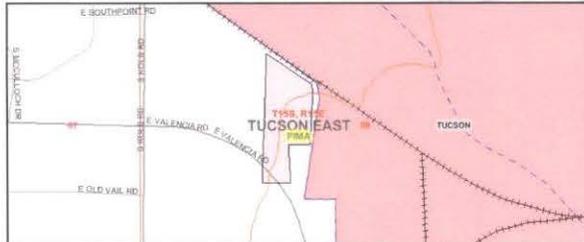
There are no significant habitats or areas of concern on the site.



Exhibit I.F.1.a: Arizona Game and Fish Department Online References – North Block

Arizona's On-line Environmental Review Tool
 Search ID: 20080605006170
 Project Name: South Kolb - North Block
 Date: 6/5/2008 3:39:12 PM

Project Location



The Department appreciates the opportunity to provide in-depth comments and project review when additional information or environmental documentation becomes available.

Special Status Species Occurrences/Critical Habitat/Tribal Lands within 3 miles of Project Vicinity:

Name	Common Name	ESA	USFS	BLM	State
<i>Athene cunicularia hypugaea</i>	Western Burrowing Owl	SC		S	
<i>Myotis velifer</i>	Cave Myotis	SC		S	

Project Name: South Kolb - North Block
Submitted By: Steven Eddy
On behalf of: PRIVATE
Project Search ID: 20080605006170
Date: 6/5/2008 3:39:06 PM
Project Category: Development Within Municipalities, Commercial/Industrial (mall) and associated infrastructure, New construction
Project Coordinates (UTM Zone 12-NAD 83): 515479.727, 3555913.761 meter
Project Area: 11.912 acres
Project Perimeter: 1081.739 meter
County: PIMA
USGS 7.5 Minute Quadrangle ID: 1766
Quadrangle Name: TUCSON EAST
 Project locality is not anticipated to change

Location Accuracy Disclaimer

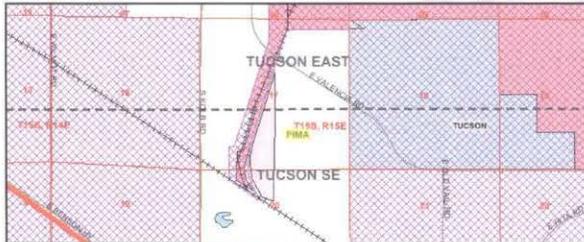
Project locations are assumed to be both precise and accurate for the purposes of environmental review. The creator/owner of the Project Review Receipt is solely responsible for the project location and thus the correctness of the Project Review Receipt content.



Exhibit I.F.1.b: Arizona Game and Fish Department Online References – South Block

Arizona's On-line Environmental Review Tool
 Search ID: 20080605006169
 Project Name: South Kolb - South Block
 Date: 6/5/2008 2:54:48 PM

Project Location



The Department appreciates the opportunity to provide in-depth comments and project review when additional information or environmental documentation becomes available.

Special Status Species Occurrences/Critical Habitat/Tribal Lands within 3 miles of Project Vicinity:

Name	Common Name	ESA	USFS	BLM	State
Athene cunicularia hypugaea	Western Burrowing Owl	SC		S	
Myotis velifer	Cave Myotis	SC		S	

Project Name: South Kolb - South Block
Submitted By: Steven Eddy
On behalf of: PRIVATE
Project Search ID: 20080605006169
Date: 6/5/2008 2:54:41 PM
Project Category: Development Within Municipalities, Commercial/industrial (mall) and associated infrastructure, New construction
Project Coordinates (UTM Zone 12-NAD 83): 515698.168, 3553879.866 meter
Project Area: 55.007 acres
Project Perimeter: 2949.020 meter
County: PIMA
USGS 7.5 Minute Quadrangle ID: 1766
Quadrangle Name: TUCSON EAST
Project locality is not anticipated to change

Location Accuracy Disclaimer

Project locations are assumed to be both precise and accurate for the purposes of environmental review. The creator/owner of the Project Review Receipt is solely responsible for the project location and thus the correctness of the Project Review Receipt content.



G. Soils

1. On-Site Soils Testing

No known soil testing has been done on the subject property; however, general information has been obtained from the Pima County DOT Geographic Information Services indicating the presence of three soil type on-site: Hantz Loam; Sahuarita Soils, Mohave Soils and Urban Land; and Tubac Gravelly Loam. Soil testing is expected to be done prior to development.

Table I.G.1: Soil Descriptions

Association	Characteristics	Soil Make Up	Runoff Potential	Hazard of Erosion
Hantz Loam, 0 to 1% Slopes	This soil is associated with level swales on alluvial fans and floodplains.	Typically, the surface layer is brown loam about 5 inches thick. The subsurface layer is grayish brown clay loam 7 inches thick. The substratum is grayish brown clay 33 inches thick. The next layer is brown clay 16 inches or more thick. These soils are calcareous throughout. In some areas, the surface layer is silty clay loam, clay loam, or clay.	Moderate	Slight
Sahuarita Soils, Mohave Soils and Urban Land, 1 to 5% Slopes	This soil is associated with floodplains, alluvial fans, and fan terraces.	<p>The Sahuarita soil is very deep and well drained. Typically, the surface is covered by 35 to 55% gravel. The surface layer is light yellowish brown very gravelly fine sandy loam about 3 inches thick. The subsoil is light yellowish brown fine sandy loam 25 inches thick. The next layer is a buried subsoil of brown loam 17 inches thick and brown very gravelly sandy clay loam 15 or more inches thick. Common fine lime filaments are in the buried subsoil. These soils are calcareous throughout. In some areas, the surface layer is fine sandy loam.</p> <p>The Mohave soil is very deep and well drained. It formed in mixed alluvium. The surface layer is yellowish brown loam about 3 inches thick. The upper 5 inches of the subsoil is brown sandy clay loam, the next 13 inches is brown and light brown clay loam, and the lower 16 inches is reddish brown, light reddish brown, and pink sandy clay loam and clay loam. The substratum to a depth of 60 inches or more is light reddish brown and white loam. In places, these soils are effervescent to the surface. Many soft masses of lime are in the lower part of the subsoil and in the substratum. In some areas, the surface layer is gravelly sandy loam.</p> <p>Urban land consists of areas of soil so altered by construction or obscured by structures and pavement that identification of the soil is difficult or impossible. Most of the Urban land is located in the City of Tucson. In general, the underlying and interspersed soil material has many of the characteristics of the associated soils in the complex.</p>	Rapid in shallow rills and deep gullies, otherwise slow to moderate	Slight to moderate.
Tubac Gravelly Loam, 1 to 8% Slopes	This soil is associated with broad, gently sloping fan terraces shallowly dissected by ephemeral drainageways.	Typically the surface is covered by 25% gravel and 5% cobble. The surface layer is brown to dark brown gravelly loam about 2 inches thick. The subsurface layer is reddish brown and pinkish gray loam 12 inches thick. The upper 17 inches of the subsoil is reddish brown clay. The lower subsoil to a depth of 60 inches or more is reddish brown and brown gravelly sandy clay loam. In places, these soils are effervescent to the surface. Many soft masses of lime are in the lower part of the subsoil and in the substratum. In some areas, the surface layer is coarse sandy loam.	Medium	Slight

Source: Soil Survey of Tucson

2. Soil Suitability for Septic Use

If temporary on-site wastewater disposal is required as part of development, site specific soil testing will be conducted at the time of the development plan or request for building permit.



H. Viewsheds

The following descriptions of views and vistas are separated into two categories for clarity; the north block and the south block. The views beyond adjacent properties have similar views; therefore the description is combined into one category.

1. Views Onto and Across the Site

Various photos were taken of views from and across the project site. A photo key map indicates the locations from which the photos were taken (*see Exhibit I.H.1.a: Photo Key Map – North Block, Exhibit I.H.1.b: Photo Key Map – South Block and Exhibit I.H.2: Site Photos*).

North Block

The views onto and across the site consist mainly of moderate vegetation due to the flatness of the area. There is a graded roadway running north and south through the middle of the property. Valencia Road runs through the southwest corner and is highly visible from the southern portion of the site. The northern boundary has low visibility due to DMAFB restricted uses and setbacks adjacent to the site.

South Block

The views onto and across the site consist of moderate vegetation with the exception of the northern half, which has been graded. Valencia Road, the existing buildings (up to 40 feet in height) and storage yards to the east of the property are highly visible along the eastern property line. Grading has also occurred on the southern portion of the site due to abandoned railroad lines, which contains areas of high visibility.

a. Views and Vistas from Adjacent Properties

North Block

Northern views from adjacent properties: The views from Valencia Road to the north and south of the site are highly visible. Other views north from adjacent southern properties are minimal due to the moderate vegetation.

Eastern views from adjacent properties: Western properties adjacent to the site are vacant and within the existing South Kolb Road Specific Plan. The site is very similar to the western properties and the views are of moderate vegetation and highly visible from the property line. Interior visibility is moderate.

Western views from adjacent properties: The adjacent eastern property is vacant with the exception of abandoned railroad tracks. Views looking west from the eastern property line are highly visible but due to the moderate vegetation, the interior property has minimal visibility.



Southern views from adjacent properties: Northern properties adjacent to the site consist of an abandoned railroad track and an airplane storage yard within the restricted areas of DMAFB. Due to a rather large distance (approximately 1/2 mile) from the site, the views to the south from the DMAFB are minimal.

South Block

Northern views from adjacent properties: The Union Pacific Railroad is located south of the site and views to the north are visible due to grading on the site.

Eastern views from adjacent properties: Western properties adjacent to the site are vacant.

Western views from adjacent properties: The adjacent eastern property is vacant with the exception of the Goodwill Center and Hansen Pipe and Products. Their views are highly visible due to the grading of the site.

Southern views from adjacent properties: There are minimal views and vistas to the south of the site due to moderate vegetation.

b. Views and Vistas from Areas Beyond Adjacent Properties

The north block and the south block have similar views beyond adjacent properties; therefore the following description is combined into one category.

Northern Views: Southern properties south of the UPRR have distant views of the Santa Catalina Mountains to the north and the Rincon Mountains to the northeast. The site is minimally visible from Interstate 10 but moderately visible from south Kolb Road.

Eastern Views: Western commercial and industrial properties across Kolb Road have distant views of the Rincon Mountains to the east. Their vistas and views will be minimally impacted.

Southern Views: Northern properties have distant views of the Santa Rita Mountains. There are minimal views and vistas to the south of the site because of the restricted uses and setbacks to the site.

Western Views: The eastern commercial properties have distant views of the Tucson Mountains and the Sierrita Mountains to the west. Their views and vistas will be minimally impacted due to restricted uses.



2. Areas of High, Medium, and Low Visibility

The project site is comprised primarily of areas with moderate visibility. Due to the flat topography of the site it is highly visible from Valencia and Kolb from off-site locations. Areas that have been previously graded on the south block are also highly viable. Moderate vegetation creates a few areas within the interior of the parcels with moderate visibility (*See Exhibit I.G.3: Visibility*).

Areas of the subject property defined as having high, medium and low visibility as viewed from off-site were determined as follows:

Table I.H.2: Visibility

High Visibility	Medium Visibility	Low Visibility
Areas not obscured by vegetation or terrain and visible from the edge of the site.	Areas visible from off-site but obscured somewhat by vegetation.	Areas on-site that cannot be seen from off-site due to vegetation and/or topography.

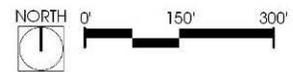


Exhibit I.H.1.a: Photo Key Map – North Block



LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  2' Contour
-  Photo ID and location photo was taken



FILE NAME: LLR-01_photokeymap_north.mxd
SOURCE: Pima County DOT GIS, 2007

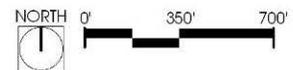


Exhibit I.H.1.b: Photo Key Map – South Block



LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  2' Contour
-  Photo ID and location photo was taken



FILE NAME: LLR-01_photokeymap_south.mxd
SOURCE: Pima County DOT GIS, 2007



Exhibit I.H.2: Site Photos



Photo 1: Looking northwest from southern portion of site (North Block).



Photo 2: Looking north from southern portion of site (North Block).



Photo 3: Looking east from central portion of site (North Block).



Photo 4: Looking northeast from central portion of site (North Block).



Photo 5: Looking southwest at parcel across Valencia Road (North Block).



Photo 6: Looking south from northern portion of site (South Block).



Exhibit II.H.2: Site Photos (continued)



Photo 7: Looking south from northern portion of site (South Block).

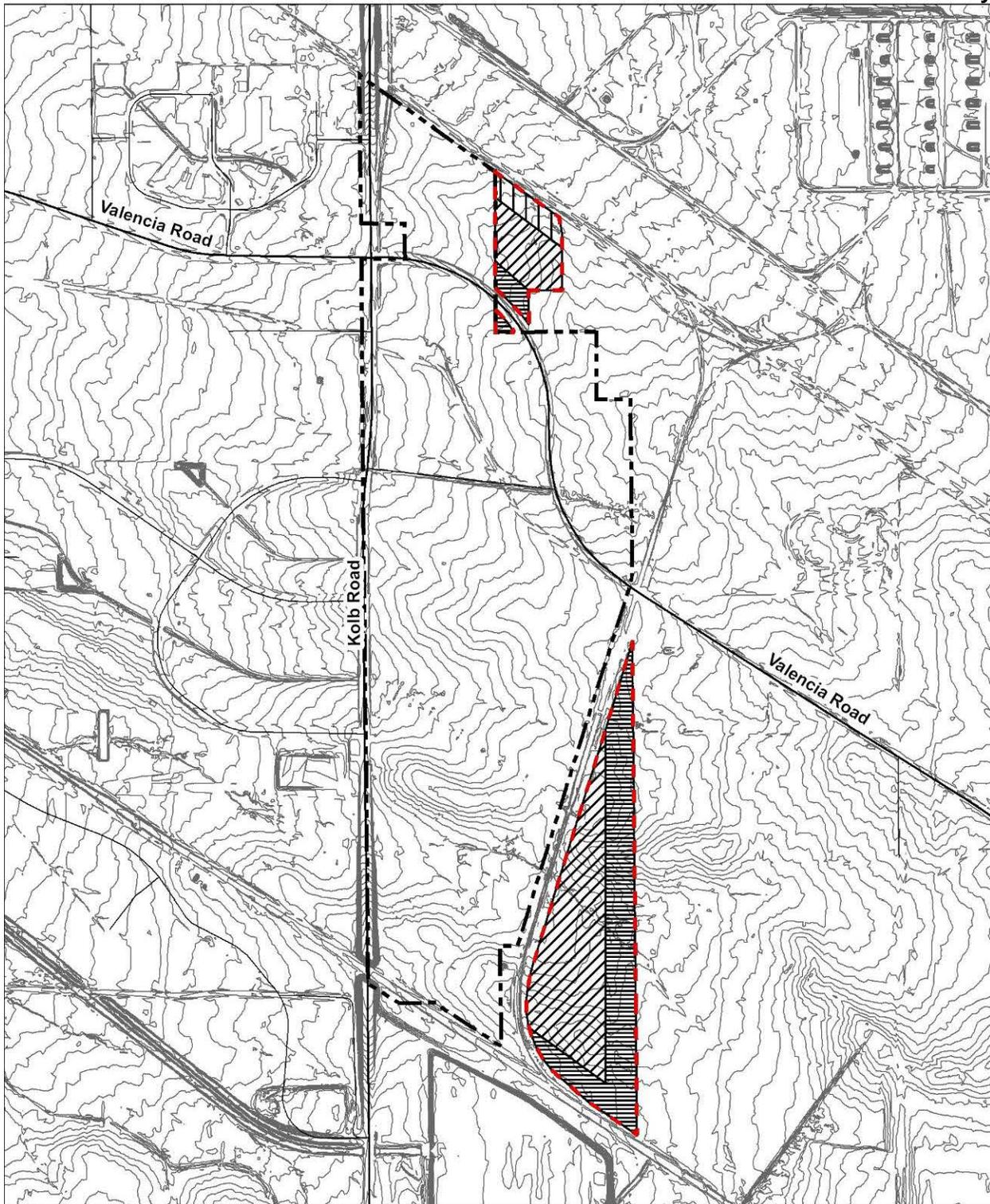


Photo 8: Looking northeast from northern portion of site (South Block).



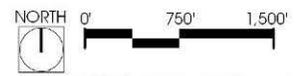
Photo 9: Looking north from northern portion of site (South Block).

Exhibit I.G.3: Visibility



LEGEND
 - - - - - South Kolb Specific Plan
 - - - - - Amendment Boundary
 ——— 2' Contour

Visibility
 [Diagonal Hatching] Low
 [Horizontal Hatching] Moderate
 [Vertical Hatching] High



FILE NAME: LLR-01_visibility.mxd
 SOURCE: Pima County DOT GIS, 2007



I. Traffic

1. Existing and Proposed Off-site Streets

The project site consists of two separate sites: a north block and a south block. The north block is located directly east of Kolb Road bisected by Valencia Road and the south block is located south of Valencia Road and slightly further east of Kolb Road directly north of the Union Pacific Railroad. Valencia Road and Kolb Road are the only major roadways within one mile of the sites. Both Valencia and Kolb Roads are scenic routes within the vicinity of the blocks. Other major routes and collector roadways within a three-mile radius include Interstate 10, Wilmot Road, Los Reales Road, Irvington Road, Escalante Road, Pantano Road, and Camino Seco (see *Exhibit I.I.1: Traffic*).

Road improvements for Valencia Road from Alvernon Way to Houghton Road include widening from a 2-lane to a 6-lane desert parkway with bike lanes and sidewalks. These improvements are scheduled in the late period of the Regional Transportation Plan. The late period is expected to be constructed after 2020. Kolb Road is planned for improvements to the existing parkway facility. This improvement is also scheduled for the late period. This information was gathered from Pima County Department of Transportation (DOT), Pima County DOT Geographical Information Services, 2030 Regional Transportation Plan, and Pima Association of Governments (PAG).

The following tables identify the rights-of-way information, roadway characteristics, and average daily trips as outlined in the Pima County Site Analysis Requirements Traffic section (a – g).

Table I.I.1: Existing Rights-of-Way

	a	b	c	d
Street	Existing Right-of- Way (feet)	Future Right-of-Way (feet)	Meets Right-of-Way Standards	Continuous Right-of-Way
Valencia Road	150	200	No	Yes
Kolb Road	150-200	150	Yes	Yes
Interstate 10	380	380	Yes	Yes
Wilmot Road	105-150	150	Yes	Yes
Irvington Road	150	150	Yes	Yes
Pantano Road	150	150	Yes	Yes
Camino Seco	150	150	Yes	Yes
Escalante Road	150	150	Yes	Yes



Table I.I.2: Street Characteristics

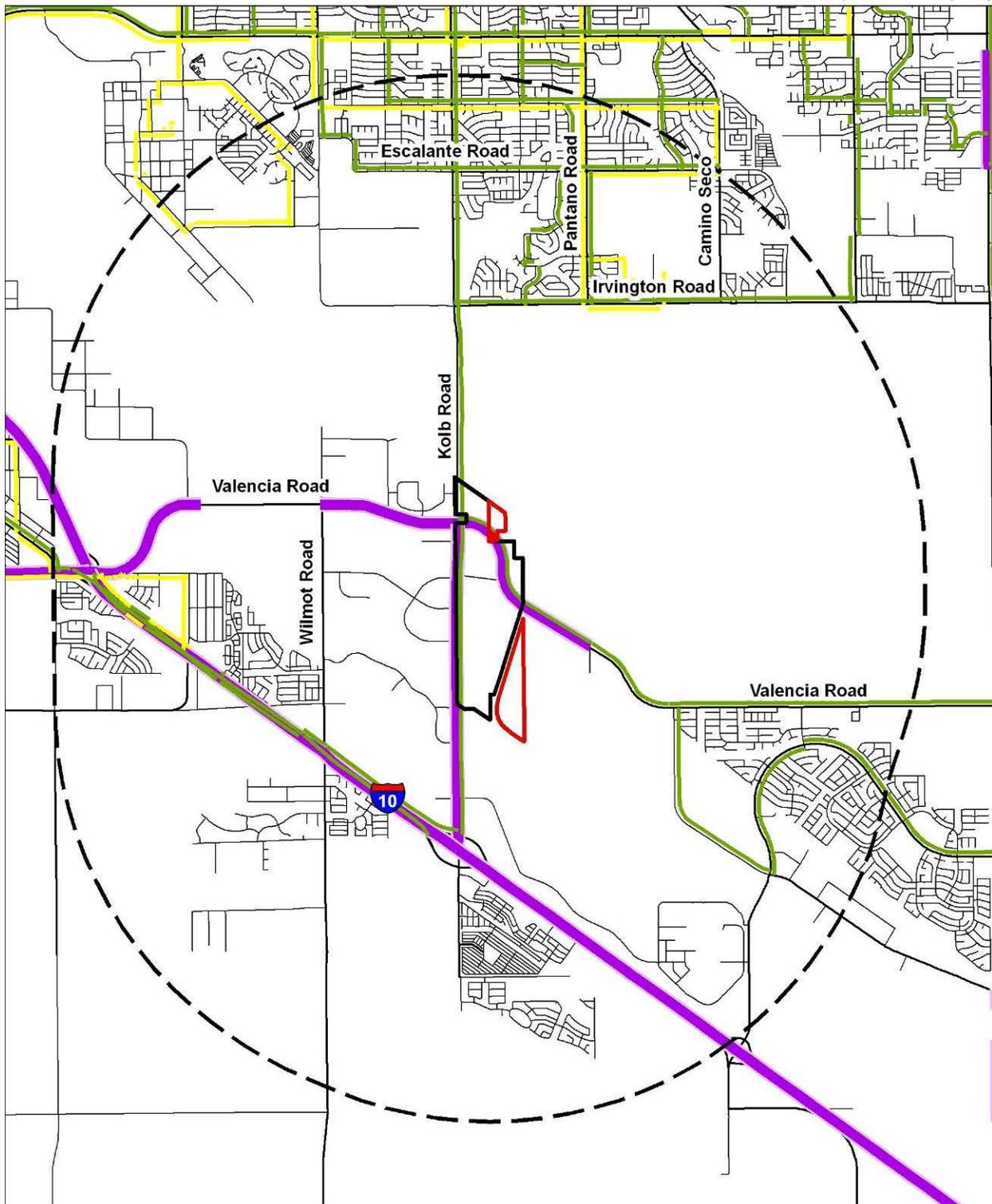
Street	Classification	Travel Lanes	Bus Route	Bike Route	Capacity at LOS E	Speed Limit	Paved
Valencia Road	Rural Principal Arterial	2	No	Yes	14,600	50	Yes
Kolb Road	Urban Principal Arterial	4	No	Yes	31,100	55	Yes
Interstate 10	Principal Arterial-Interstate	4	No	No	67,100	65/75	Yes
Wilmot Road	Major Collector-Rural	2	No	No	14,600	50	Yes
Irvington Road	Minor Arterial	2	Yes	Yes	14,600	45	Yes
Pantano Road	Minor Arterial	2	Yes	Yes	14,600	35	Yes
Camino Seco	Minor Arterial	2	Yes	No	14,600	35	Yes
Escalante Road	Minor Arterial	2	Yes	Yes	14,600	40	Yes

Table I.I.3: Average Daily Trips

Street	Segment	Average Daily Trips (Year Taken)
Valencia Road	I-10 to Wilmot	28,000 (2006)
	Wilmot to Kolb Road	32,400 (2006)
	Kolb Road to 1.4E of Kolb Road	19,1198 (2006)
Kolb Road	Valencia Road to Interstate 10	15,000 (2007)
	Valencia Road to Irvington	48,000 (2007)
	Irvington to Escalante Road	42,000 (2007)
Escalante Road	Kolb Road to Pantano Road	12,000 (2005)
Irvington Road	Kolb Road to Pantano Road	13,000 (2005)
Pantano Road	Irvington Road to Escalante Road	9,000 (2005)
Wilmot Road	Valencia Road to Interstate 10	4,000 (2006)

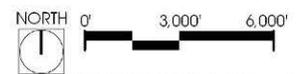


Exhibit I.I.1: Traffic



LEGEND

- South Kolb Specific Plan
- Amendment Boundary
- Three-Mile Radius
- Scenic Corridor Streets (PC)
- Bicycle Routes
- Sun Tran Routes



FILE NAME: LLR-01_traffic.mxd
SOURCE: Pima County DOT GIS, 2007



J. Sewers

1. Capacity Response Letter from Pima County Department of Wastewater Management

A Type II Capacity Response Letter has been provided (see *Exhibit I.J.1: Pima County Wastewater Letter*).

2. Locations of Existing Public Sewers in Relation of the Project Site

Locations of public sewers are provided (see *Exhibit I.J.2: Existing Public Sewers*).



Exhibit I.J.1: Pima County Wastewater Letter



**Pima County
Regional Wastewater Reclamation Department**

Michael Gritzuk, P.E.
Director

201 N. Stone Ave., 8th Floor
Tucson, Arizona 85701
(520) 740-6500

Visit our website:
<http://www.pima.gov/wwrm>

June 19, 2008

Pam Lopez
Psomas
800 E. Wetmore Road, #110
Tucson, AZ 85719

Capacity Response No. 08-092A Type II

**RE: CENTURY PARK RESEARCH CENTER PHASE II, BLOCKS 1 THRU 19 AND
COMMON AREA 'A', 'B' AND 'C'
P1208-035
Estimated Flow: 422, 700 gpd (ADWF)**

Greetings:

The above referenced project is tributary to the Roger Road Wastewater Treatment Plant via the Southeast Interceptor and the Northwest Outfall. Treatment and conveyance capacity in the downstream public sewer system for new development within the area served by these facilities is currently limited.

Additional conveyance capacity for new development in this service area will become available upon the completion of the construction of the proposed Mission View Wash Interceptor, and the Phase III extension of the Santa Cruz Interceptor. These projects are also currently funded, underway, and expected to be complete in or around December 2009.

Additional treatment capacity for new development in this service area will become available upon the completion of the plant interconnect between Roger Road Wastewater Treatment Plant and the Ina Road Water Pollution Control Facility. This project is currently funded, underway, and expected to be complete in or around December 2010.

This letter is not a reservation or commitment of treatment for conveyance capacity for this project. To reserve capacity in the downstream public sewer system a Sewer Service Agreement must be executed between the owner / developer and the County. The design and construction for the spine sewers for this project may proceed with the understanding that the owner(s) / developer(s) of this project will need to agree to special measures that must be taken to ensure that the latter phases of this project will not cause Pima County's public sewer system to fail to meet the performance standards of Arizona Administrative Code R18-9-E301(B), or cause any flow or effluent quality limits to be exceeded at the treatment facility.

For the purpose of ensuring that these goals are met, the Sewer Service Agreement for this block plat shall contain the following provisions:

- a) By signing this agreement, Applicant acknowledges that treatment and conveyance capacity in the downstream public sewer system for new development is currently limited, and that County may not be able to provide such capacity as rapidly as may be desired by Applicant via its normal capital improvement program.
- b) To ensure that the flows from the latter phases of this development will not cause County's public sewer system to fail to meet the performance standards of Arizona Administrative Code R18-9-E301(B), or cause any flow or effluent quality limits to be exceeded at the treatment facility, Applicant agrees to obtain an updated capacity



Exhibit I.J.1: Pima County Wastewater Letter cont.

Capacity Response No. 08-092A Type II

June 19, 2008

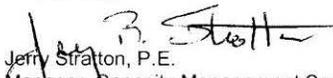
Page 2

- determination letter from County prior to the preparation of any building site plan, tentative plat, or development plan for each of the proposed blocks.
- c) County shall identify all anticipated shortages of capacity within its updated capacity determination letters for the development of the individual blocks, but shall not unreasonably withhold capacity for new development within this project.
 - d) Should capacity for the development of a block be desired before Pima County can provide the necessary capacity through its normal capital improvement program, Applicant and County agree to negotiate a mutually acceptable method for addressing the identified capacity issues, such as allowable build-out schedules, etc. the use of temporary on-site (septic) systems, or other means.
 - e) The resulting negotiated agreements shall be documented within a separate Sewer Agreement that covers just that block prior to approval of the building plan, tentative plat, or development plan for each of the proposed blocks.

Note: This letter does not address PDEQ's capacity assurance requirements. An update to this letter must be obtained before PDEQ will issue the Construction Authorization necessary to build the proposed sewers. This update should be obtained just prior to submitting the construction plans to PDEQ for review and approval.

If further information is needed, please feel free to contact us at (520) 740-6500.

Respectfully,


Jerry Stratton, P.E.
Manager, Capacity Management Section

JS:TR:tr

c: Subhash Raval, DSD; 15-15-17 & 20



Exhibit I.J.2: Existing Public Sewer



LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  Manhole Covers
-  Sewer Network with Pipe Diameter and As-Built Identification Number



FILE NAME: LLR-01_sewer.mxd
SOURCE: Pima County DOT GIS, 2007



K. Schools

1. Public Schools within One Mile of the Site

The Amendment area is located in the Vail Unified School District. There are no existing or proposed schools within one mile of the Amendment area.

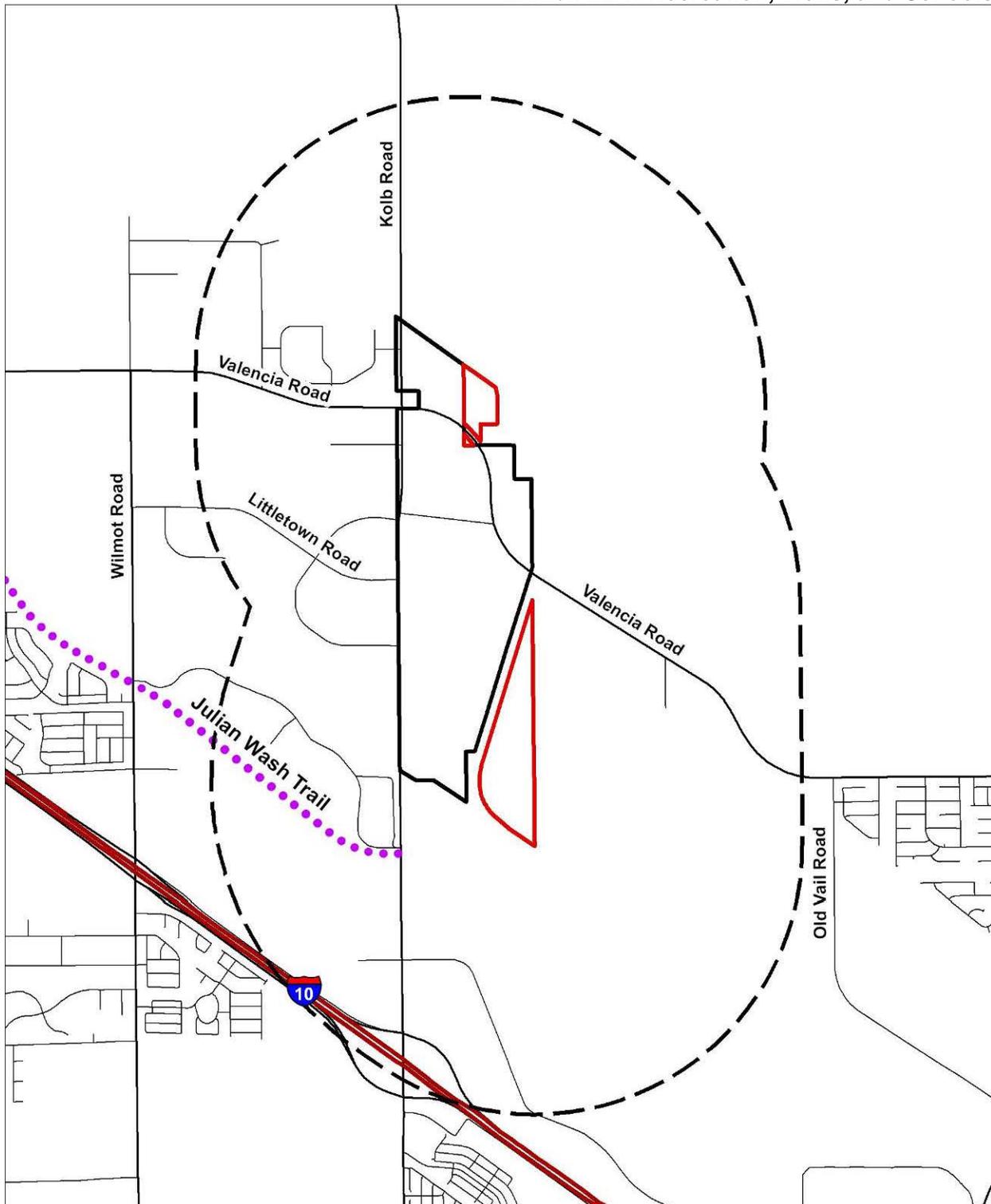
L. Recreation and Trails

1. Parks, Recreation Areas, Public Trails within One Mile

There are no parks, recreation areas or adopted trails within a one-mile radius of the project site. Thomas Jay Regional Park and Littleton Recreation Center is outside the one-mile radius, directly west of the project site at 6465 S. Craycroft Road. The center offers ADA accessible facilities, a lighted baseball field and basketball court, a concession building, volleyball courts, a t-ball field, a lighted softball field, picnic areas, ramadas, playground equipment, restrooms, drinking water and a community center. The Julian Wash Trail (Local Trail #319) is the only trail near the vicinity of the one-mile radius, which is considered a third-priority candidate trail according to the Eastern Pima County Trail System Master Plan. Fred Enke Golf Course is also outside the vicinity of the one-mile radius and is one of five municipal golf courses operated by Tucson City Golf. The course is located at 8251 E. Irvington Road, just northeast of the project site, and offers public golf, banquet facilities, a driving range, a full service clubhouse and practice areas (see *Exhibit I.L.1: Recreation, Trails and Schools*).



Exhibit I.L.1: Recreation, Trails, and Schools

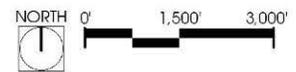


LEGEND

-  South Kolb Specific Plan
-  Amendment Boundary
-  One-Mile Radius
-  Trails

Notes:

- There are no schools or parks within 1 mile of site.
- The project site is served by Vail Unified School District.



FILE NAME: LLR-01_schools_recreation.mxd
SOURCE: Pima County DOT GIS, 2007



M. Cultural Resources: Archaeological and Historic Sites

1. Arizona State Museum Letter

A letter from the Arizona State Museum Pima County Archaeological Records is provided (*see Exhibit I.M.1: Arizona State Museum Letter*).

2. Map and Description of Archaeological or Historic Sites

North Block

WestLand Resources, Inc., conducted an archaeological survey of 137 acres of land near the intersection of Kolb Road and Valencia Road in Pima County, Arizona, on December 14 and 15, 2006, for Kolb Road Development Limited Partnership. This land includes the north block for the Amendment.

South Block

WestLand Resources, Inc., also conducted an archaeological survey of 300 acres of land near the intersection of Kolb Road and Valencia Road in Pima County, Arizona, on September 12, 13, 14, 2006, for Levin and Sons. This land includes the south block for the Amendment.

The reports from both surveys have been submitted under a separate cover.

3. Field Survey Requirements and Results

North Block

The archaeological survey of 137 acres, which includes the north block of the Amendment, identified one previously unrecorded archaeological site, AZ BB:13:799(ASM), and five isolated occurrences of archaeological remains were located during the survey. In addition, an unrecorded segment of AZ BB:13:725(ASM)/Old Vail Road crossed the project area; however, all identified archaeological sites are not within the north block of the Amendment. The full survey results, which identify the location of all recorded sites, have been documented in the submitted report.

South Block

The 300-acre archaeological survey conducted by Westland Resources, Inc., which included the south block of the current Amendment request, identified 15 isolated occurrences, two newly identified sites, AZ BB:13:792(ASM) and AZ BB:13:793(ASM), a previously unrecorded section of historic Old Vail Road, AZ BB:13:725(ASM), and previously recorded EPNG Line 1007, AZ AA:12:875(ASM). Westland recommends that sites AZ BB:13:792(ASM), AZ BB:13:793(ASM), and the portion of AZ BB:13:725(ASM) be ineligible for listing in the National Register. Historic property AZ AA:12:875(ASM)/EPNG 1007 Line – which runs through the “South Block” defined in the Amendment – is exempt from Section 106 Historic Preservation Review.



Exhibit I.M.1: Arizona State Museum Letter



Arizona State Museum

RECEIVED

P.O. Box 210026
Tucson, AZ 85721-0026
Tel: (520) 621-6302
Fax: (520) 621-2976

PIMA COUNTY ARCHAEOLOGICAL RECORDS SEARCH REQUEST

Letter Request Received: 6/12/2008

Search Completed: 6/18/2008

Requester Name and Title: Steven Eddy, Planner
Company: The Planning Center
Address: 110 South Church, Suite 6320
City, State, Zip Code: Tucson 85701
Phone/Fax/or E-mail: (520) 623-6146

Project Name and/or Number LLR-01 / Parcels14106-005B, -012B&C, -013B&C, -0090, -0020
Project Description Specific Plan for 83.4 commercial acres

Project Location: 5901 through 7011 S. Kolb Road and others, Pima County, Arizona.

Legal Description: A portion of the W½, S8, and the W½, S17, & the NE, NW, NW, S20, T15S, R15E, G&SR B&M, Pima County, Arizona.

Search Results: A search of the archaeological site records retained at the Arizona State Museum (ASM) identified no cultural resources within the subject project area. This area has never been completely inspected archaeologically, however. Two small block surveys were completed in the project area in 1998 and 2001. Twenty-eight additional archaeological inspections have been conducted within a mile of the project area between 1955 and 2005. One cultural resource, a segment of the El Paso Natural Gas pipeline, is recorded in the project area. A 2005 color orthophotograph taken of the proposed commercial acres (enclosed) shows native vegetation growing on a relatively unmodified ground surface.

Sites in Project Area: None recorded; the project area, however, has never been completely inspected.

Recommendations: Because Pima County has jurisdiction in this parcel, the county's cultural resources staff bases its recommendations for additional archaeological inquiry on its own data sources and it may include the ASM's search results and/or others. Should the county require further archaeological investigation in the project area, you may wish to consult a professional archaeological contractor. A list of qualified contractors is posted on the ASM website at the following address:
<http://www.statemuseum.arizona.edu/crservices/permits/index.shtml>.

Pursuant to *Arizona Revised Statutes* § 41-865, if any human remains or funerary objects are discovered during the project work, all effort will stop within the area of the remains and Mr. John Madsen, the ASM associate curator of archaeology, will be contacted immediately at (520) 621-4795.

If you have any questions regarding the results of this records search, please contact me at the letterhead address or at the phone number or email address as follows.

Sincerely,

Nancy E. Pearson
Assistant Permits Administrator
(520) 621-2096 Phone / FAX
nepearso@email.arizona.edu



N. Air Quality

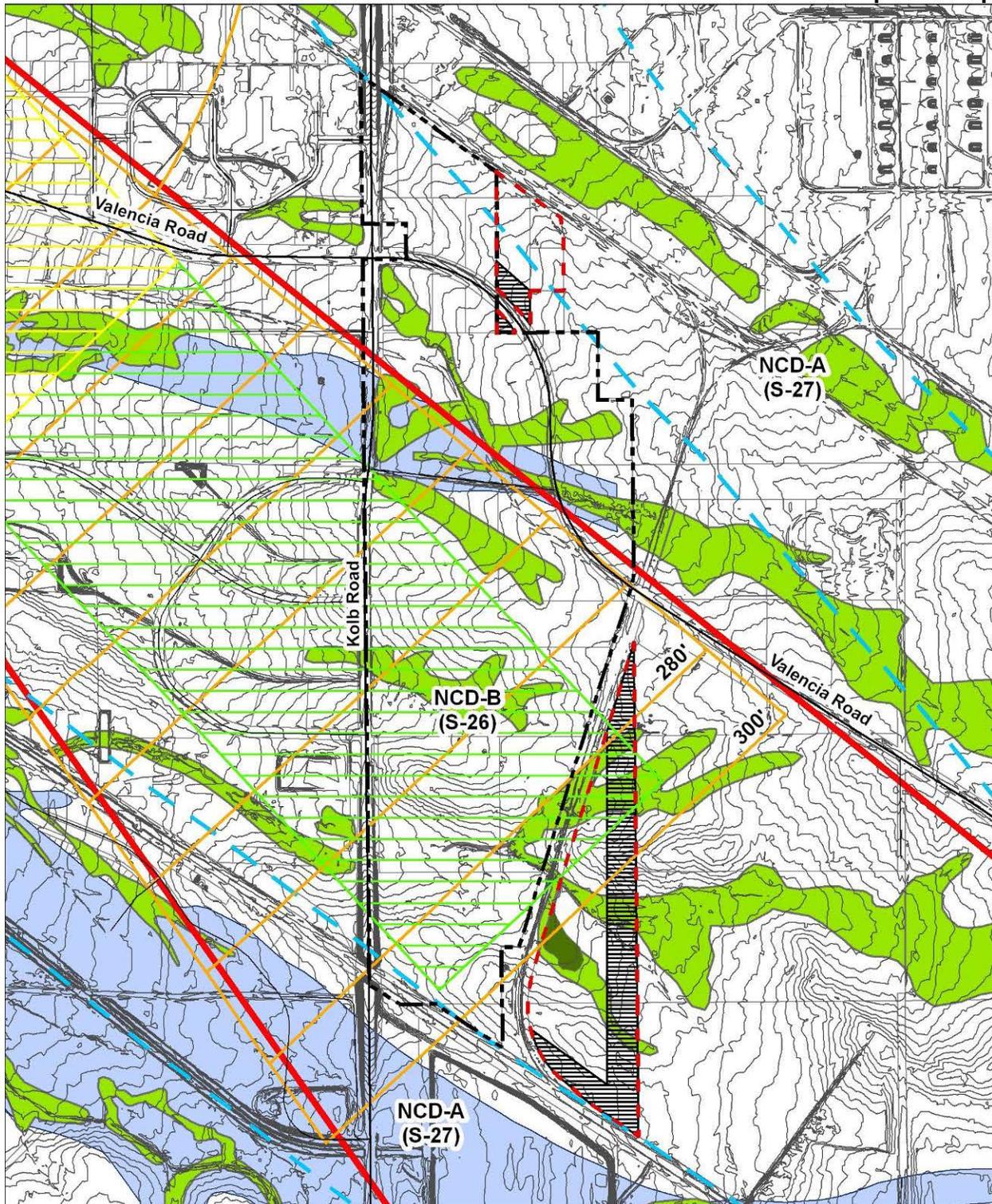
According to the existing South Kolb Road Specific Plan, Pima County Health Department has no air quality monitoring systems in or near the study area. Because Tucson has prevailing southeast winds it is assumed the air quality at Kolb and Valencia Roads would be excellent. Carbon monoxide levels at the intersection would be higher at rush hour, but should those levels be tested 100 feet from the intersection, they would most likely register at almost non-existent. Proposed site users will be clean industries, and will not emit pollutants into the atmosphere; hence, the proposed users do not require an air quality review.

O. Composite Map

The Composite Map graphically illustrates the summation of constraints and opportunities identified in the Site Inventory Section addressing the developed and natural features of the site. The identified constraints are existing on-site hydrology, xeroriparian habitat, vegetation density, on-site visibility and DMAFB Compatible Use Zones.

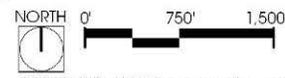


Exhibit I.O.1: Composite Map



LEGEND

 South Kolb Specific Plan	 Accident Potential Zone 2	 High Density Vegetation
 Amendment Boundary	 Airport Height Overlay Zone	 Xeroriparian Habitat C
 2' Contour	 Noise Control Districts	 FEMA Zone A
 DMAFB ADC-2	 High Visibility	

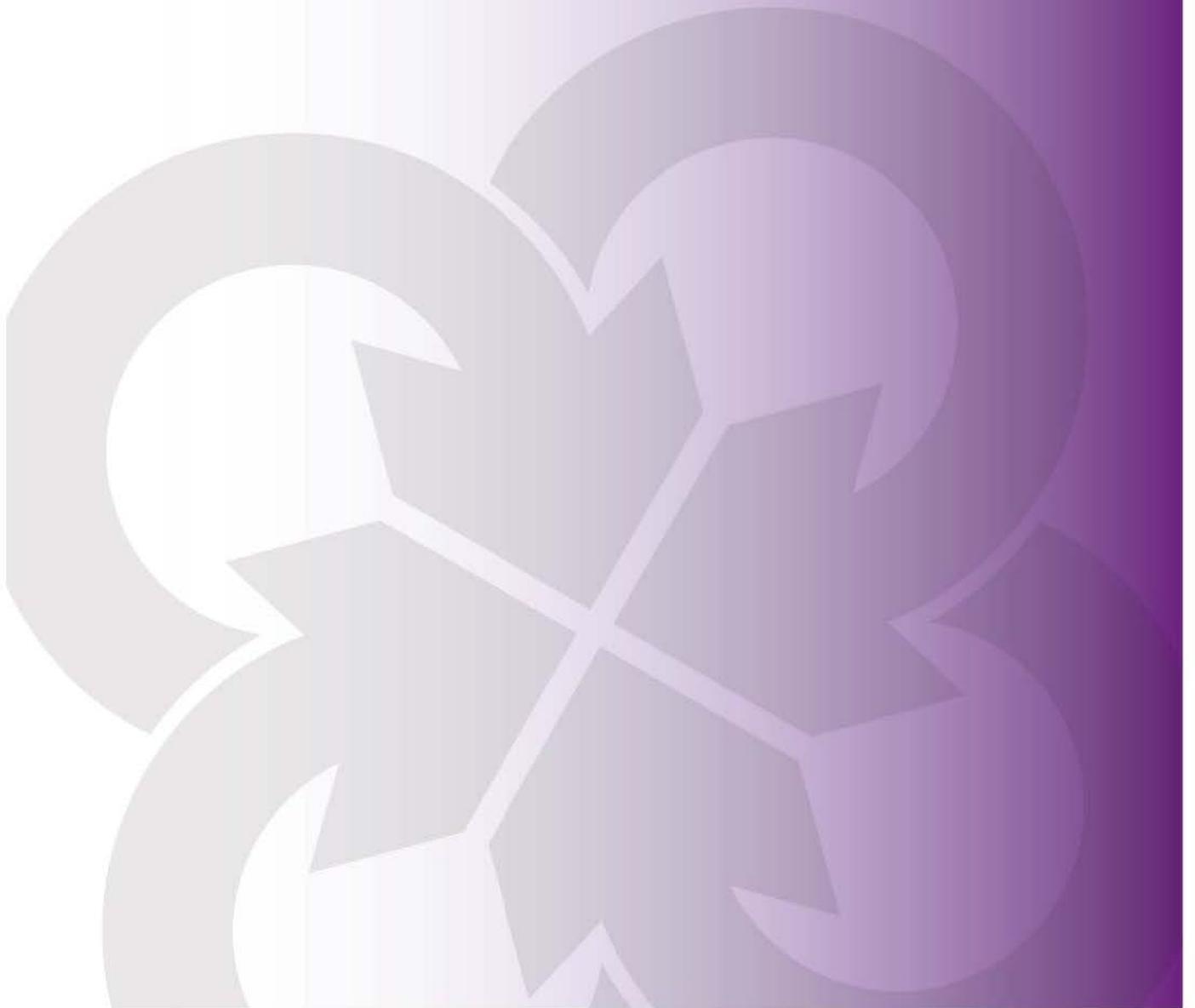


FILE NAME: LLR-01_composite.mxd
SOURCE: Pima County DOT GIS, 2007



Land Use Proposal divider page

Part II Land Use Proposal



A. Land Use Proposal

1. Overview

The South Kolb Road Specific Plan Amendment proposes industrial and commercial uses, which account for on-site and off-site constraints and opportunities including DMAFB Compatible Use Zones, proximity to Union Pacific rail lines and Interstate 10, xeroriparian habitat mitigation, and on-site and off-site hydrology.

On October 3, 2006, the Pima County Board of Supervisors approved a Modification (non-substantial change) of Rezoning Condition for the South Kolb Road Specific Plan to eliminate the distinction between “Primary Uses” and “Secondary Uses” as set forth in the South Kolb Road Specific Plan for Planning Areas A, C, D, E, F, G, and H for a total area of approximately 336 acres. The approval is subject to conditions that must be met by the owner/developer.

Industrial uses are appropriate for the area given the proximity to transportation facilities including the interstate, Union Pacific Railroad and Tucson International Airport. There have been a number of ongoing, recent and planned industrial parks in the area (including Southpointe, Century Park and the Port of Tucson, University of Arizona Science and Technology Park, Rita Tech Park and the Target Fulfillment Center, and Rita Commerce Park) that support the viability of industrial uses in the area.

2. Planning Areas

The existing South Kolb Road Specific Plan is divided into eight planning areas generally bounded by street rights-of-way or aircraft approach compatible use zones. The Amendment, which consists of two blocks (north block and south block), is to be integrated into the existing planning areas or separated into new planning areas based on those same boundary parameters.

The north block, which is primarily north of Valencia Road, is to be integrated into Planning Area A. A small portion of the north block is bisected by Valencia Road and is to be integrated into Planning Area C. The south block, which is south of Valencia Road and east of the existing Specific Plan, is to be a separate Planning Area (Planning Area I) because the abandoned DMAFB rail line separates it from the existing Specific Plan (*see Exhibit II.A.2: Planning Areas*).

A Land Use Summary Table (*see Table II.A.2*) has been provided which lists the existing Specific Plan planning areas with the Amendment areas and their new acreages and land uses.



Table II.A.2: South Kolb Road Specific Plan Amendment Land Use Summary

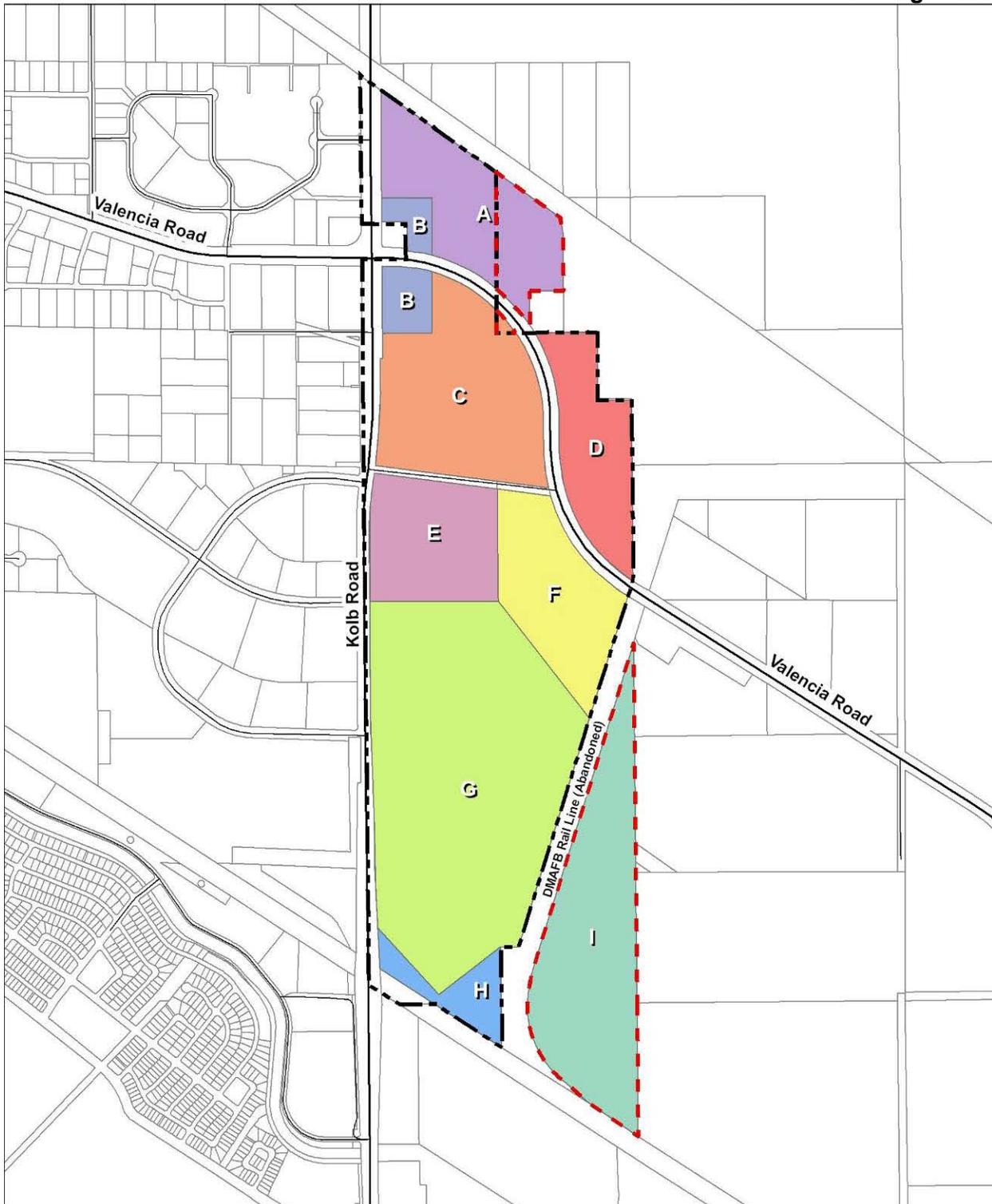
Planning Area	Land Use	Original Acreage	Amendment Acreage	Revised Acreage
A	Industrial (CI-1), Commercial (CB-1 & CB-2)	24.5	15.3	40.3
B	Industrial (CI-1), Commercial (CB-1 & CB-2)	12.2	-	-
C	Industrial (CI-1), Commercial (CB-1 & CB-2)	59.5	0.5	60.0
D	Industrial (CI-1), Commercial (CB-1 & CB-2)	31.0	-	-
E	Industrial, Research and Development/Office (CI-1 & CI-2)	34.4	-	-
F	Industrial, Research and Development/Office (CI-1 & CI-2)	36.2	-	-
G	Industrial (CI-1, CI-2)	137.7	-	-
H	Industrial (CI-1), Commercial (CB-1 & CB-2)	12.7	-	-
I	Industrial (CI-1, CI-2)	-	67.6	67.6
	TOTAL	348.2	83.4	431.6

3. Land Use Concept Plan

As stated earlier, The South Kolb Road Specific Plan Amendment proposes industrial and commercial uses, which account for on-site and off-site constraints and opportunities. A Land Use Concept Plan has been provided to show proposed land uses, preliminary circulation and proposed access points for the existing Specific Plan and the Amendment. The South Kolb Road Specific Plan Amendment proposes commercial and industrial uses for Amendment Planning Areas A & C and industrial uses for Amendment Planning Area I. The northern portion of the existing South Kolb Road Specific Plan, including the north block of the Amendment is proposed as a regional shopping center; however other commercial and industrial uses could be proposed for this site. Preliminary access and circulation for the southern portion of the existing Specific Plan and Amendment include two access points at Littletown Road and Century Park Drive and an interior roadway to provide access within the site and access to Planning Area I. The railroad use in the southern portion of the existing Specific Plan and Planning Area I of the Amendment is an extension of the Port of Tucson using the switching agreements currently in place. This project will provide locations for users to have direct access to the rail line from sidings at warehouses, manufacturing facilities and commodity distribution centers.



Exhibit II.A.2: Planning Areas



LEGEND

- South Kolb Specific Plan
- Amendment Boundary

Planning Areas

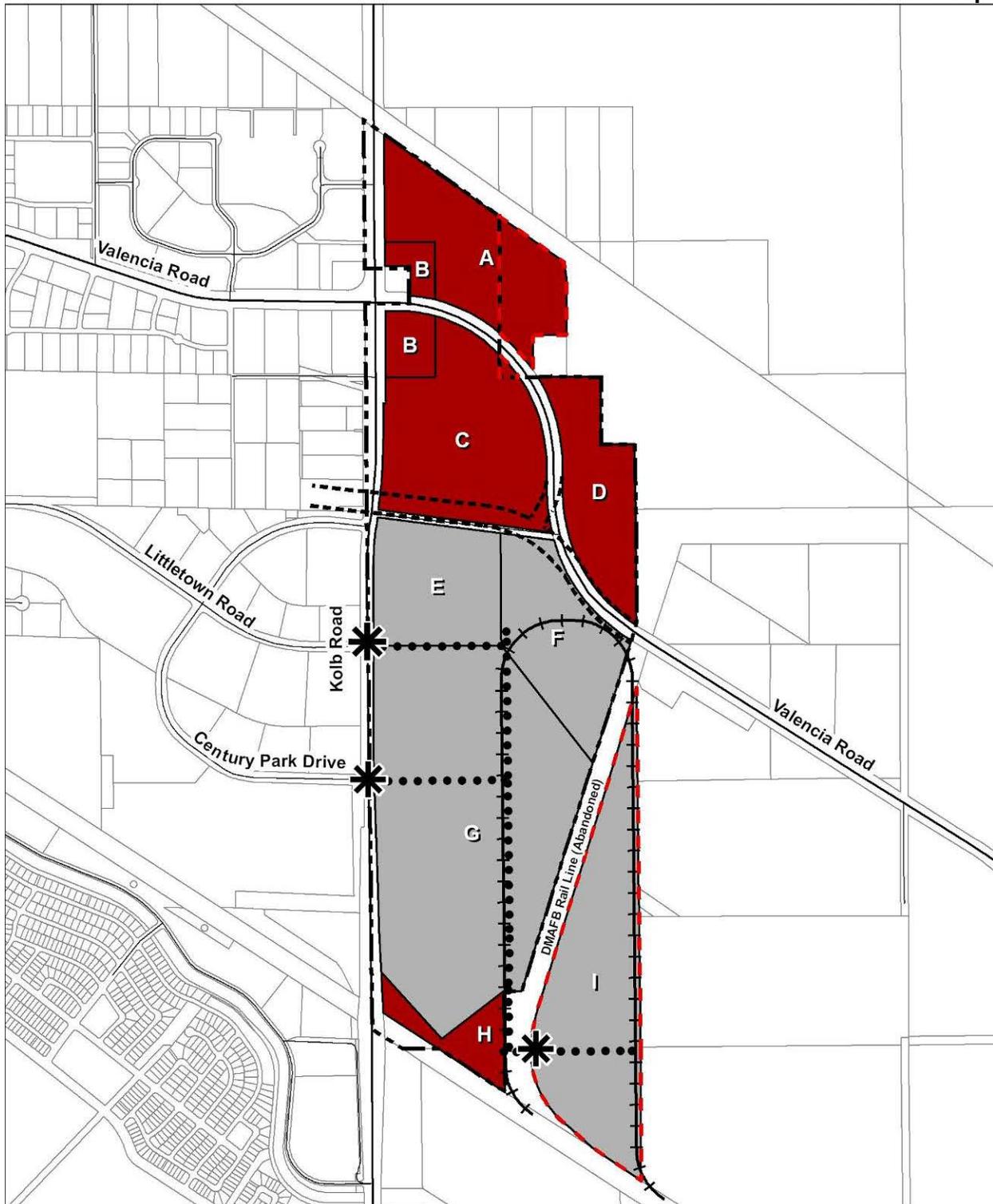
- | | | |
|---|---|---|
| A | E | I |
| B | F | |
| C | G | |
| D | H | |



FILE NAME: LLR-01_planning_areas.mxd
 SOURCE: Pima County DOT GIS, 2007

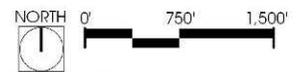


Exhibit II.A.3: Land Use Concept



LEGEND

- | | | | |
|--|--------------------------|--|---------------------------------------|
| | South Kolb Specific Plan | | Preliminary Access Point |
| | Amendment Boundary | | Rail Spur |
| | Industrial/Commercial | | Preliminary Circulation Plan |
| | Industrial | | Preliminary Valencia Road Realignment |



FILE NAME: LLR-01_land_use_concept.mxd
SOURCE: Pima County DOT GIS, 2007



B. Comprehensive Plan Compliance

The project site is designated Military Airport by the Pima County Comprehensive Plan. Military Airport is defined as a planned land use designation in which the purpose is to recognize DMAFB as “a unique and significant factor in shaping the history, character, and economy of Eastern Pima County; provide guidance for future compatible land uses to promote the health, safety and welfare of the community; and to promote the long-term viability of the base and its missions.” The Comprehensive Plan specifies that the CB-1 (Local Business), CB-2 (General Business), CPI (Campus Park Industrial), CI-1 (Light Industrial/Warehousing), and CI-2 (General Industrial) zones comply with the Military Airport planned land use designation. This designation would not allow new residential development. These compatible land uses for the Military Airport designation do not make a Comprehensive Plan amendment or any other modification to the Plan necessary for the proposed Amendment.

The proposed Amendment is intended to comply with and meet several Regional Plan Policies outlined within the Pima County Comprehensive Plan. A few of these policies include the following:

- The proposed Amendment is anticipated to incorporate a multimodal function by utilizing a rail spur off the Union Pacific rail line for potential rail commerce. This concept supports Land Use Element Regional Plan Policy C.1.c: Scale of Development.
- The proposed Amendment is intended to be a high-intensity, mixed use commercial and industrial development situated near three major transportation corridors: Interstate 10, Valencia Road and Kolb Road. The Amendment is also located within the Airport Growth Area designated by Pima County. These attributes support Circulation Element Regional Plan Policy F: Density and Use.
- As stated above, the proposed Amendment is intended to be a mixed use commercial and industrial development located within the Airport Growth Area. RTA transit improvements are anticipated to bring Sun Tran bus service to the Kolb/Valencia area. These attributes support Growth Area Element Regional Plan Policy C.
- The proposed Amendment is outside CLS designation, which supports Environmental Element Regional Plan Policy B.1.
- The proposed Amendment will be in compliance with Chapter 18.57 of the Pima County Zoning Code and all proposed land uses will be in compliance with the DMAFB JLUS as outlined in Military Airport Regional Plan Policies.

C. Existing Land Uses

1. Effect of Proposed Development on Existing Land Uses

The Amendment area is currently vacant; therefore, no impacts are anticipated from proposed development.



2. Effect of Proposed Development on Surrounding Land Uses

As stated in Section II.1.A, the proposed Amendment and its designated land uses are compatible with existing and planned industrial developments in the vicinity. The Amendment's proximity to the interstate, Union Pacific Railroad and Tucson International Airport make this a viable site for international and national commerce exchange and economic development opportunities.

D. Topography

1. Development on 15% or Greater Slopes

There are no slopes greater than 15% within the Amendment area.

2. Natural Areas under the Hillside Development Zone

There are no natural areas under the Hillside Development Zone within the Amendment area.

3. Percent of Site to be Graded

Grading Standards of Chapter 18.81 apply to this site. It is expected that 100% of the Amendment Area will be graded.

4. Change in Natural Grade more than 5 Feet

It is not anticipated that there will be more than 5 feet of change in natural grade. The range of cuts and fills is anticipated to be two to four feet in elevation. A grading plan will be submitted as part of the development plan process.

5. Engineering and Design Features

The use of retaining walls as a grading measure is not anticipated for areas within the Amendment. No other engineering or design features are anticipated.

E. Hydrology

A drainage report was produced by Baker and Associates Engineering Inc. for the northern portion of the existing South Kolb Road Specific Plan, which includes the north block of the Amendment area. A drainage report was produced by PSOMAS for the southern portion of the existing South Kolb Road Specific Plan, which includes the south block of the Amendment area. Information from both reports has been condensed to address the Amendment area and is included in this section. Both reports, including full size exhibits, have been submitted to Pima County Regional Flood Control District as part of the tentative plat/development plan.



1. Post-Development Hydrology

North Block

Under developed conditions, the off-site runoff (Drainage Areas A & C) will be collected at the eastern project boundary in proposed future channels that will be designed to convey completely contained flow in a west/northwest direction across the site to the west boundary. Drainage Area C channel improvements would be sized to convey the entire 100 year off-site flow and remove the floodplain from Planning Area A. Future channel improvements to convey flows from off-site Drainage Area A would likely occur as a part of Planning Area “D” development coupled with improvements for the eventual realignment of Valencia Road (south of Planning Area C); however, this area remains outside of the Amendment area. The only regulatory flow effecting the site is the FEMA delineated “A” Zone Special Flood Hazard Area that encroaches upon the south portion of Planning Area C. It is anticipated that once the aforementioned channel improvements are designed, a CLOMR will be submitted to remove the block encroachment areas from the FEMA floodplain.

Conceptual detention/retention basins are included in Exhibit II.E.1.a: Post-Development Hydrology – North Block for the purpose of illustrating their feasibility. Currently, there are no basins within the north block of the Amendment.

Although the developed conditions analyses are conceptual in nature, they indicate that the site can be designed to safely convey all flows. No adverse effects to the project or adjacent properties are foreseen as a consequence of any future development associated with the Amendment.

South Block

Hydrologic and Hydraulic analyses have been performed for the south block of the Amendment and the existing South Kolb Road Specific Plan. Hydraulic structures were sized and located to convey flow safely and effectively through the project area. Flow/Discharge either will be collected and transported through various channels or allowed to flow naturally overland to the natural washes onsite. Erosion protection will be provided at culvert and channel outlets.

On-site detention/retention through the use of private culverts and weirs to public culverts as outlet flow restrictors are proposed to lessen the amount of stormwater leaving the site to/below existing conditions. Additional traditional basins with weir outlets will be provided as necessary to supplement the online detention/retention. All proposed drainage improvements are shown in Exhibit II.E.1.b: Post-Development Hydrology – South Block. All post development measures for the Amendment and existing South Kolb Road Specific Plan are discussed in the PSOMAS drainage report.



Exhibit II.E.1.a: Post-Development Hydrology – North Block

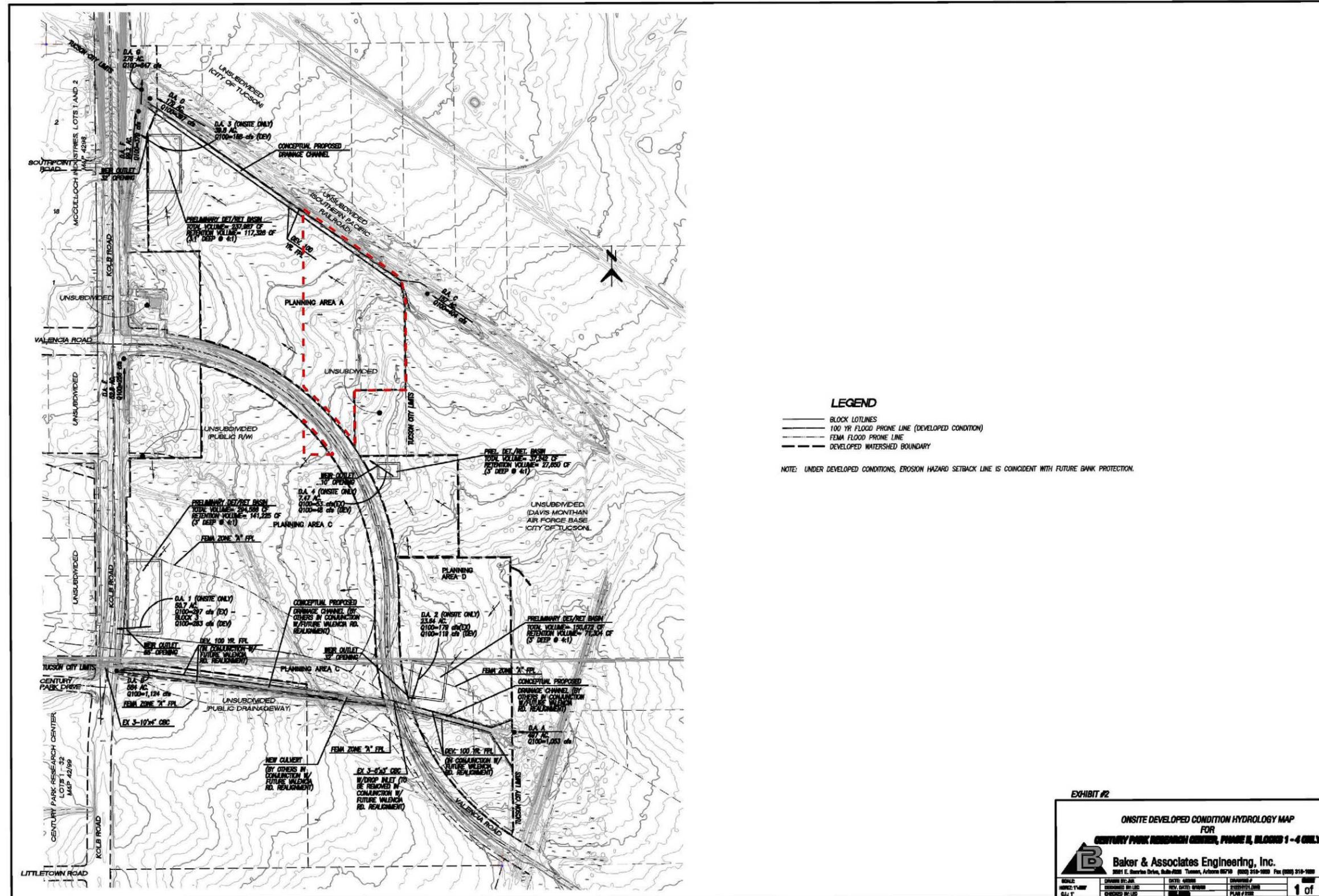


Exhibit II.E.1.b: Post-Development Hydrology – South Block

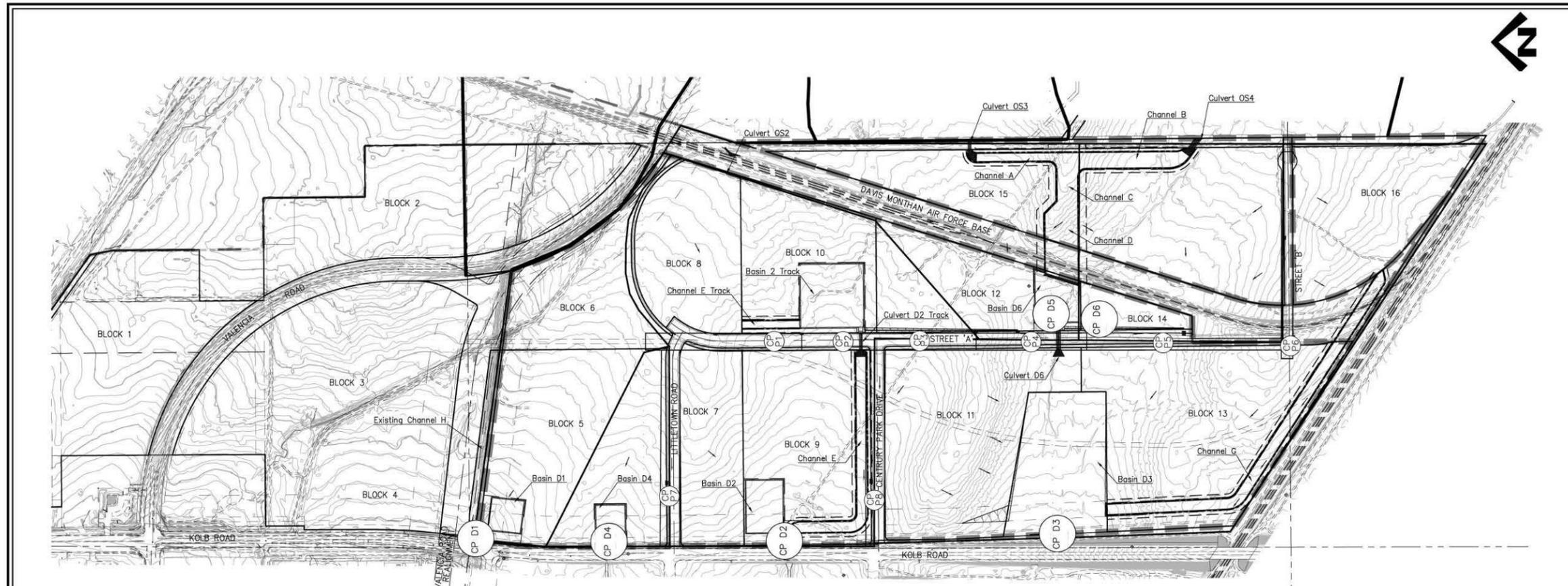


Table 3: Summary of Hydrologic Erosion Hazard Setback

Channel CP	Q ₅₀ (cfs)	EHS (ft)
CP D1*	212	25
CP D2	402	25
CP D4	141	25
CP D4*	98	N/A
CP D5*	32	N/A
CP D6*	170	30

*Includes offsite watershed contributions. †The OS1 shown in Table 1 associated with CP D5 is already included in the channel from Baker Engineering and therefore just the watershed was mapped.

Table 4a: Summary of Hydrologic Analysis (Developed Conditions Before Detention)

Concentration Point	Area (sq ft)	Length (ft)	Mean Slope (%)	Weighted Basin Factor (WBF)	Tc (min)	Watershed Peak Discharge (cfs)	Contributing Watersheds Concentration	Quantum
D1	34	2004	0.92	0.02	6.1	292	D1, OS1*	1347
D2	02	4062	0.99	0.02	8.6	595	---	595
D3	08	9706	0.18	0.02	16	909	D3, D5, D6, OS2, OS3, OS4	2423
D4	14	1128	1.28	0.02	5	128	---	128
D5	42	3453	0.52	0.02	9.7	300	D4, OS2, OS3	660
D6	39	3149	0.64	0.02	8.3	292	D6, OS4	1184
P1	2.3	1012	8.1	0.022	9.4	20	---	20
P2	1	287	8.1	0.022	5	10	---	10
P3	1.9	742	8.1	0.022	7.6	16	---	16
P4	1.8	715	8.1	0.022	7.4	16	---	16
P5	2.7	1261	8.1	0.022	11.1	30	---	30
P6	2.4	1136	8.1	0.022	5.4	24	---	24
P7	2.6	1156	8.5	0.022	5.3	26	---	26
P8	3.5	1365	8.5	0.022	5.2	36	---	36

D=Developed Conditions Watersheds P=Pavement Watersheds
*Per Baker Engineering Century Park Blocks 1-6 Drainage Report

Table 4b: Comparison of Existing to Developed Conditions

Existing Conditions Concentration Point	Developed Conditions Concentration Point
E1	D1
E2	D2
E3	Included in D3
E4	D4
E5	Included in D1
E6	D3, D5 and D6

Table 5: Summary of Proposed Inlet Design

Concentration Point	Inlet	Q ₅₀ (cfs)	Structure Type	Opening Height (ft)	Effective Opening Length (ft)
CP P1	P1-A	10	Scupper	0.5	20
	P1-E	10	Scupper	0.5	20
CP P2	P2	10	Scupper	0.5	20
	P2-A	10	Scupper	0.5	20
CP P3	P3-E	10	Scupper	0.5	20
	P3-A	10	Scupper	0.5	20
CP P4	P4-A	10	Scupper	0.5	20
	P4-E	10	Scupper	0.5	20
CP P5	P5-A	10	Scupper	0.5	20
	P5-E	10	Scupper	0.5	20
CP P6	P6-A	10	Scupper	0.5	20
	P6-E	10	Scupper	0.5	20
CP P7	P7-A	35	Scupper	0.5	20
	P7-E	3	Scupper	0.5	8
CP P8	P8-A	35	Scupper	0.5	20
	P8-E	3	Scupper	0.5	8

Table 6: Summary of Proposed Channel Design

Channel Number	Q ₅₀ (cfs)	Manning's n	Total Depth (ft)	Flow Velocity (ft/s)	Slope (%)	Side Slope (H:V)	Bottom Width (ft)	Channel Top Width (ft)
CHANNEL A	287	0.05	2.2	4.82	0.5	3:1	35	46.2
CHANNEL B	806	0.05	2.1	4.89	0.5	3:1	100	131.4
CHANNEL C	1240	0.05	2.2	5.09	0.5	3:1	130	141
CHANNEL D	1844	0.05	2.1	4.96	0.5	3:1	200	220.4
CHANNEL E (Track)	323	0.05	1.8	4.32	0.5	3:1	45	54.1
CHANNELS E	462	0.05	1.8	4.4	0.5	3:1	65	74.1
CHANNEL G	259	0.05	1.2	3.44	0.5	3:1	65	71.1
CHANNEL H*	1124	0.05	3.7	14.1	0.5	1.5:1	25	33.3

Note: Total Depth = Depth of flow + freeboard. Top width is at depth of flow.
* Proposed Channel per Baker Engineering Century Park Blocks 1-6 Drainage Report.

Table 7: Summary of Proposed Culvert Design

Culvert	Culvert Type	Q ₅₀ (cfs)	Slope (%)	Length (ft)	Req. HW (ft)	Velocity (ft/s)
D2 Track*	3-60" Aluminum Culverts	323	1	100	7.01	10.53
D6**	9-60" Aluminum Culverts	1528	1	100	7.5	10.79
OS2	2-36" Aluminum Culverts	84	1	55	3.83	7.9
OS3	2-60" Aluminum Culverts	280	1	100	6.64	9.88
OS4	5-60" Aluminum Culvert	896	1	100	6.32	10.16

*Used in online detention for site
**Modify existing conditions culvert

Table 8: Summary of Hydrologic Erosion Hazard Setback

Channel CP	Q ₅₀ (cfs)	EHS (ft)
Channel A	364	25
Channel B	806	30
Channel C	1240	30
Channel D	1844	30
Channel E (Track)	323	25
Channel F	422	25
Channel G	259	25

Table 8: Summary of Proposed Splash Pad Design

Concentration Point	Q ₅₀ (cfs)	Structure Location	D ₅₀ (ft)	Length (ft)	Width** (ft)	Thk (feet)
D2 Track	323	Culvert Outlet	12	44	15	2
D6	1528	Culvert Outlet	12	46	15	2
OS2	84	Culvert Outlet	6	25	9	1
P1 (A and B)	10	Scupper Outlet	6	3	24	1
P2	10	Scupper Outlet	6	3	20	1
P3 (A and B)	10	Scupper Outlet	6	3	24	1
P4 (A and B)	10	Scupper Outlet	6	3	24	1
P5 (A and B)	10	Scupper Outlet	6	3	24	1
P6 (A)	35	Scupper Outlet	6	8	24	1
P7(B)	3	Scupper Outlet	6	1	12	1
P8 (A)	35	Scupper Outlet	6	8	24	1
P8(B)	3	Scupper Outlet	6	1	12	1
OS1	280	Culvert Outlet	9	38	15	2
OS3	896	Culvert Outlet	12	48	15	2
Channel D	1844	Channel Outlet	6	19	224	1
Channel E	422	Channel Outlet	6	14	76	1
Channel G	259	Channel Outlet	6	11	75	1

**Note: All splash pads will be underlined with filter fabric and flared out at 2:1 within constraints. Design is given per culvert/channel. Width will be 3 times the culvert diameter per culvert. Width will be 2' wider on each side than channel.

Table 10: Summary of Proposed Detention Design

Basin Location	Type of Detention	Detention Volume (Ac Ft)	WSFL (ft)	Quant. Down Stream (CFS)
D1	Basin	2	2791	244
D2	Basin	2.9	2794.5	308
D2 Track	Basin	4.8	2805.3	313
D3	Basin	37.2	2788.9	1470
D4	Basin	1.8	2793.3	62
D6	Basin	3.9	2806.9	1360

*Per HEC-HMS Model routed Qs

Table 11: Summary of Site Discharges

CP	Existing Site Discharge* (cfs)	Developed Site Discharge* (cfs)
D1(D1)	1149	1088
D2(D2)	405	367
D3(D3)	98	92
D6(D6)	1490	1470

*Per HEC-HMS Model routed Qs

Table 12: Summary of Proposed Retention Design

Block Number	Retention Volume (Ac Ft)
5	1.4
6	0.6
7	0.5
8	0.6
9	1
10	0.7
11	1.1
12	0.7
13	1.4
14	0.2
15	1.7
16	0.8

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DEVELOPED CONDITIONS FOR CENTURY PARK FIGURE 4
PROJ. NO: 07002 SCALE: HORIZ 1" = 300'
DATE: JUNE 2008 1 OF 1



F. Vegetation

Novak Environmental, Inc. has produced a Native Plant Preservation Plan, Landscape Mitigation Plan and Riparian Habitat Mitigation Plan for the entire South Kolb Road Specific Plan area, including the Amendment area. The Native Plant Preservation Plan and Landscape Mitigation Plan have been submitted to Pima County Development Services Department for approval and the Riparian Habitat Mitigation Plan has been submitted to Pima County Regional Flood Control for approval.

1. Vegetation Encroachment Areas

The south block will encroach into Pima County Xeroriparian Habitat C. Novak Environmental field verified the xeroriparian habitat during site visits. The total area of encroachment is 5.63 acres. The Cover Page from the Novak Environmental Riparian Habitat Mitigation Plan has been provided (*see Exhibit II.F.1: Riparian Habitat Mitigation Plan Cover Page*).

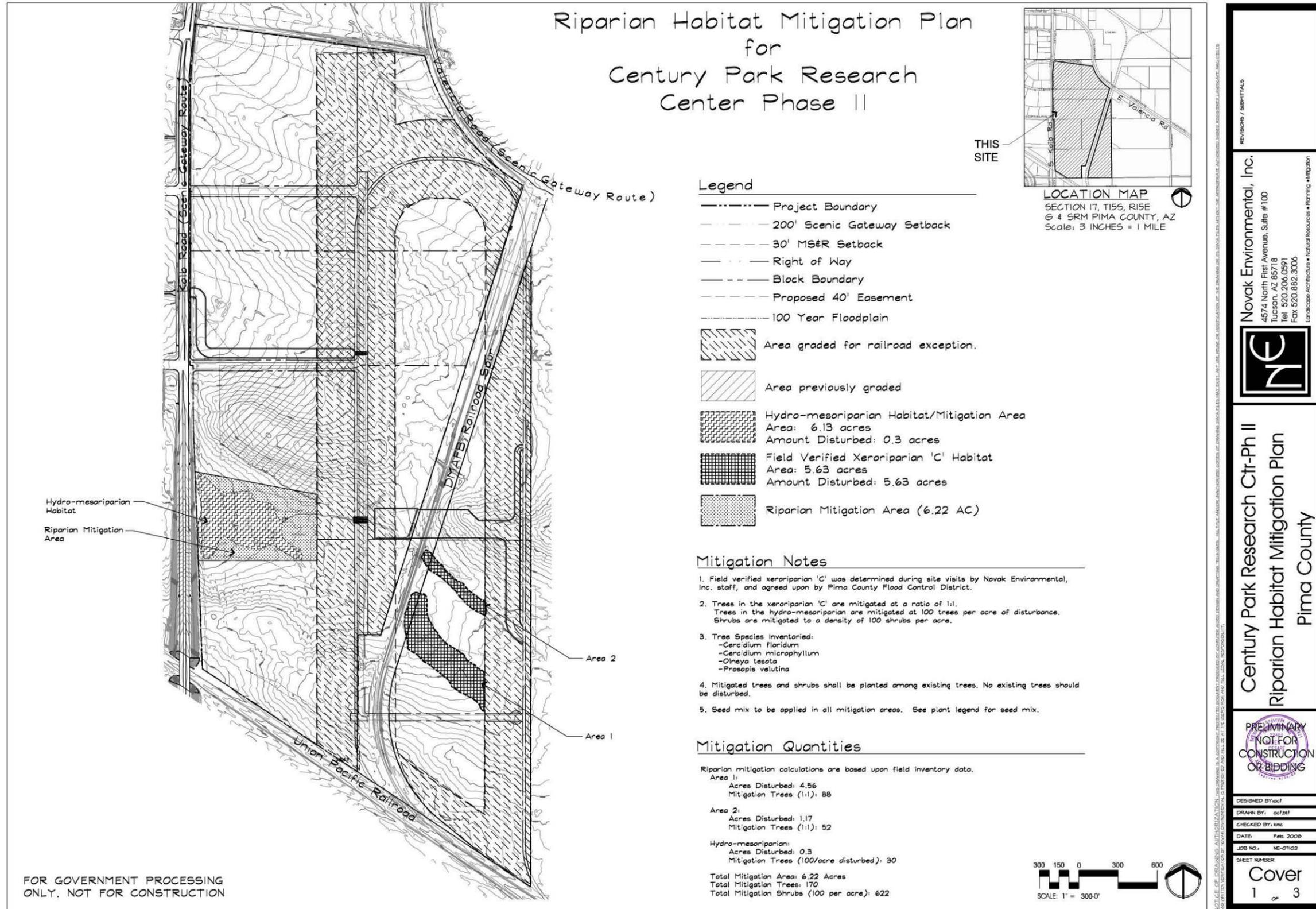
2. Vegetation Mitigation and Preservation

Trees in the xeroriparian area are to be mitigated at a ratio of 1:1 and planted in an area of Pima County Hydro-mesoriparian Habitat located within the existing South Kolb Road Specific Plan area. Mitigated trees and shrubs from the xeroriparian habitat will be planted among existing trees in the receiving area with a seed mix. No existing trees within the hydro-mesoriparian habitat should be disturbed. A total of 140 trees will be mitigated for the total area of encroachment.

Mitigation for on-site vegetation will be done through the selected plant inventory method for both the north block and south block. Mitigated vegetation will be planted within bufferyards and on-site amenity landscaped areas. A Landscape Mitigation Plan has been submitted as part of the tentative plat/development plan.



Exhibit II.F.1: Riparian Habitat Mitigation Plan Cover Page



G. Wildlife

A western burrowing owl survey has been submitted to Pima County Development Services Department for review as part of the tentative plat/development plan. No burrowing owls were found on the existing South Kolb Road Specific Plan or the Amendment area.

H. Soils

Soil surveys will be conducted in conjunction with the tentative plat/development plan.

I. Buffer Plan

The Buffer Plan for the Amendment area is to follow the same guidelines set forth in the existing South Kolb Road Specific Plan. A buffer plan is implemented for each individual planning area and is consistent with Pima County landscaping, buffering and screening ordinance requirements (Chapter 18.73). The Amendment area is divided into three Planning Areas (A, C, and I) as noted in Section II.A.2. A table has been provided that gives required and provided bufferyard information for the planning areas that apply to the Amendment area (see *Table II.I.1: Buffer Plan*). Landscape concepts for entries and internal streets within the Amendment area will follow the same guidelines set forth in the existing South Kolb Road Specific Plan.

Table II.I.1: Buffer Plan

Planning Area	Bufferyard Boundary	Required Bufferyard	Provided Bufferyard (feet)
A	1	D	10'
	2 (Kolb Road)	D	15'
	3	No Bufferyard Required	-
	4	No Bufferyard Required	-
	5 (Valencia Road)	D	15'
	6	No Bufferyard Required	-
	7	No Bufferyard Required	-
	8	D	10'
C	1 (Valencia Road)	D	15'
	2	D	15'
	3 (Kolb Road)	D	15'
	4	D	15'
	5	D	15'
	6	No Bufferyard Required	-
	7	No Bufferyard Required	-
I	-	No Bufferyard Required	-

1. Bufferyard Map

Pages from the Landscape Mitigation Plan produced by Novak Environmental Inc. have been provided for Planning Areas A, C & I as Appendix A: Landscape



Plans. The Amendment areas are outlined in red. The original size of these pages is 24x36; however, the pages have been reduced to 11x17.

J. Viewsheds

1. Measures to Minimize Visual Impacts from Development

a. Views and Vistas from Off-site Locations

Many views and vistas from off-site properties are already impacted by existing industrial development in the area. In order to minimize any additional visual impact, the buildings along Valencia Road will be sited to provide visual relief. Instead of one large structure with a hard, continuous line, the buildings will be broken up into smaller groupings and sited at a variety of distances from the right-of-way, providing an undulating edge for visual diversity and areas for additional landscaping. Distant regional views will also be retained by the grouping of the buildings. Open areas and vistas will occur throughout the site, providing distant regional views from Kolb Road and throughout the site.

b. Areas of High and Medium Visibility

As stated in Section I.H.2, the Amendment area is comprised primarily of areas with moderate visibility, with some areas of high visibility. Visual impacts on the edge of the site will be minimized by the inclusion of landscape buffers along Kolb and Valencia Roads. The buffers will adhere to the required landscape buffers ordinance (Chapter 18.73 of the County Zoning Code). Amenities in the area will include additional trees, cacti, ground cover, berms and walls.

K. Traffic

North Block

A Traffic Impact Study (TIS) has been produced by Mathieu Engineering Corp. for the northern portion of the existing South Kolb Road Specific Plan, which includes the north block of the Amendment, and has been submitted as part of the tentative plat/development plan. The findings from this report have been incorporated into this section.

South Block

A TIS has been produced by PSOMAS for the southern portion of the existing South Kolb Specific Plan, which includes the south block of the Amendment, and has been submitted as part of the tentative plat/development plan. The findings from this report have been incorporated into this section.

1. Access Points

North Block



Access points for the north block have not yet been determined, however, access to the north block will be limited due to limited frontage along Valencia Road. Currently, there is 500 feet of frontage along Valencia Road to the north and 300 feet of frontage to the south. Based on the TIS produced for the northern portion of the existing South Kolb Road Specific Plan, one major access point is proposed along the existing stretch of Valencia Road. The location of this access and other auxiliary access driveways has not yet been determined and will require Pima County Department of Transportation approval. Any proposed access points shall meet Subdivision and Development Street Standards.

South Block

Access to the south block will be from the southern portion of the existing South Kolb Road Specific Plan (see *Exhibit IIA.3: Land Use Concept*). Access in easement or in fee is currently being negotiated with Davis-Monthan Air Force Base.

2. Future Road Improvements

Valencia Road is identified as a key corridor by the Regional Transportation Authority. Major improvements include widening the roadway from four lanes to a six-lane desert parkway between Kolb Road and Houghton Road, and additional turn lanes, signalization and intersection improvements at the intersection of Kolb and Valencia Roads. There are preliminary designs to reroute the intersection of Kolb and Valencia Roads south of the existing intersection and preserve the existing intersection and roadway for local access into the project area (see *Exhibit II.K.2: Valencia Road Realignment*). This project is to be coordinated by Pima County Department of Transportation. A project schedule has not yet been determined.

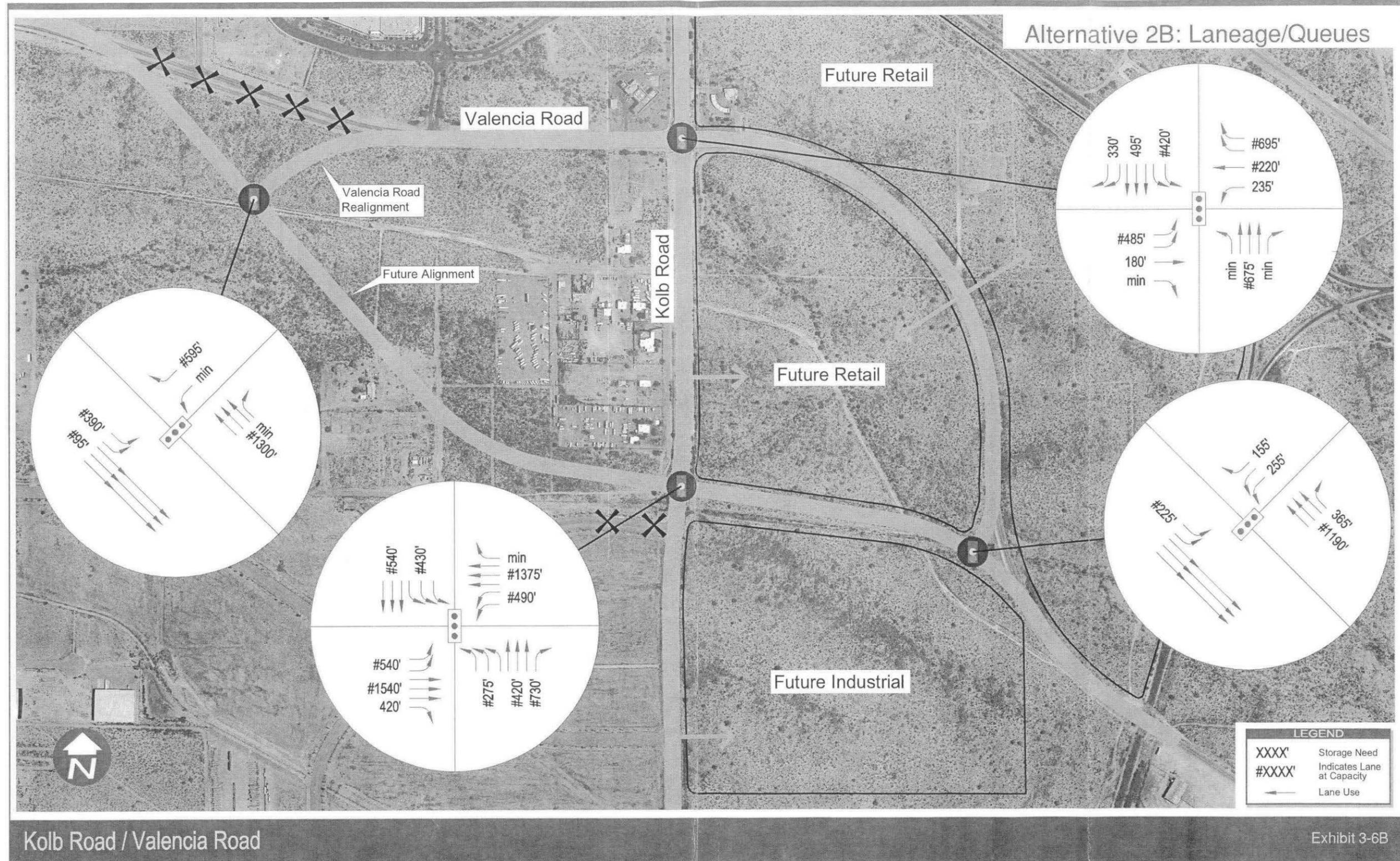
Road improvements associated with the existing South Kolb Road Specific Plan include two access points at Littletown Road and Century Park Drive and an interior roadway to provide access within the site and access to the south block of the Amendment (see *Exhibit IIA.3: Land Use Concept*). It is recommended that both roadways be two-lane roadways with three lanes at the intersections of Kolb Road, one each for through, left turns and right turns. It is recommended that a northbound right-turn lane be constructed at Century Park Drive and Kolb Road at buildout of the project; no other right-turn lanes are recommended. There are no other road improvements proposed that affect the Amendment area.

Kolb Road will remain a four-lane roadway.

Coordination between Pima County Department of Transportation and the owners of property within the South Kolb Road Specific Plan will be ongoing for future road improvements in the vicinity of the project.



Exhibit II.K.2: Valencia Road Realignment



3. Changes to ADT and LOS

North Block

The TIS produced by Mathieu Engineering Corp. for the northern portion of the existing South Kolb Road Specific Plan provides total traffic generation for the entire site including the north block of the Amendment. The average daily traffic (ADT) volume and the AM and PM Peak Hour volumes generated for a proposed shopping center has been estimated using trips rates or equations provided in the Institute of Transportation Engineer’s (ITE) *“Trip Generation, 7th Edition”*, Land Use Code: 820 – Shopping Center. At full build-out (2030), the proposed development is expected to generate a total of 57,430 daily trips, 1,378 AM Peak Hour trips, and 5,015 PM Peak Hour trips, based on an estimated 1,337,456 SF of shopping center (see *Table II.K.3.a: Trip Generation at Buildout – North Block*). Due to the conceptual nature of the northern portion of the existing South Kolb Road Specific Plan and the north block of the Amendment, ADT was generated using Shopping Center counts to estimate the “worst case scenario” for traffic generation as recommended by Carla Blackwell of Pima County Development Services. If a shopping center is not developed, traffic generation from any proposed development can only decrease since shopping centers generate the most traffic of any use.

Table II.K.3.a: Trip Generation at Buildout – North Block

LAND USE	ITE CODE	NO. OF SF	NUMBER OF VEHICLE TRIPS						
			AM PEAK HOUR			PM PEAK HOUR			DAILY (TWO-WAY)
			IN	OUT	TOTAL	IN	OUT	TOTAL	
Shopping Center	820	1,337,456	840	537	1,378	2,407	2,608	5,015	57,430
TOTAL TRIPS			840	537	1,378	2,407	2,608	5,015	57,430

South Block

According to the TIS produced by PSOMAS, the southern portion of the South Kolb Road Specific Plan is expected to generate 2,403 trips per day at buildout, including 177 trips in the morning peak hour and 256 in the evening peak hour. The trip generation was determined using data collected from the warehousing site just west of this project. The trip generation rates calculated from that data were found to be acceptable for use for this project because the land use and project owner are the same and the existing site is served by rail and trucks, all indications that the two sites are comparable and the trip generation rates are valid (see *Table II.K.3.b: Trip Generation at Buildout – South Block*).



Table II.K.3.b: Trip Generation at Buildout – South Block

Warehousing Field Data - Buildout (2018)						
Floor Area (1000 sq.ft.)			3,940			
Period	Trips/Unit	Trips	% In	% Out	Trips In	Trips Out
AM Peak	0.045	177	50%	50%	89	89
PM Peak	0.065	256	44%	56%	113	143
Daily	0.61	2,403	50%	50%	1,202	1,202

The LOS analysis indicated that left turns from Littletown Road onto Kolb Road will operate at LOS E in the evening peak hour at buildout. The eastbound and westbound left turns at that location will operate at LOS D and E in the morning peak hour, respectively. All other movements will operate at LOS D or better. Assuming a signal at Century Park Drive and Kolb Road at buildout of the project, all movements will operate at LOS C or better in both peak hours at that location. The project will have very little impact on the delays at the new Kolb Road/Valencia Road intersection, which will operate at LOS C in both peak hours at buildout (2018) with or without the project (see Table II.K.3.c: Projected LOS at Buildout).

Table II.K.3.c: Projected LOS at Buildout

		New Valencia Road						Kolb Road						Traffic Control	Intersection LOS
		EB			WB			NB			SB				
		L	T	R	L	T	R	L	T	R	L	T	R		
AM	LOS	D	C	A	C	C	B	D			D			Signal	C
	Delay	37.8	23.6	5.7	32.5	22.7	10.3	42.1			43.7				21.0
PM	LOS	D	D	B	D	D	A	C			C				C
	Delay	53.6	48.5	13.5	35.7	36.8	9.3	22.3			34.8				34.3

		Littletown Road						Kolb Road						Traffic Control	Intersection LOS
		EB			WB			NB			SB				
		L	T	R	L	T	R	L	T	R	L	T	R		
AM	LOS	C		B	D		B	A			B			Two-Way Stop	N/A
	Delay	20.0		11.0	27.9		12.5	9.4			11.2				N/A
PM	LOS	D		B	E		B	B			B				N/A
	Delay	29.2		12.4	42.7		13.7	10.2			12.4				N/A

		Century Park Drive						Kolb Road						Traffic Control	Intersection LOS
		EB			WB			NB			SB				
		L	T	R	L	T	R	L	T	R	L	T	R		
AM	LOS	C		A	C		A	A	A	A	A	A	A	Signal	A
	Delay	24.0		0.1	24.3		0.6	4.8	3.9	2.0	5.7	4.2	4.2		4.8
PM	LOS	C	C	B	C		A	A	A	A	A	A	A		A
	Delay	24.4	23.0	13.0	22.1		3.2	6.9	5.4	2.5	7.4	5.8	5.8		6.4

4. Minimizing Traffic Impact on Local Streets

It is anticipated that the realignment of Valencia Road will allow for the existing Valencia Road to provide local access to the north block of the Amendment. The realigned Valencia Road will become the main arterial route. The south block of the Amendment is not anticipated to have any impact on local streets since there will be no access to existing local streets.



5. Bicycle and Pedestrian Pathways

It is anticipated that all interior roadways will include a six-foot bicycle lane and a landscaped pedestrian pathway as provided in the existing South Kolb Road Specific Plan.

6. Typical Roadway Sections

As stated in the existing South Kolb Road Specific Plan, all internal roadways are anticipated to be divided two-way roads with 24 feet of pavement each side of the median. The pavement section would be striped for one 18-foot travel lane and a 6-foot bicycle lane. Both the median and perimeters would be landscaped and pedestrian ways provided in the perimeter area. A 90-foot right-of-way would be provided.

It is recommended that any four-lane divided roadways or six-lane divided roadways be constructed per Pima County Roadway Design Manual.

L. Sewers

1. Method of Sewer Service

A Type II Capacity Response Letter was received (*see Exhibit I.J.1: Pima County Wastewater Letter*) on June 19, 2008. The provisions set forth in the letter will be agreed upon by the owner / developer of the Amendment prior to the approval of a tentative plat/development plan.

2. Sewers within Natural Areas

Sewer lines are not anticipated to be located within natural areas.

M. Water

1. Statement of Water Service

Tucson Water is designated as having a 100-year assured water supply and will be serving the site (*see Exhibit II.M.1: Statement of Water Service Provision and Availability*).



Exhibit II.M.1: Statement of Water Service Provision and Availability

October 26, 2007



Valencia Kolb Properties, LLC
6601 E. Grant Road, Suite 101
Tucson, AZ 85715

Attn: Michael Farley

CITY OF
TUCSON
TUCSON WATER
DEPARTMENT

**SUBJECT: Water Availability for project: Research Project F Zone Industrial Area, APN:
Multiple Parcels, T-15, R-15, SEC-08, Lots, Location Code:, Total Area: 739ac**

WATER SUPPLY

Tucson Water will provide water service to the subject project. Tucson Water has an assured water supply (AWS) designation from the State of Arizona Department of Water Resources (ADWR). An AWS designation means Tucson Water has met the criteria established by ADWR for demonstration of a 100-year water supply – it does not mean that water service is currently available to the subject project.

WATER SERVICE

The approval of water meter applications is subject to the current availability of water service at the time an application is received. The developer shall be required to submit a water master plan identifying, but not limited to: 1) Water Use; 2) Fire Flow Requirements; 3) Offsite/Onsite Water Facilities; 4) Loops and Proposed Connection Points to Existing Water System; and 5) Easements/Common Areas.

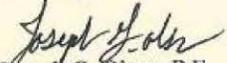
Any specific area plan fees, protected main/facility fees and/or other needed facilities' cost, are to be paid by the developer. *If the existing water system is not capable of meeting the requirements of the proposed development, the developer shall be financially responsible for modifying or enhancing the existing water system to meet those needs.*

This letter shall be null and void one year from the date of issuance.

Issuance of this letter is not to be construed as agency approval of a water plan or as containing construction review comments relative to conflicts with existing water lines and the proposed development.

If you have any questions, please call New Development at 791-4718.

Sincerely,


Joseph G. Olsen, P.E.
Engineering Manager
New Development Unit

JGO:rlm
CC:File



NEW DEVELOPMENT • P.O. BOX 27210 • TUCSON, AZ 85726-7210
(520) 791-4718 • FAX (520) 791-5288 • TTY (520) 791-2639 • www.cityoftucson.org



N. Schools

There are no residential uses proposed for the Amendment; therefore, no school information is provided.

O. Recreation and Trails

1. Recreation Areas to be Provided

There are no proposed recreation areas within the existing South Kolb Specific Plan or the Amendment area.

2. Proposed Ownership of Open Space

Landscape bufferyards and common areas are to be owned and maintained by future owners, who are yet to be determined.

P. Cultural Resources: Archaeological and Historic Sites

The Class III cultural resources surveys conducted by WestLand Resources, Inc. have been submitted under a separate cover. A summary of their findings follow:

1. Cultural Resources Survey

North Block

A 137-acre survey conducted by Westland Resources, Inc. identified five isolated occurrences, an unrecorded segment of AZ BB:13:725(ASM)/Old Vail Road, and a newly identified site, recorded as AZ BB:13:799(ASM) all located within the existing South Kolb Road Specific Plan and Amendment areas. Westland recommends that the segment of Old Vail Road, AZ BB:13:725(ASM) be ineligible for inclusion in the National Register of Historic Places. Site AZ BB:13:799(ASM) is not located within the “North Block” area defined in this Amendment, but is located within the existing Specific Plan land area. Westland recommends that site AZ BB:13:799(ASM) be eligible for inclusion in the National Register and therefore recommends either avoidance or implementation of a mitigation plan.

South Block

A 300-acre survey conducted by Westland Resources, Inc. identified 15 isolated occurrences, two newly identified sites, AZ BB:13:792(ASM) and AZ BB:13:793(ASM), a previously unrecorded segment of AZ BB:13:725(ASM)/Old Vail Road, and one previously recorded site AZ AA:12:875(ASM)/EPNG 1007 Line. Site AZ BB:13:793(ASM) is found within the “South Block” as defined in this Amendment while sites AZ BB:13:792(ASM) and AZ BB:13:725(ASM) are located outside the “South Block”. Westland recommends that all three sites [AZ BB:13:792(ASM), AZ BB:13:793(ASM), and the portion of AZ BB:13:725(ASM)] are ineligible for listing in the National Register.



Site AZ AA:12:875(ASM)/EPNG 1007 Line runs through the “South Block” defined in the Amendment as well as the land included in the existing Specific Plan. The EPNG 1007 Line, AZ AA:12:875(ASM) is an “in-use” gas line and therefore is exempt from Section 106 Historic Preservation Review (Federal Register Vol. 67, No. 66, April 5, 2002).

2. Archaeological and Historic Sites

There have been archaeological surveys conducted within the areas defined in this document as the “North Block” and the “South Block.” Only one recommended eligible cultural resource exists within these areas. However, since the site AZ AA:12:875(ASM) is an “in-use” gas line and is exempt from Section 106 requirements. Therefore, the survey report includes the recommendation that the proposed development will have no impact on cultural resources; therefore, no mitigative actions are necessary.

It is noted, however, that there are known cultural resources located within the area defined in the existing South Kolb Road Specific Plan. Therefore, a cultural resources mitigation plan for any identified archaeological and historic sites on the subject property shall be submitted to Pima County at the time of, or prior to, the submittal of any tentative plan or development plan. Following rezoning approval, any subsequent development requiring a Type II grading permit will be reviewed for compliance with Pima County’s cultural resources requirements under Chapter 18.81 of the Pima County Zoning Code.

3. Archaeological Resource Measures

In the event that cultural resources are revealed during ground-disturbing activities, all construction shall cease, and consultation shall be initiated with ASM to assess the potential significance of any unearthed materials (ARS §41-841). If human skeletal remains or funerary objects are discovered, ASM will be contacted immediately (ARS §41-865 & §41-844).

Q. Air Quality

Air quality activity permits will be secured by the developer or primary contractor before constructing, operating or engaging in an activity which may cause or contribute to air pollution.

R. Other

As this is a Shovel Ready project, users have not been identified and details regarding the design of buildings and grounds are not available at this time. Future buyers and tenants will be encouraged to incorporate water harvesting and solar features into their plans where practicable.

1. Agreements with Neighboring Properties

Both property owners (Lucky Levin Railroad, LLC. and Valencia Kolb Properties, LLC.) of the South Kolb Road Specific Plan Amendment have met with and



discussed the proposed Amendment and future development opportunities with DMAFB. Both parties agree that any proposed development must adhere to land use policies in the JLUS. This includes restricting commercial development within the Approach-Departure Corridor that affects the south block of the Amendment.



Part III
Development Standards



A. Introduction

The development regulations for the South Kolb Road Specific Plan Amendment are similar to the standards approved for the existing South Kolb Road Specific Plan; however, an increased height allowance has been applied to the Amendment areas as noted in Section III.C. The regulations include standards for developed land uses, landscape coverage, and aircraft approach compatible use zones. Pima County Ordinances regarding scenic routes, landscaping, buffering and screening, off-street parking and loading, sign standards, grading, flood control, and airport approach will be adhered to. Additional buffering guidelines are proposed in the Development Regulations and Design Guidelines and Policies.

B. Definitions

Pima County Zoning Code definitions and usage will be applied to the South Kolb Road Specific Plan Amendment.

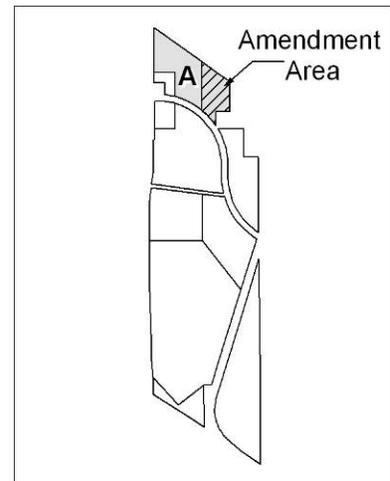
C. Land Use Designations and Development Standards

1. Planning Area A

Under the existing South Kolb Road Specific Plan, Planning Area A was approximately 24.5 acres.

a. Location

Planning Area A is approximately 40.3 acres with Amendment acreage and is located at the northern edge of the existing South Kolb Road Specific Plan. It is bounded on the north and east by DMAFB, on the south by Valencia Road and on the west by Kolb Road.



b. Land Use

Light industrial and warehouse uses as allowed in CI-1 zone and commercial uses as allowed in CB-1 and CB-2 zones are proposed for Planning Area A.

c. Airport Environs and Facilities Overlay Zone

Planning Area A is not impacted by the Airport Environs Zone.

d. Joint Land Use Study (JLUS) Special Area Policies

Planning Area A is impacted by JLUS Policy S-26 (Noise Control Zone B) and S-27 (Noise Control Zone A). Noise attenuation measures are required for noise sensitive uses to reduce interior noise levels for both zones. The proposed development must be in full conformance with

JLUS use restrictions and development standards as stated in the Pima County Comprehensive Plan Special Area Policies.

- e. **Applicable Zoning Categories**
Chapter 18.51, CI-1 Zone; Chapter 18.39, General Commercial Standards; Chapter 18.43, CB-1 Zone; and Chapter 18.45, CB-2 Zone; in conjunction with Chapter 18.57, Airport Environs and Facilities Overlay Zone.
- f. **Additional Requirements**
Building heights shall not exceed 62 feet.

A buffer along the northern boundary with DMAFB will be provided to mitigate on-site visual and noise impacts. The buffer shall be consistent with Bufferyard D in the Pima County Design Manual.

Development shall meet the Scenic Routes Ordinance, 18.77.

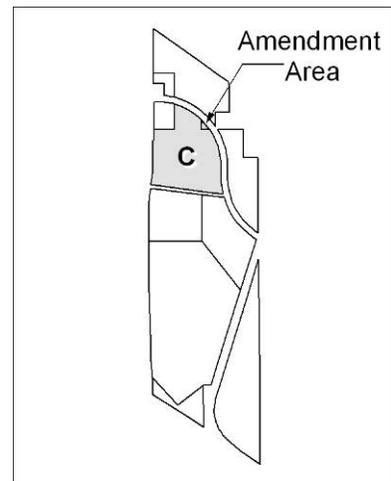
The landscape treatment along Valencia Road shall meet the Landscaping, Buffering and Screening Standards Ordinance, 18.73. CB-1 and CB-2 uses shall meet Bufferyard D requirements.

Impervious coverage shall be limited to 75%.

2. **Planning Area C**

Under the existing South Kolb Road Specific Plan, Planning Area C was approximately 59.5 acres.

- a. **Location**
Planning Area C is approximately 60.0 acres with Amendment acreage and is centrally located within the existing South Kolb Road Specific Plan. It is bounded by Valencia Road to the north and east, Kolb Road to the west and the proposed Valencia Road realignment to the south.
- b. **Land Use**
Light industrial and warehouse uses as allowed in CI-1 zone and commercial uses as allowed in CB-1 and CB-2 zones are proposed for Planning Area C, and restricted by Chapter 18.57, Airport Environs and Facilities Overlay Zone.



- c. **Airport Environs and Facilities Overlay Zone**
Planning Area C is impacted by Compatible Use Zone 4 (CUZ-4) of the Airport Environs Zone. Noise attenuation measures are required for noise sensitive uses to reduce interior noise levels to 45 Ldn. The proposed development must be in full conformance with Chapter 18.57, Airport Environs and Facilities Overlay Zone.
- d. **Joint Land Use Study (JLUS) Special Area Policies**
Planning Area C is impacted by JLUS Policy S-24 (Approach-Departure Corridor 2) and S-26 (Noise Control Zone B). Residential and commercial uses are restricted within Approach-Departure Corridor 2 and use intensity, planned development area and floor area ratios are controlled. Noise attenuation measures are required for Noise Control Zone B. The proposed development must be in full conformance with JLUS use restrictions and development standards as stated in the Pima County Comprehensive Plan Special Area Policies.
- e. **Applicable Zoning Categories**
Chapter 18.51, CI-1 Zone; Chapter 18.39, General Commercial Standards; Chapter 18.43, CB-1 Zone; and Chapter 18.45, CB-2 Zone; in conjunction with Chapter 18.57, Airport Environs and Facilities Overlay Zone..
- f. **Additional Requirements**
Building heights shall not exceed 62 feet.

Development shall meet the Scenic Routes Ordinance, 18.77.

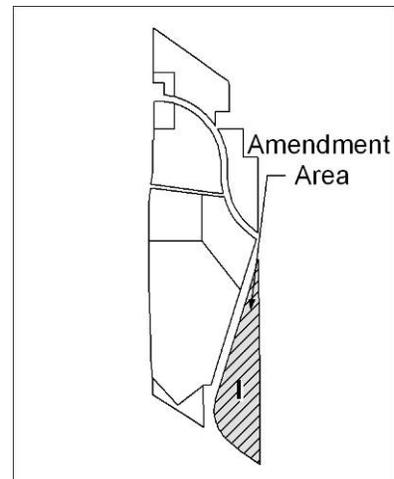
The landscape treatment along Valencia Road, Kolb Road and the proposed Valencia Road realignment shall meet the Landscaping, Buffering and Screening Standards Ordinance, 18.73.

Impervious coverage shall be limited to 70%.

3. **Planning Area I**

There is no Planning Area I under the existing South Kolb Road Specific Plan.

- a. **Location**
Planning Area I is approximately 67.6 acres and located east of the existing South Kolb Road Specific Plan. It is bounded by private property to the east, Union Pacific Railroad to the south and the abandoned DMAFB rail spur to the west.



- b. Land Use
Light industrial and warehouse uses as allowed in CI-1 zone and general industrial uses as allowed in CI-2 are proposed for Planning Area I, restricted by Chapter 18.57, Airport Environs and Facilities Overlay Zone.
- c. Airport Environs and Facilities Overlay Zone
Planning Area I is impacted by Compatible Use Zones 2, 3 & 4 (CUZ-2, CUZ-3 & CUZ-4) of the Airport Environs Zone. The purpose of the regulations for CUZ-2 is to minimize safety risks. Noise attenuation measures are required for noise sensitive uses to reduce interior noise levels to 45 Ldn. The proposed development must be in full conformance with Chapter 18.57, Airport Environs and Facilities Overlay Zone.
- d. Joint Land Use Study (JLUS) Special Area Policies
Planning Area I is impacted by JLUS Policy S-24 (Approach-Departure Corridor 2) and S-26 (Noise Control Zone B). Residential and commercial uses are restricted within Approach-Departure Corridor 2. Use intensity is restricted to a maximum of 20 employees per acre, site area is restricted to a minimum of 5 acres and floor area ratio is restricted to a maximum of 30 percent. Noise attenuation measures are required for Noise Control Zone B. The proposed development must be in full conformance with JLUS use restrictions and development standards as stated in the Pima County Comprehensive Plan Special Area Policies.
- e. Applicable Zoning Categories
Chapter 18.51, CI-1 Zone and Chapter 18.53, CI-2 Zone; in conjunction with Chapter 18.57, Airport Environs and Facilities Overlay Zone.
- f. Additional Requirements
Building heights shall not exceed 62 feet.

There are no landscape bufferyard requirements for Planning Area I.

Impervious coverage shall be limited to 60%.

D. Other Variations from Zoning Code

There are no variations from the current Pima County Zoning Code for the South Kolb Road Specific Plan Amendment.



Implementation and Phasing, etc

Part IV

Implementation and Phasing Schedule,
Amendment Procedures, Design Guidelines
and Monitoring Report



A. Implementation and Phasing Schedule

1. Introduction

The existing South Kolb Road Specific Plan and the Amendment is a pilot “shovel ready” project. The Shovel Ready Program is being introduced by Tucson Regional Economic Opportunities Inc. (TREO) to address the needs of targeted industries for certified, high quality information about sites and for assured quick turnaround on facility permitting. The program is intended to increase the region’s ability to compete for high-skill/high-wage companies and is intended to be a strong selling point for companies to relocate to Tucson. Under the Shovel Ready Program for this project, a tentative plat/development plan was submitted to Pima County Development Services Department. Once a tentative plat is approved, a grading plan will be submitted for approval to allow for land clearing and plant relocation in preparation for channel and detention basin construction. A paving plan for the private roads will also be submitted with the grading plan. Currently, sewer and water plans are in review, and Southwest Gas, Tucson Electric Power and Qwest are preparing utility plans. Once the final plat, grading plan, paving plan and utility plans are approved, an application will be made to TREO to certify "shovel ready" status.

2. Implementation Measures

This Amendment is the third step in an 8 stage process; however, this process is subject to revision because the Shovel Ready Program is in its early stages and this project is the first to be subject to the program’s standards. The two previous steps include the existing Specific Plan, which was approved in 1988, and the modification (non-substantial change) of rezoning condition, which was approved in 2006. The following steps are as follows:

1. Existing South Kolb Road Specific Plan (1988);
2. Modification (non-substantial change) of rezoning condition (2006);
3. South Kolb Road Specific Plan Amendment
4. Tentative Plat/Development Plan;
5. Grading Plan/Paving Plan/Utility Plan;
6. Final Plat;
7. Shovel Ready status;
8. Building Permits.

The implementation of the proposed South Kolb Road Specific Plan Amendment will be accommodated through the application of this document and the existing Specific Plan document and additional studies. Land use development in the Amendment area shall be guided by the Land Use Concept Map, Development Regulations, and Design Guidelines and Policies. Transportation improvements will be done in conjunction with the Traffic Impact Study that was submitted with the tentative plat/development plan. The Study addresses the following items:

- Major Streets and Routes



- Internal Project Circulation Improvements
- Roadway Capacity
- Total Facility Demands and Proportionate Shares
- Areas of Responsibility for Pima County, and the developers
- Financing Mechanisms

All development related to internal transportation and improvements/financing will be linked with project phase, building permits/impact fees, or other measures identified in the study.

The establishment of design criteria to include internal street widths, typical cross-sections, design speeds, utility locations, maximum design roadway slopes, access control, bike paths and pedestrian way or sidewalks shall be subject to approval by the Department of Transportation and Pima County Regional Flood Control District.

Necessary drainage improvements will be installed by the developer in conjunction with the tentative plat/development plan. A master or basin drainage study is not necessary given the site location at the upstream end of drainage areas.

Monitoring Reports described in Section IV.D will summarize the land use and infrastructure implementation on an annual basis. Pima County Development Services will maintain an ongoing implementation file on the project. All development plans and building permits will be required to conform to the existing Specific Plan and Amendment.

3. Phasing Plan

The phasing plan is subject to revision. The Amendment includes parcels that are either included in existing Planning Areas (Planning Areas A & C) or are new Planning Areas (Planning Area I). Planning Area A is considered Phase 5 in the existing Specific Plan while Planning Area C is considered Phase 3. Planning Area I would be considered Phase 8 within the existing Specific Plan. The development of these Planning Areas is going to be dependent on market conditions and the marketability of the property.

B. Administration/Amendment Procedures

1. Overview

There are three entities involved in the implementation of the Amendment; the primary developer, the secondary builders/developers, and Pima County Development Services.

The primary developer is responsible for planning and coordinating the construction of the basic infrastructure facilities to serve the planning areas within the existing Specific Plan and Amendment. The primary developer shall be responsible for the implementation of the spine infrastructure system. If



ownership of the primary developer changes, the subsequent owner will be responsible for implementing and coordinating the construction of the basic infrastructure facilities.

2. Administrative Changes

Pima County Development Services Department will administer the South Kolb Road Specific Plan Amendment. Administrative changes to the plan may be decided and coordinated by the Planning Director.

3. Specific Plan Modification

The Specific Plan may be amended as outlined in Section 18.90.080 of the Pima County Zoning Code. An application for non-substantial changes shall be submitted to the Pima County Development Services Department and will require action by the Board of Supervisors. A substantial change in land use designations, land use regulations, or design guidelines requires a plan amendment in accordance with Section 18.90.080 of the Pima County Zoning Code. Plan amendments require staff, Planning and Zoning Commission and Board of Supervisors review. A substantial change, as determined by the planning director, shall require a recommendation by the Planning and Zoning Commission and action by the Board of Supervisors.

C. Design Guidelines

1. Introduction

The purpose of the Design Guidelines is to guide the character of future development. The intent of the guidelines is to encourage innovative planning while maintaining high standards of architectural and landscape design. The Design Guidelines are intended to enhance the visual quality of the two adjacent scenic routes, Kolb and Valencia Roads. The guidelines also promote project-wide architectural and landscape integrity, protecting property values within and adjacent to the specific plan boundary. The provisions of this chapter are not regulatory, but are designed to augment the existing zoning code.

The following design elements are encouraged:

- Architectural themes with an aesthetically pleasing and cohesive development which create a sense of identity;
- Building height and mass should be varied with integrated buffers and landscape themes.

The following elements are to be avoided or minimized in site and architectural design:

- Long unarticulated building facades;
- Continuous, unbroken screen walls;
- Incompatible use of colors and materials among different uses.



2. Streetscapes

a. Perimeter Streets

Kolb and Valencia Roads are designated scenic routes. The objective of the Scenic Routes Ordinance is to create and maintain attractive streetscapes on well traveled roadways with native and drought-tolerant vegetation. Development on the South Kolb Road Specific Plan Amendment site is required to meet the Scenic Routes Ordinance as stated in Chapter 18.77 of the Zoning Code. In addition, the following elements are to be incorporated into the project design:

- A landscape buffer along the Valencia and Kolb Road frontages shall consist of a minimum of a 15 foot wide buffer.
- A variety of building setbacks shall be provided from Kolb and Valencia Roads with minimum setbacks of 30 feet plus half of the required right-of-way.
- Sidewalks will be provided along the arterial and internal spine roadway.
- The perimeter landscape buffer shall vary in width to reflect varied building setbacks.
- The buffer shall contain either existing, undisturbed vegetation or revegetation of drought tolerant species which are typically found on the project site and surrounding properties.
- Buildings to avoid a monotonous appearance through the use of materials and architectural design. Buildings to have projections, varying rooflines, or a change in building materials. Landscaping to be integrated with the building height and mass.
- All parking areas shall be screened from view from Kolb and Valencia Roads and shall be enhanced with perimeter screening and landscaping.
- All new or relocated utility lines shall be placed underground if feasible.
- Building elevations along Kolb Road and Valencia Road shall be varied to provide an appropriate streetscape review.

b. Internal Streets

The streetscape for the spine road within the existing Specific Plan area shall include landscape material from the recommended plant list in the Pima County Design Manual. The spine road does not impact the Amendment. The official plant list includes a wide variety of drought tolerant vegetation which is well suited to the desert environment. Landscape berms will be utilized to visually enhance the development and to screen parking areas. The landscape berms may be provided with or without walls, and shall be in conformance with the Landscape Design Manual. All new or relocated utility lines shall be placed underground if feasible.



3. Entries

There are no major entries for the Amendment at this time; however, if a major entry is proposed for the Amendment area, the following elements are to be integrated into the entry design:

- The entries shall be defined by a 50 foot radius as measured from the property corners at these intersections.
- The area within the radius shall be landscaped with accent trees and plantings which may vary from but shall be consistent with the scenic route landscape theme. The landscaping must also be in accordance with the landscaping and buffer (Chapter 18.75) requirements and Scenic Route (Chapter 18.77) requirements of the Zoning Code.
- The plantings and monuments shall be consistent at all entries. Monument signs are to be constructed and designed in conformance with the standards for freestanding signs in Chapter 18.79 of the Pima County Zoning Code Sign Standards. Monument signs shall also conform to Scenic Route requirements.
- Monument signs at entry points and along the internal spine roadway shall reflect and be compatible with building materials, colors, texture and style.

4. Lighting

Lighting shall be provided in scale with its function and surrounding structures.

- Parking lot lighting fixtures shall have a maximum height of 20 feet, and walkway lighting fixtures shall have a maximum height of 15 feet.
- Accent illumination shall be provided where necessary at entrances and exits. In all instances, outdoor lighting shall be in full compliance with the Pima County Outdoor Lighting Ordinance.
- Parking lot, driveway and walkway lighting shall provide uniform illumination. High pressure sodium lighting (fully shielded, and as permitted in the light pollution code) is encouraged in consideration of light pollution concerns and the provision of a lighting pattern which minimizes contrast between lit and unlit.

5. Architectural Guidelines

The purpose of the Architectural Guidelines is to promote superior site and building design on the project which is compatible with surrounding development and enhances the natural desert environment.

- The sides, front, and roof lines of buildings visible from Valencia and Kolb Roads shall be designed such that they jog, have slight projections or material changes or are interrupted by landscaping in order to avoid a monotonous appearance. Such design features are intended to complement the scenic route status of Kolb and Valencia Roads.
- For any development within 200 feet of Valencia Road, a view corridor with a combined width of at least twenty percent of the street frontage of each planning area shall be provided. A viewshed analysis to demonstrate compliance shall be submitted to Pima County Development Services Department.
- A common theme for lighting, signage, street furniture, materials and colors shall be established in conformance with the Scenic Routes



Ordinance, the Landscape Ordinance, and the Design Guidelines of this Specific Plan.

- All rooftop mechanical/electrical equipment shall be screened. Buildings shall have varied setbacks from street frontages.
- Loading dock areas shall not be visible from the major streets through a recessed building design, screening, or other means.

6. Transportation Conservation

Transportation conservation measures include both policy decisions whereby ride sharing and alternative modes of transportation are promoted, as well and physical improvements which facilitate the implementation of conservation measures. The following policy and design measures are proposed to supplement the county trip reduction ordinance.

- Encourage employers to provide incentives for ride sharing (such as preferential parking) or to subsidize alternative transportation modes (such as bus passes or vanpooling programs).
- Coordinate employer transportation programs on a site-wide basis to increase potential benefits.
- The property owner(s) shall participate in a sub-regional park and ride lot if warranted in the future.
- Promote flexible working hours and staggered shifts to avoid traffic congestion at peak hours.
- Encourage employers to provide physical improvements which will facilitate the use of alternative modes of transportation including bicycle racks and showers/locker rooms.

D. Monitoring Report

The South Kolb Road Specific Plan Amendment Phasing Plan will guide development in the plan area over time. The Monitoring Program is designed to facilitate development plan review and to ensure continuity in plan administration.

The Pima County Specific Plan Ordinance requires that the principal owner or developer of the Specific Plan prepare annual Progress Reports for County staff, the Planning and Zoning Commission and Board of Supervisors. The purpose of the report is to compare actual progress with the Phasing Plan. The progress reports shall contain the following information:

- Project Phasing and Project Development
- Plan Area Phasing
- Infrastructure Improvements
- Zoning Violations (if applicable)
- Review and Approval Progress
- Number of permits issued
- Number of structures built or under construction



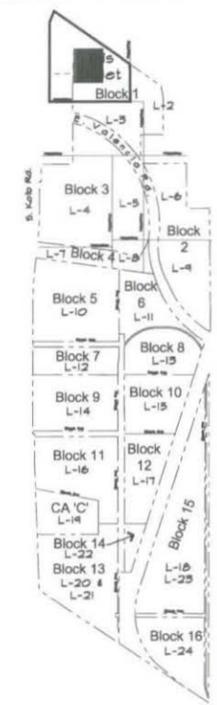
Appendix



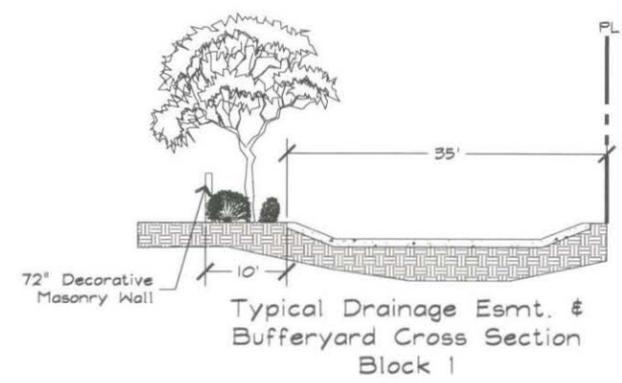
Landscape Plans Century Park Research Center, Phase II Block 1

Legend

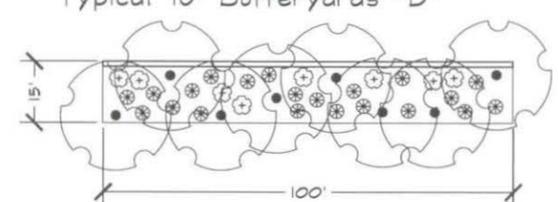
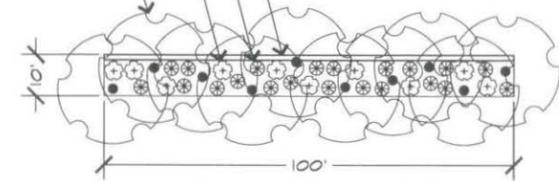
- Project Boundary
- Block Boundary
- Bufferyard
- Xeroriparian "C"
- 100 yr Floodplain
- Easements
- 200' Scenic Gateway Setback
- 30' MS&R Setback
- Matchline
- Drainage Channel
- Existing Contours



SHEET LAYOUT
Note: Sheets for Block 5-16 are laid out by block.

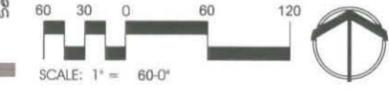


Decorative Masonry Wall
Accent (Barrel Cactus)
Shrub
Tree



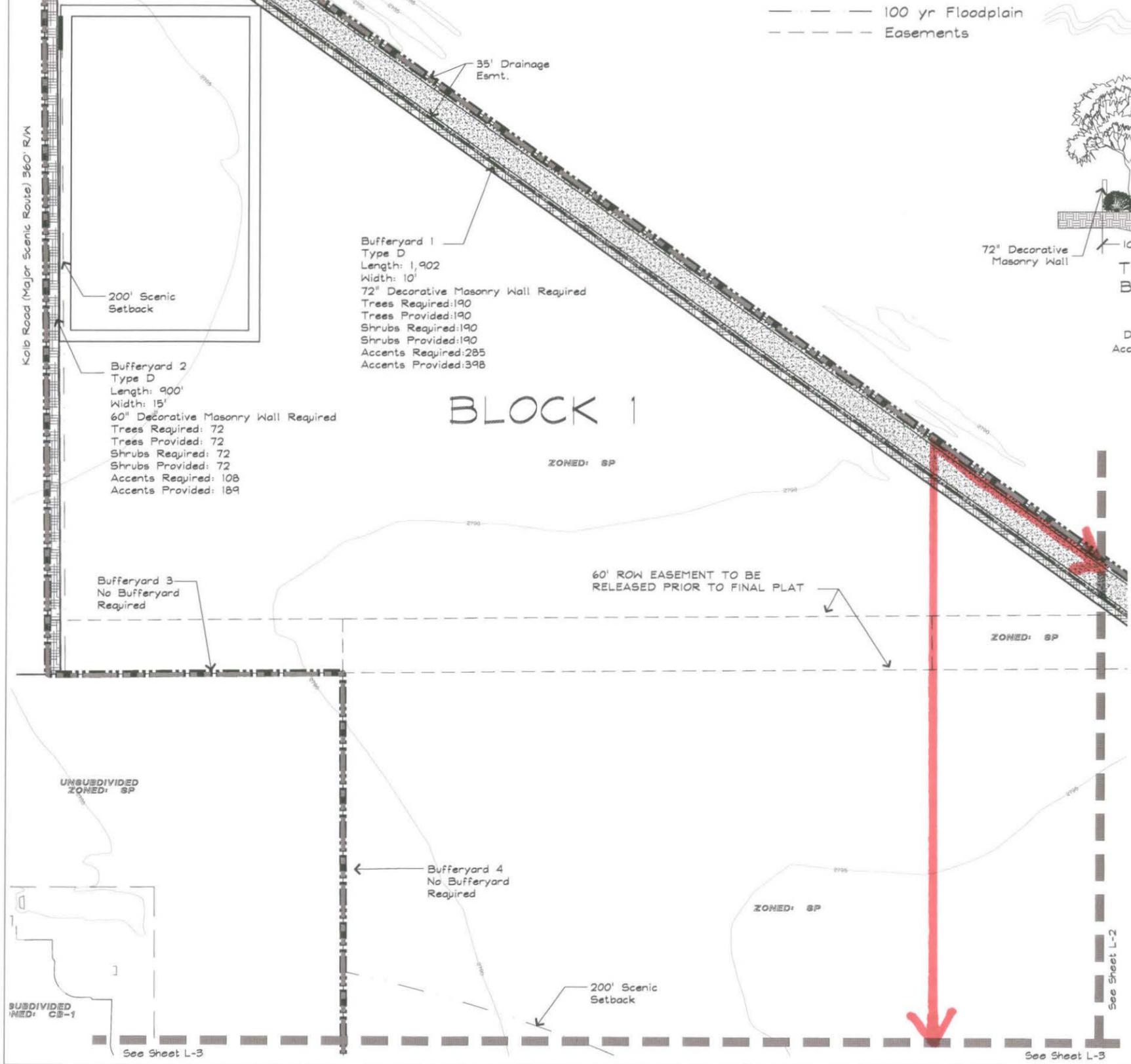
Block 1 Mitigation Calculations

MITIGATION CALCULATIONS						
	T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	JPR Credits	Net Mitigation
Trees						
Acacia constricta	4	8	3	9	0	17
Cercidium floridum	5	10	4	12	0	22
Cercidium microphyllum	17	34	16	48	0	82
Prosopis juliflora	5	10	5	15	0	25
Shrubs						
Celtis pallida	8	8	7	14	0	22
Cacti						
Ferocactus wislizeni	403	403	0	0	0	403
Succulents						
Yucca elata	0	0	1	2	0	2



CALL TWO WORKING DAYS BEFORE FOR DIS 602-263-1100 1-800-STAKE-IT (OUTSIDE MARICOPA COUNTY)

Reference No. P1208-035
Co23-88-01
Co9-01-11
Co23-08-01



Bufferyard 1
Type D
Length: 1,902
Width: 10'
72" Decorative Masonry Wall Required
Trees Required:190
Trees Provided:190
Shrubs Required:190
Shrubs Provided:190
Accents Required:285
Accents Provided:398

Bufferyard 2
Type D
Length: 900'
Width: 15'
60" Decorative Masonry Wall Required
Trees Required: 72
Trees Provided: 72
Shrubs Required: 72
Shrubs Provided: 72
Accents Required: 108
Accents Provided: 189

Bufferyard 3
No Bufferyard Required

Bufferyard 4
No Bufferyard Required

Novak Environmental, Inc.
4574 North First Avenue, Suite #100
Tucson, AZ 85718
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Landscape Mitigation Plan
Pima County

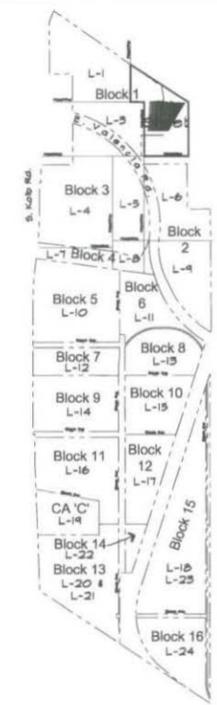


DESIGNED BY: jrp:bst
DRAWN BY: jrp:bst
CHECKED BY: kmc
DATE: April 2008
JOB NO.: 07091
SHEET NUMBER

L-1
59 of 85

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Landscape Plans Century Park Research Center, Phase II Block 1



SHEET LAYOUT
Note: Sheets for Block 5-16 are laid out by block.

REVISIONS / SUBMITTALS
 ▲ Revised per comments 7/2/09
 △ Revised per comments 10/1/08

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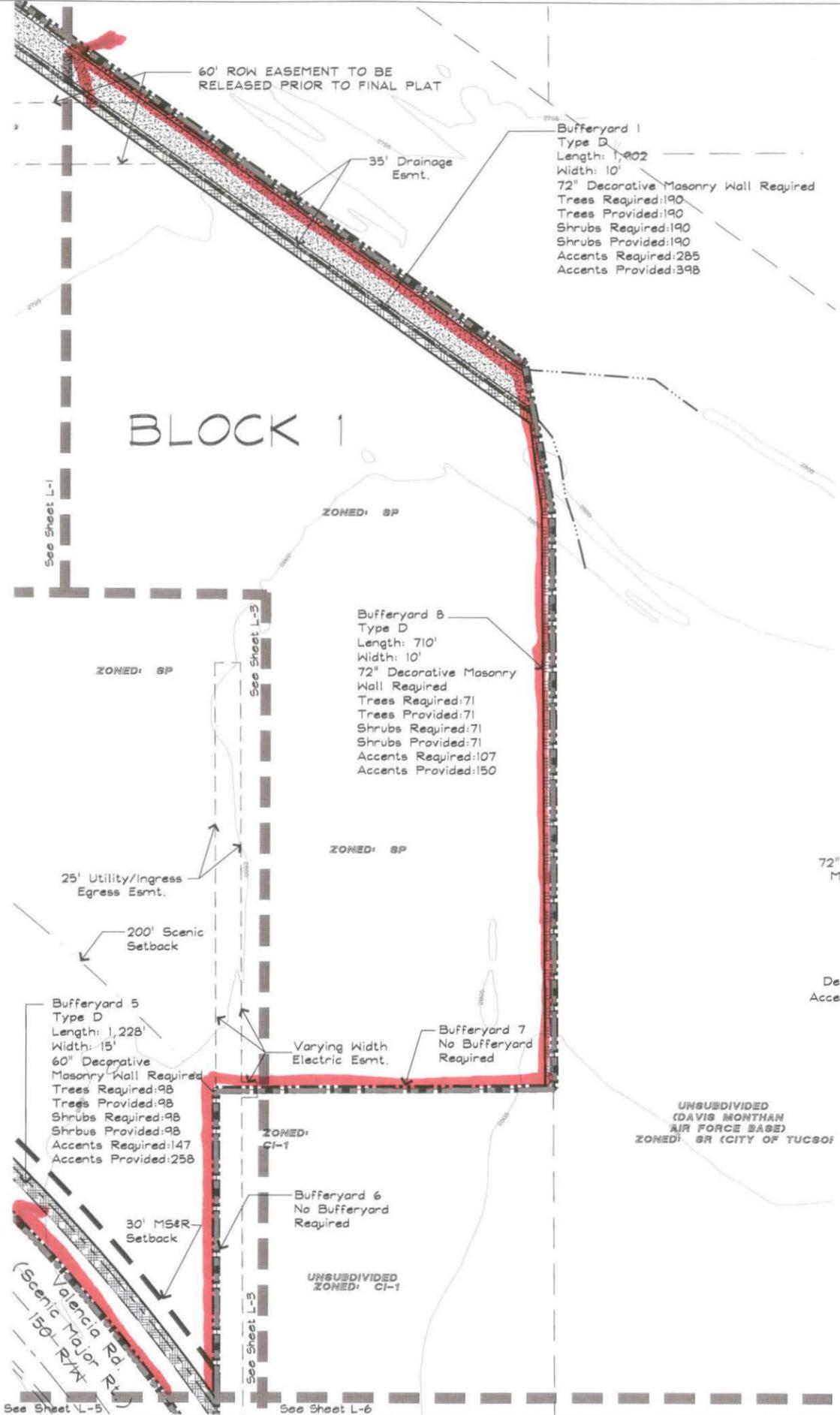


**Century Park Research Ctr-Ph II
 Landscape Mitigation Plan
 Pima County**

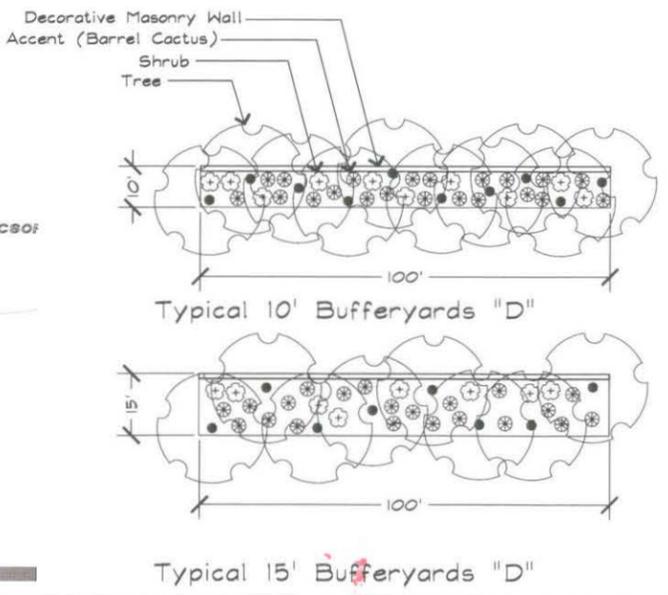
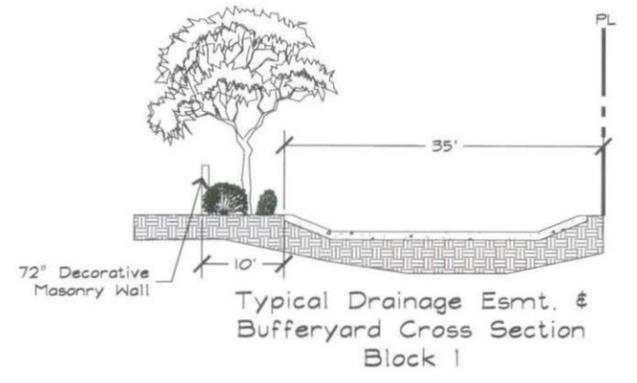


DESIGNED BY: jhp/ptf
 DRAWN BY: jhp/ptf
 CHECKED BY: kmc
 DATE: April 2008
 JOB NO.: 07041
 SHEET NUMBER

L-2
 60 of 85

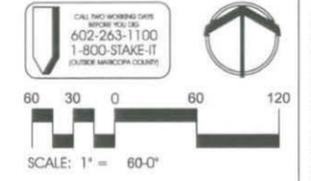


- Legend**
- Project Boundary
 - Block Boundary
 - Bufferyard
 - Xeroriparian "C"
 - 100 yr Floodplain
 - Easements
 - 200' Scenic Gateway Setback
 - 30' MS&R Setback
 - Matchline
 - Drainage Channel
 - Existing Contours



Block 1 Mitigation Calculations

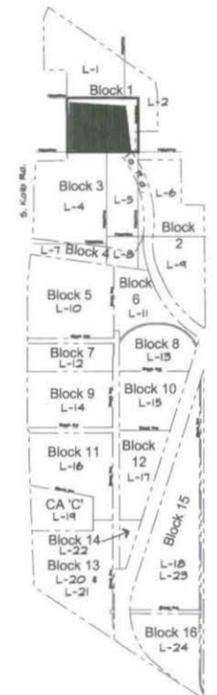
MITIGATION CALCULATIONS							Net Mitigation	SIZE
Trees	T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	HP Credits			
Acacia constricta	4	8	3	9	0	17	(14) 15-gal (3) 24"box	
Cardidium floridum	5	10	4	12	0	22	(18) 15-gal (4) 24"box	
Cardidium microphyllum	17	34	16	48	0	82	(66) 15-gal (18) 24"box	
Prosopis velutina	5	10	5	15	0	25	(20) 15-gal (5) 24"box	
Shrubs								
Callis pallida	8	8	7	14	0	22	5-gal	
Cacti								
Ferocactus wislizenii	403	403	0	0	0	403	(403) from site (0) >4" H or D	
Succulents								
Yuca elata elata	0	0	1	2	0	2	(1) from site, (1) 5-gal	



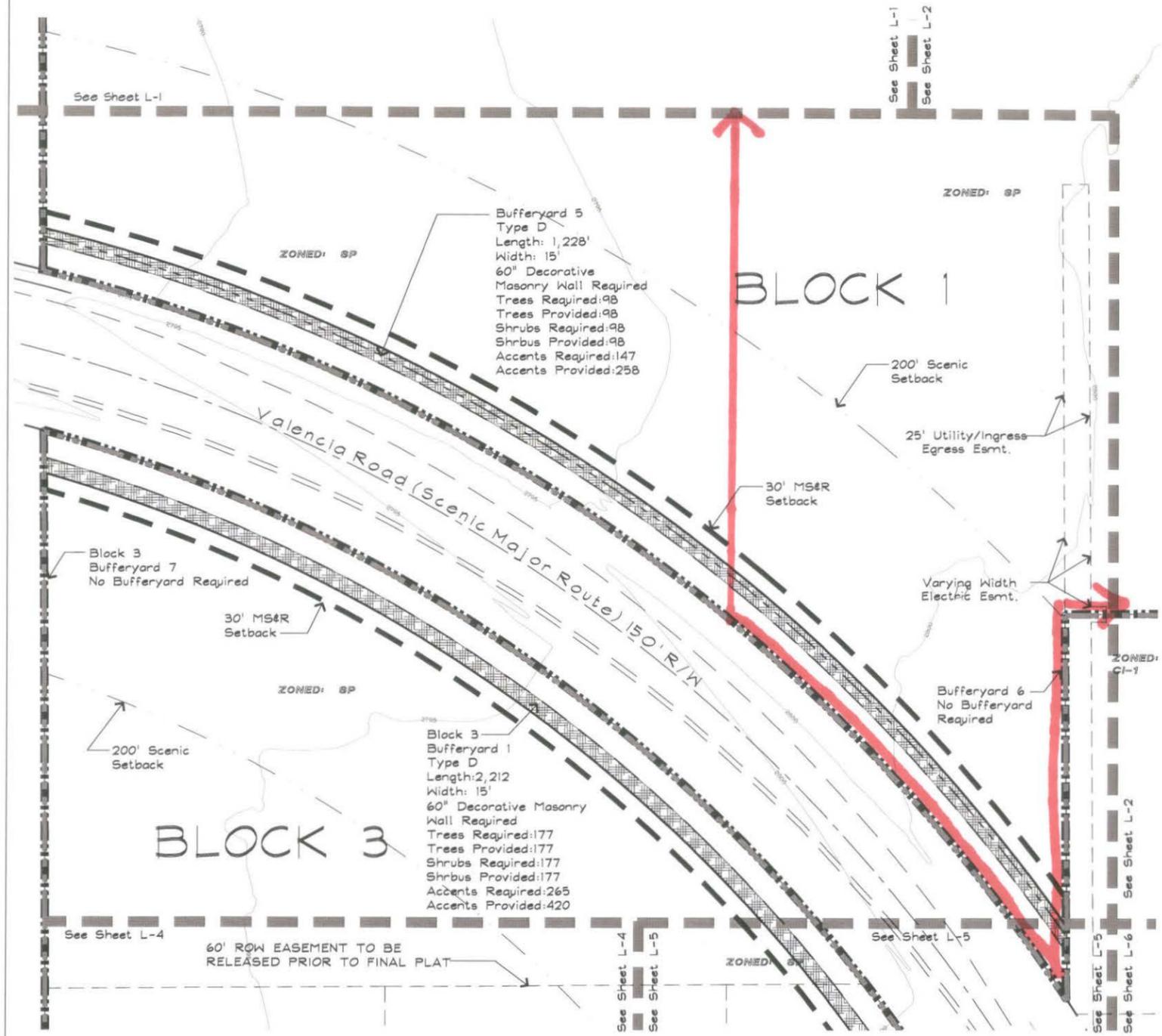
Reference No. **P1208-035**
 Co23-88-01
 Co9-01-11
 Co23-08-01

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Landscape Plans Century Park Research Center, Phase II Block 1&3



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Note: Sheets for Block 5-16 are laid out by block.



Legend

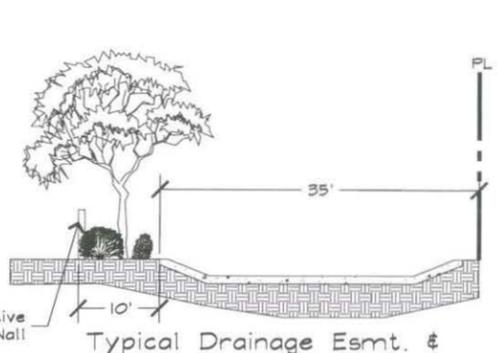
- Project Boundary
- Block Boundary
- Bufferyard
- Xeroriparian "C"
- 100 yr Floodplain
- Easements
- 200' Scenic Gateway Setback
- 30' MS&R Setback
- Matchline
- Existing Contours

Block 1 Mitigation Calculations

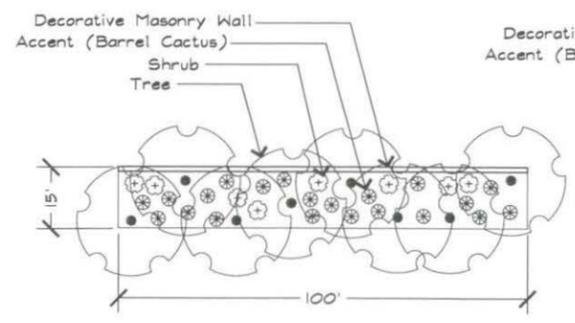
MITIGATION CALCULATIONS		T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	PP Credits	Net Mitigation	SIZE
Trees	Acacia constricta	4	8	3	9	0	17	(14) 15-gal (3) 24" box
	Cerodidum floridum	5	10	4	12	0	22	(18) 15-gal (4) 24" box
	Cerodidum microphyllum	17	34	16	48	0	82	(86) 15-gal (16)
	Prosopis juliflora	5	10	5	15	0	25	(20) 15-gal (5) 24" box
	Cacti	-# (saguaro)-# (saguaro)						
Shrubs	Celtis pallida	8	8	7	14	0	22	5-gal
	Succulents							(1) from site. (1) 5-gal
	Fouquieria splendens	1	1	0	0	0	1	(0) >2" from site. (1) >=4" from nursery

Block 3 Mitigation Calculations

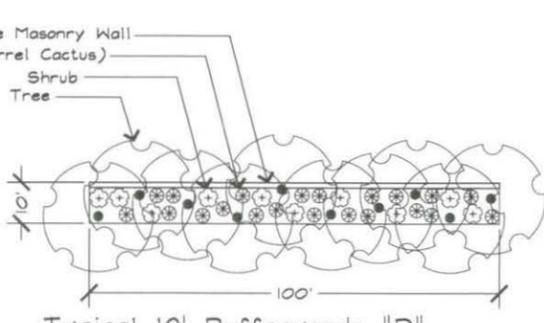
MITIGATION CALCULATIONS		T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	PP Credits	Net Mitigation	SIZE
Trees	Acacia constricta	1	2	1	3	0	5	(4) 15-gal (1) 24" box
	Cerodidum floridum	16	32	15	45	0	77	(82) 15-gal (15)
	Cerodidum microphyllum	35	70	34	102	0	172	(138) 15-gal (34) 24" box
	Prosopis juliflora	7	14	6	18	0	32	(26) 15-gal (6) 24" box
	Shrubs	Celtis pallida	41	41	41	82	0	123
Cacti		-# (saguaro)-# (saguaro)						(474) from site. (1) >=4" H or D
Succulents	Fouquieria splendens	1	1	0	0	0	1	(0) >2" from site. (1) >=4" from nursery



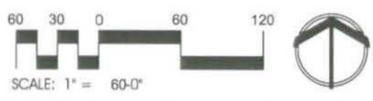
Typical Drainage Esmt. & Bufferyard Cross Section Block 1



Typical 15' Bufferyards "D" Block 1 (See L-4 & L-5 for Block 3)



Typical 10' Bufferyards "D" Block 1



Reference No. **P1208-035**
Co23-88-01
Co9-01-11
Co23-08-01

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Landscape Mitigation Plan
Pima County**

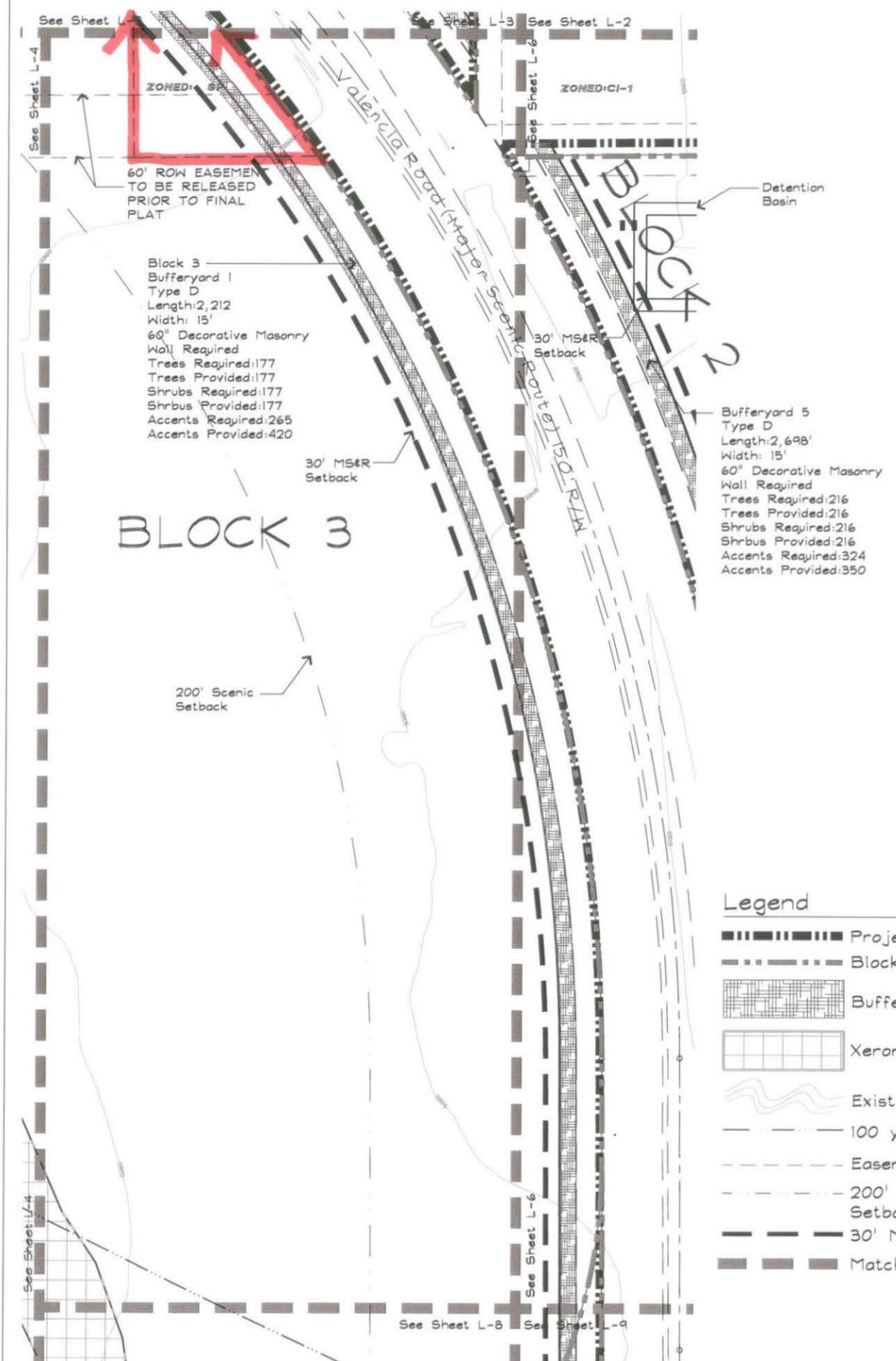


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DRAWN BY: jhp/bst
CHECKED BY: kmc
DATE: April 2008
JOB NO.: 07091

SHEET NUMBER
L-3
61 of 85

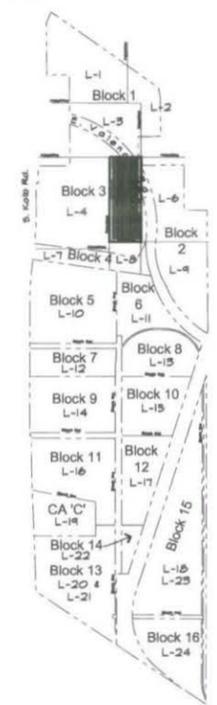
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 △ Revised per comments 7/21/08
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Landscape Plans Century Park Research Center, Phase II Block 3



Block 3 Mitigation Calculations

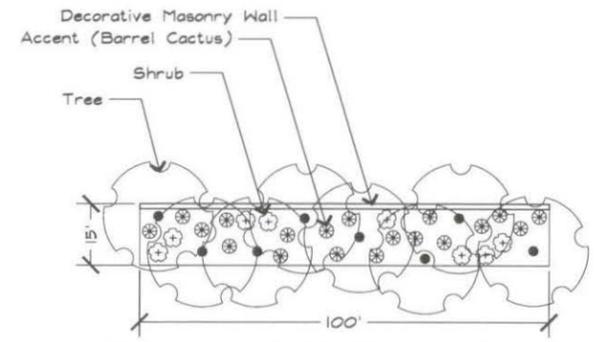
MITIGATION CALCULATIONS		T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	TPP Credits	Net Mitigation	SIZE
Trees	Acacia constricta	1	2	1	3	0	5	(4) 15-gal (1) 24" box (62) 15-gal (15)
	Cercidium floridum	16	32	15	45	0	77	24" box (138) 15-gal (34)
	Cercidium mitrophyllum	35	70	34	102	0	172	24" box (26) 15-gal (8) 24" box
	Prosopis juliflora	7	14	6	18	0	32	
Shrubs	Acacia constricta	1	1	1	2	0	3	5-gal
	Celtis pallida	41	41	41	82	0	123	5-gal
Cacti	-if (saguaro) =if (saguaro)							(474) from site (1)
	Ferocactus wislizenii	475	475	0	0	0	475	>=4" H or D
Succulents	Fouquieria splendens	1	1	0	0	0	1	(0) >2' from site (1) >=4' from nursery



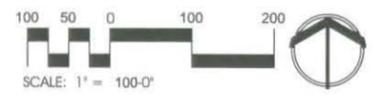
SHEET LAYOUT
Note: Sheets for Block 5-16 are laid out by block.

Legend

- Project Boundary
- Block Boundary
- Bufferyard
- Xeroriparian "C"
- Existing Contours
- 100 yr Floodplain
- Easements
- 200' Scenic Gateway Setback
- 30' MS&R Setback
- Matchline



Typical 15' Bufferyards
(No 10' Bufferyards for this block)



CALL TWO MORNING CARS
602-263-1100
1-800-STAKE-IT
(EXCEPT MONDAY MORNING)

Reference No. P1208-035
Co23-BB-01
Co9-01-11
Co23-08-01

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Tucson, AZ 85718
Tel: 520.206.0591
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Century Park Research Ctr-Ph II
Landscape Mitigation Plan
Pima County, AZ

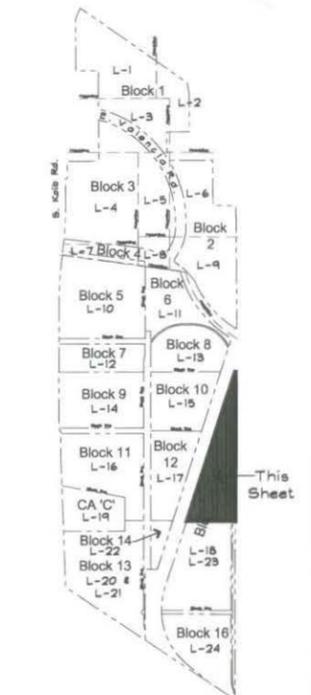
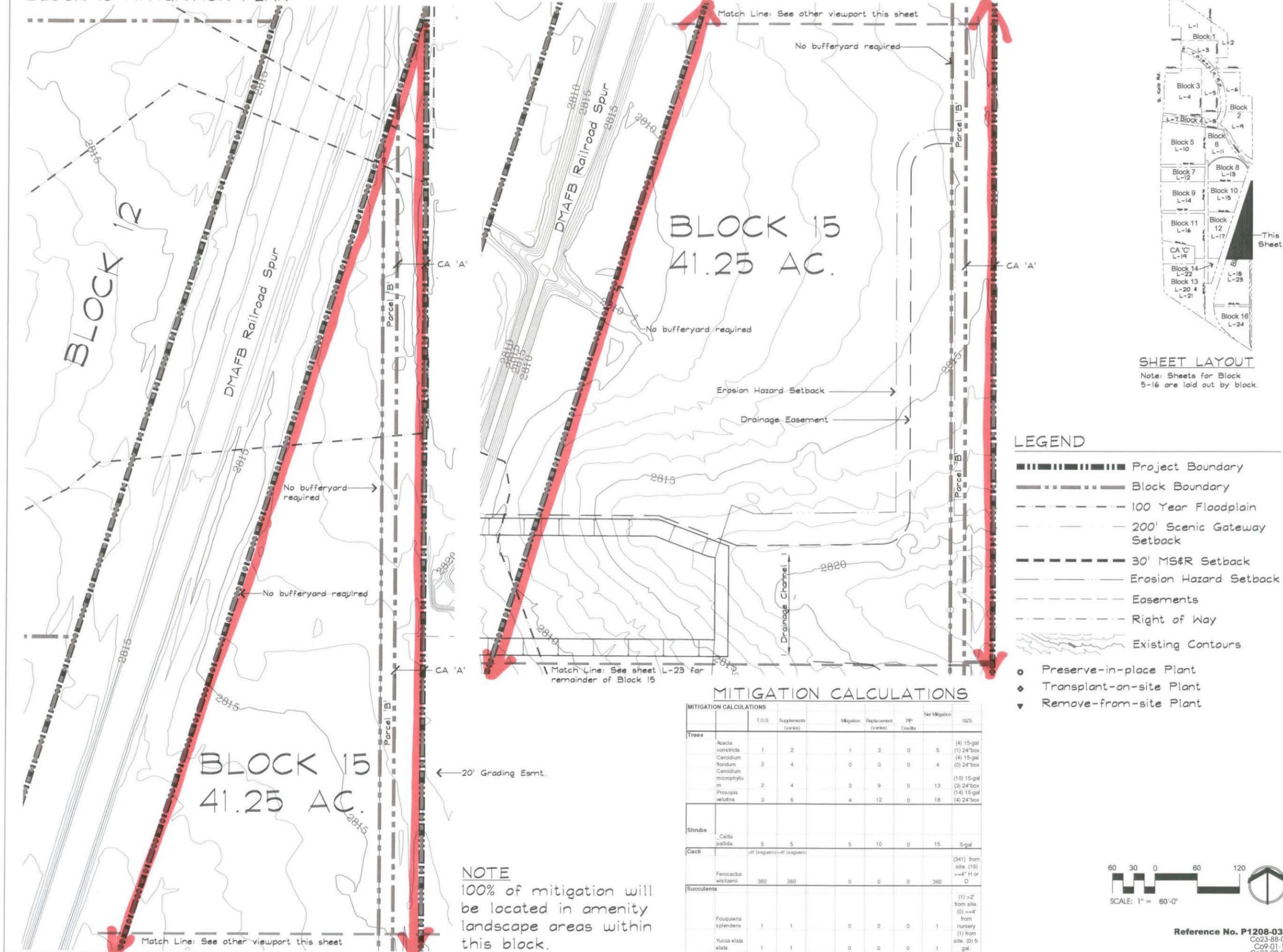


DESIGNED BY: jrp/mtf
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CHECKED BY: kmc
DATE: April 2008
JOB NO.: 070471

SHEET NUMBER
L-5
63 OF 85

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BLOCK 15 MITIGATION PLAN



SHEET LAYOUT
Note: Sheets for Block 5-16 are laid out by block.

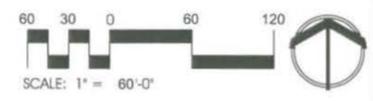
LEGEND

- Project Boundary
 - Block Boundary
 - 100 Year Floodplain
 - 200' Scenic Gateway Setback
 - 30' MS&R Setback
 - Erosion Hazard Setback
 - Easements
 - Right of Way
 - Existing Contours
- Preserve-in-place Plant
 - Transplant-on-site Plant
 - Remove-from-site Plant

MITIGATION CALCULATIONS

MITIGATION CALCULATIONS		T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	pp Credits	Net Mitigation	SIZE
Trees	Acacia constricta	1	2	1	3	0	5	(4) 15-gal (1) 24"box
	Ceroidium floridum	2	4	0	0	0	4	(4) 15-gal (0) 24"box
	Ceroidium microphyllum	2	4	3	9	0	13	(10) 15-gal (3) 24"box
	Prosopis juliflora	3	6	4	12	0	18	(14) 15-gal (4) 24"box
	Shrubs	Celtis pallida	5	5	5	10	0	15
Cacti	Ferocactus wislizenii	360	360	0	0	0	360	(341) from site, (19) >=4" H or D
Succulents	Fouquieria splendens	1	1	0	0	0	1	(1) >2' from site, (0) >=4' from nursery
	Yucca elata	1	1	0	0	0	1	(1) from site, (0) 5-gal

NOTE
100% of mitigation will be located in amenity landscape areas within this block.



Reference No. **P1208-035**
Co23-88-01
Co9-01-11
Co23-08-01

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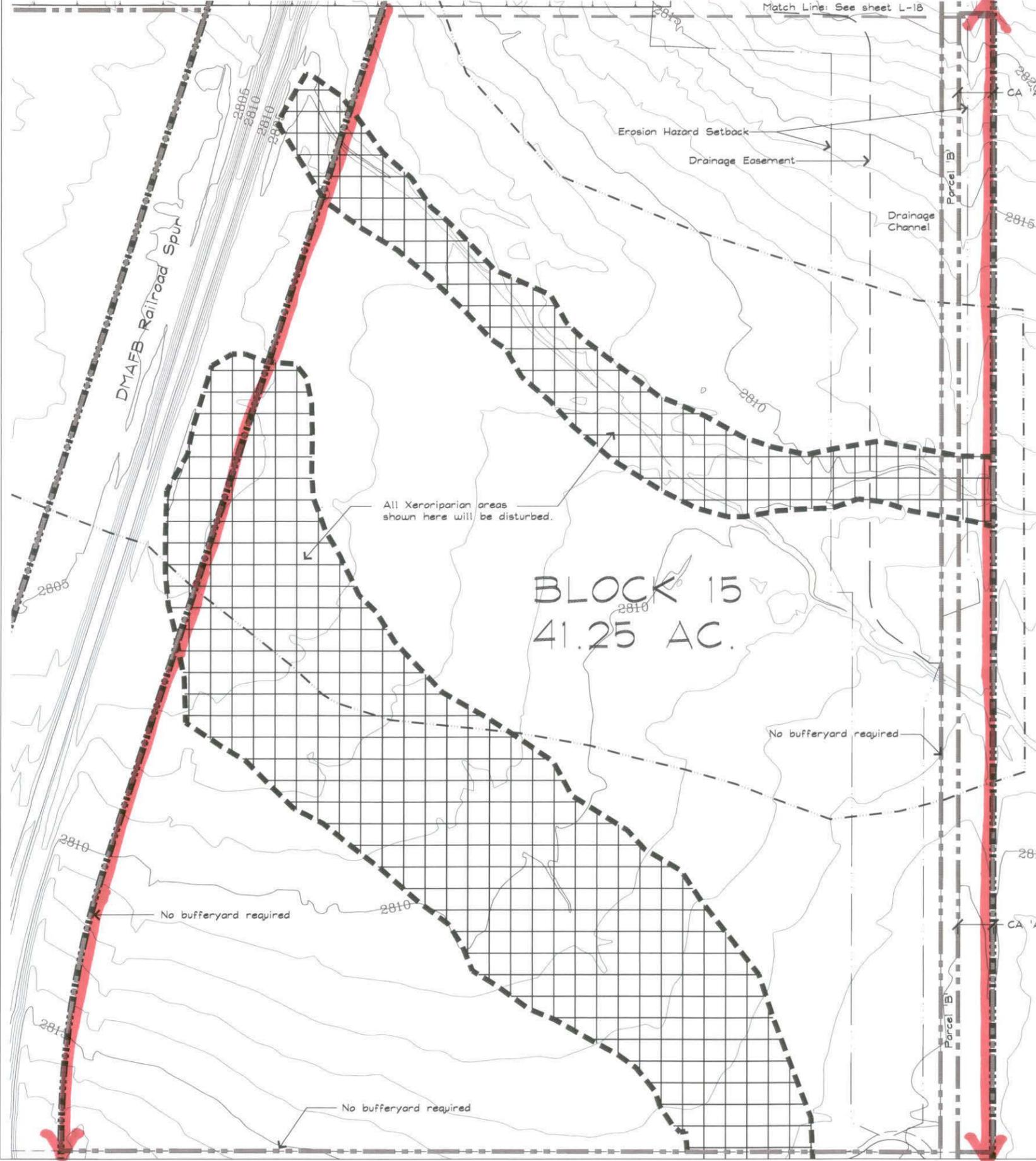
Century Park Research Ctr-Ph II
Landscape Mitigation Plan
Pima County



DESIGNED BY: acf
DRAWN BY: acf
CHECKED BY: kmc
DATE: Feb. 2008
JOB NO.: NE-07102
SHEET NUMBER
L-18
76 of 85

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BLOCK 15 MITIGATION PLAN



LEGEND

- Project Boundary
- Block Boundary
- 100 Year Floodplain
- Erosion Hazard Setback
- Right of Way
- Field Verified Xeroriparian 'C' Habitat
Area: 8.26 acres
Amount Disturbed: 8.26 acres
- Existing Contours

SHEET LAYOUT

Note: Sheets for Block 5-16 are laid out by block.

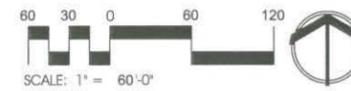


MITIGATION CALCULATIONS

	T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	PP Credits	Net Mitigation	SIZE
Trees							
Acacia constricta	1	2	1	3	0	5	(4) 15-gal (1) 24" box
Cercidium floridum	2	4	0	0	0	4	(4) 15-gal (0) 24" box
Cercidium microphyllum	2	4	3	9	0	13	(10) 15-gal (3) 24" box
Prosopis velutina	3	6	4	12	0	18	(14) 15-gal (4) 24" box
Shrubs							
Celtis pallida	5	5	5	10	0	15	5-gal
Cacti							
Ferocactus wislizenii	360	360	0	0	0	360	(341) from site. (19) >=4" H or D
Succulents							
Fouquieria splendens	1	1	0	0	0	1	(1) >2' from site. (0) >=4' from nursery
Yucca elata	1	1	0	0	0	1	(1) from site. (0) 5-gal.

NOTE

100% of mitigation will be located in amenity landscape areas within this block.



Reference No. P1208-035
Co23-88-01
Co9-01-11
Co23-08-01

REVISIONS / SUBMITTALS
 Revised per Comments 1-21-08
 Revised per Comments 10-1-08

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 Tel 520.206.0591
 Fax 520.882.3006
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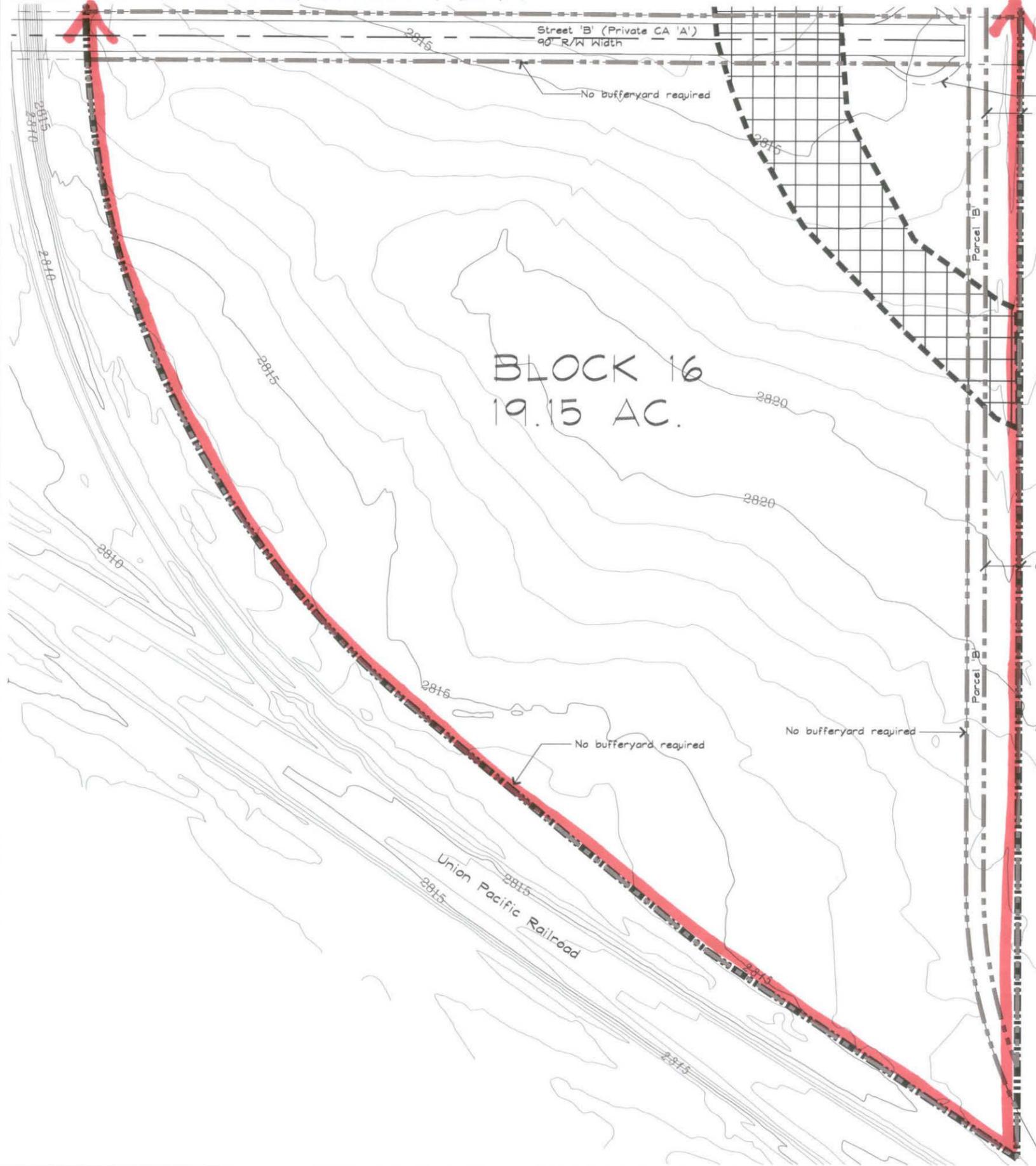
Century Park Research Ctr-Ph II
 Landscape Mitigation Plan
 Pima County



DESIGNED BY: acf
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 CHECKED BY: kmc
 DATE: Feb. 2008
 JOB NO.: NE-07102
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L-23
 81 of 85

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BLOCK 16 MITIGATION PLAN



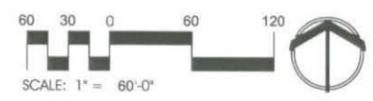
LEGEND

- Project Boundary
- Block Boundary
- 100 Year Floodplain
- Erosion Hazard Setback
- Right of Way
- Easements
- Field Verified Xeroriparian 'C' Habitat
Area: 8.26 acres
Amount Disturbed: 8.26 acres
- 1' Contours

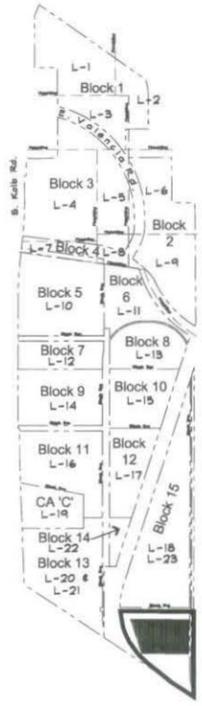
MITIGATION CALCULATIONS

	T.O.S.	Supplements (varies)	Mitigation	Replacement (varies)	PIP Credits	Net Mitigation	SIZE
Trees							
Shrubs							
Cacti							
	<6' (saguaro)	>=6' (saguaro)					(62) from site. (1) >=4" H or D
Ferocactus wislizenii	63	63	0	0	0	63	
Succulents							
Fouquieria splendens	1	1	0	0	0	1	(1) >2' from site. (0) >=4' from nursery

NOTE
100% of mitigation will be located in amenity landscape areas within this block.



Reference No. P1208-035
Co23-88-01
Co9-01-11
Co23-08-01



SHEET LAYOUT
Note: Sheets for Block 5-16 are laid out by block.

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REVISIONS / SUBMITTALS
 Revised per comments 1-21-08
 Revised per comments 10-1-08

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Century Park Research Ctr-Ph II
Landscape Mitigation Plan
 Pima County



DESIGNED BY: acj
 DRAWN BY: acj
 CHECKED BY: kmc
 DATE: Feb. 2008
 JOB NO.: NE-07102

SHEET NUMBER
L-24
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