

La Cholla Boulevard Ruthrauff Road to River Road

Final Design Concept Report

October 2008

Pima County Department of Transportation
Project No. 4LCITR



DRAFT

October 08, 2008

Mr. Dean Papajohn, PE
Civil Engineering Manager
Pima County Department of Transportation
Public Works Building
201 N. Stone Avenue, 4th Floor
Tucson, AZ 85701-1207

RE: Final Design Concept Report
La Cholla Boulevard
Ruthrauff to River Road
Project No. 4LCITR
HDR Job No. 59914

Dear Mr. Papajohn:

We are pleased to submit the Final Design Concept Report for the above-referenced project. This report was prepared by Robert Brittain, PE and was reviewed by Ted Buell, PE.

Sincerely,
HDR Engineering, Inc.

Ted W. Buell, PE
Project Manager

Attachments

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Prepared for:
Pima County Department of Transportation
201 N. Stone Avenue
Tucson, AZ 85701
Project No. 4LCITR

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APPROVED:

Pima County Director of Transportation

Date



Executive Summary

This project involves the reconstruction of La Cholla Boulevard from Ruthrauff Road to River Road from a two-lane roadway into a six-lane arterial street. The entire project is located in unincorporated Pima County. Location and vicinity maps are included as Figures 1 and 2.

On May 16, 2006, the citizens of Pima County approved a \$2.1 billion transportation plan to be funded by a one-half cent increase in the sales tax. Current project funding includes \$14,760,000 from this sales tax revenue. Other funding includes \$3,691,000 in Highway User Revenue Fund (HURF) revenue and \$1,549,000 in Pima County Impact Fees for total funding of \$20 million. It is estimated that this project will be advertised for bids in the spring of 2010. Construction is anticipated to be complete in late 2011.

This section of La Cholla Boulevard is slated to be a major arterial roadway in the Pima Association of Governments Metropolitan Transportation Plan. The widening is needed to accommodate the heavy volumes of traffic projected in the year 2030. There are also vertical deficiencies in the existing roadway profile which limit stopping sight distances. Constructing this new arterial roadway to current standards will increase traffic capacity and user safety in all weather conditions.

The design speed for this project is 50 mph. It will be posted at 45 mph. The existing right-of-way width of 150 feet will be sufficient for the six-lane roadway. Landscaping will be provided in the median and parkway areas. Artwork will also be included with this project.

Drainage improvements will include a new six-lane bridge over the Rillito River, and a storm drain system consisting of catch basins and pipe culverts capable of conveying a 50-year storm. No box culverts and only minimal channel work are anticipated.

Driveways will be provided to every property that currently has access to La Cholla Boulevard. Median openings will be provided at all side streets except Noreen Street which is too close to Ruthrauff Road to have an opening. Existing traffic signals will be modified at the intersections of River Road, Curtis Road and Ruthrauff Road. No new traffic signals will be added.

An alignment alternatives study was performed to determine how to handle access to the single family homes located at the southern end of the project. The following six main alignment alternatives were considered:

Alternative A

This alternative involves shifting the La Cholla Boulevard centerline to the east about 40 feet. This provides enough room to build a frontage road to serve the homes on the west side. It would require the purchase of 13 lots on the east side along with some partial takes from other properties.

Alternative B

This alternative involves shifting the La Cholla Boulevard centerline to the west about 40 feet. This provides enough room to build a frontage road to serve the homes on the east side. It would require the purchase of 12 lots on the west side including a City well site along with some partial takes from other properties.



Alternative C

This alternative involves maintaining La Cholla Boulevard centered on the section line. The new road would include an additional pullout lane outside of the bike lane to provide space for the homeowners to back out a car without entering the La Cholla Boulevard travel lanes. This alternative was dropped for safety concerns.

Alternative D

This alternative involves maintaining La Cholla Boulevard centered on the section line similar to Alternative C, however, no pullout lanes nor frontage roads would be used to provide space for the homeowners to back out a car. Therefore, the fronting residences on both sides of La Cholla Boulevard would need to be acquired. However, no other right of way would be needed.

Alternative E

This alternative was created to provide an option in which right-of-way was not required from either side of La Cholla Boulevard. It involves maintaining La Cholla Boulevard centered on the section line. The median, travel and bike lanes are reduced in width. One-way frontage roads are used on both sides of La Cholla Boulevard. While no right-of-way must be purchased from the fronting residences, it will be necessary to acquire temporary construction easements to build the small slopes and reconstruct the driveways.

Alternative F

This alternative also maintains La Cholla Boulevard centered on the section line with the same typical section as Alternative D. However, instead of purchasing all of the homes on both sides, only eight properties would be bought. These purchased properties would be used to build circular driveways for access to the remaining homes.

Variations on Alternative E

Several variations on Alternative E were also considered and rejected. These variations included the following:

- Replacing the median with a continuous two-way, left-turn lane.
- Replacing the frontage roads with direct driveways.
- Providing additional openings in the frontage road medians along with left turn bays in the main median.
- Widening the frontage roads from 16 feet to 20 feet so they could function as two-way. The median would then be reduced from 18 feet to 10 feet.
- Reducing the separating medians from four feet to two feet, increasing the frontage road widths to 18 feet and making them two-way.

Each of these variations had safety or budgetary problems that made them unworkable. Based on the design requirements, public input and financial constraints, PCDOT recommends Alternative E.



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- Appendix A – Construction Cost Estimate
- Appendix B – Community Advisory Committee and Public Involvement Information
- Appendix C – Stage I (15%) Construction Plans (Bound Separately)
- Appendix D – Right-of-Way Requirements Plans (Bound Separately)



1.0 Project Overview

1.1 Project Location

This project is located along La Cholla Boulevard from Ruthrauff Road to River Road. Reconstruction of the Ruthrauff Road intersection will involve the addition of turn lanes and tapers along Ruthrauff Road and on La Cholla Boulevard south of Ruthrauff Road. The official title of this project is “La Cholla Boulevard, Ruthrauff Road to River Road”. The Project No. is 4LCITR. This project involves the reconstruction of La Cholla Boulevard from a two-lane roadway into a six-lane arterial street. The entire project is located in unincorporated Pima County. A location map is included as Figure 1 and a vicinity map is included as Figure 2.

1.2 Authorization

The Pima County Board of Supervisors approved the contract for the design of this project in May, 2007. The notice to proceed was issued by the Director of the Department of Transportation on June 12, 2007. On May 16, 2006, the citizens of Pima County approved a \$2.1 billion transportation plan to be funded by a one-half cent increase in the sales tax. Current project funding includes \$14,760,000 from this sales tax revenue. Other funding includes \$3,691,000 in Highway User Revenue Fund (HURF) revenue and \$1,549,000 in Pima County Impact Fees for total funding of \$20 million.

1.3 Previous Work

The most recent work on this section of La Cholla Boulevard includes the construction of the current roadway grade and the bridge across the Rillito River in 1981, and the installation of a major storm drain in 1984. The section of La Cholla Boulevard from the bridge to River Road was built with the River Road to Omar Drive section completed in 2006.

Since the new roadway will follow the existing alignment, a Location Report was not required. A Final Traffic Report was prepared by Kimley-Horn and submitted to PCDOT on February 20, 2008. An Environmental Screening Matrix and Memorandum were submitted on November 15, 2007. It is anticipated that the Final Environmental Assessment and Mitigation Report will be sent to the Board of Supervisors for approval in December 2008. Construction is anticipated to start within the first five years of the Regional Transportation Authority (RTA) plan, 2010 if no delays occur.

1.4 Project Need

This section of La Cholla Boulevard is slated to be a major arterial roadway in the Pima Association of Governments Metropolitan Transportation Plan. The widening is needed to accommodate the heavy volumes of traffic projected in the year 2030. There are also vertical deficiencies in the existing roadway profile which limit stopping sight distances. Drainage is a concern today with several complaints concerning ponding problems in the right-of-way. The bridge over the Rillito River has been found to be deficient with regards to scour. Constructing this new arterial roadway to current standards will increase traffic capacity and user safety in all weather conditions.



Figure 1. Location Map

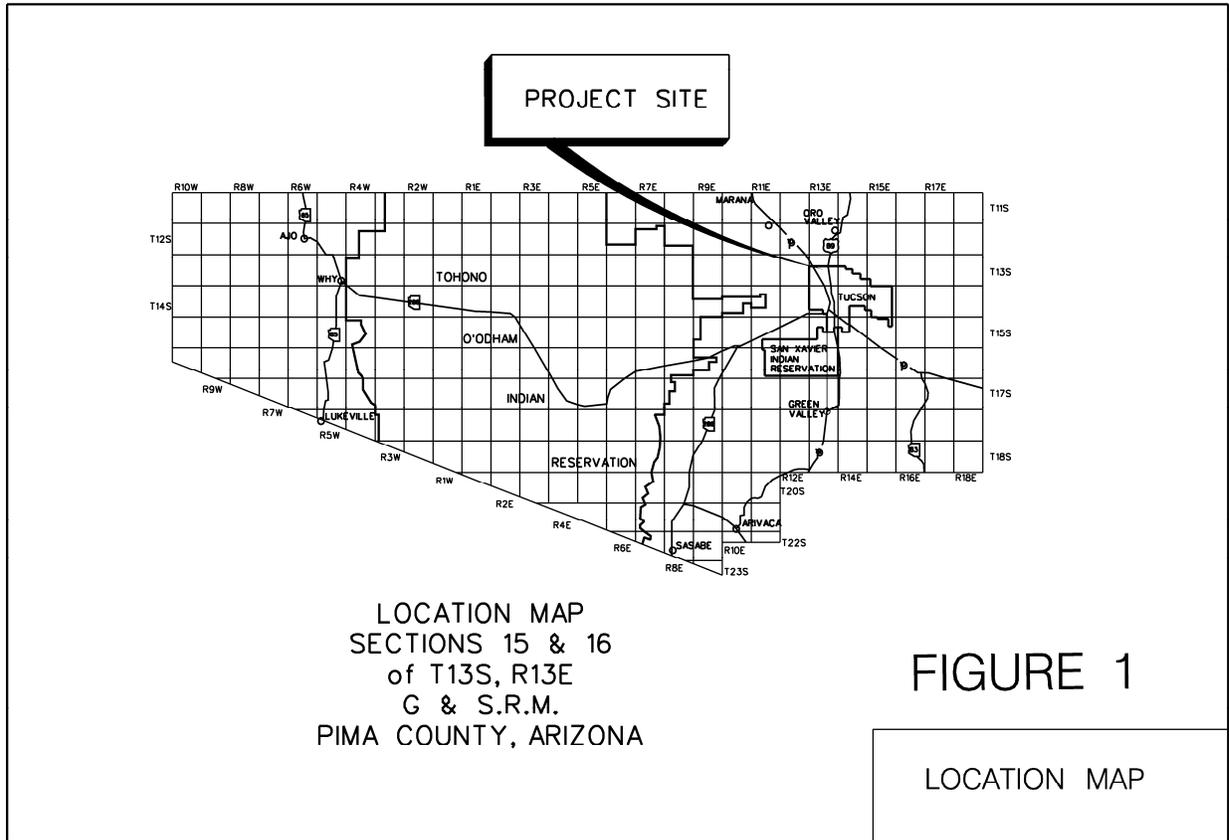
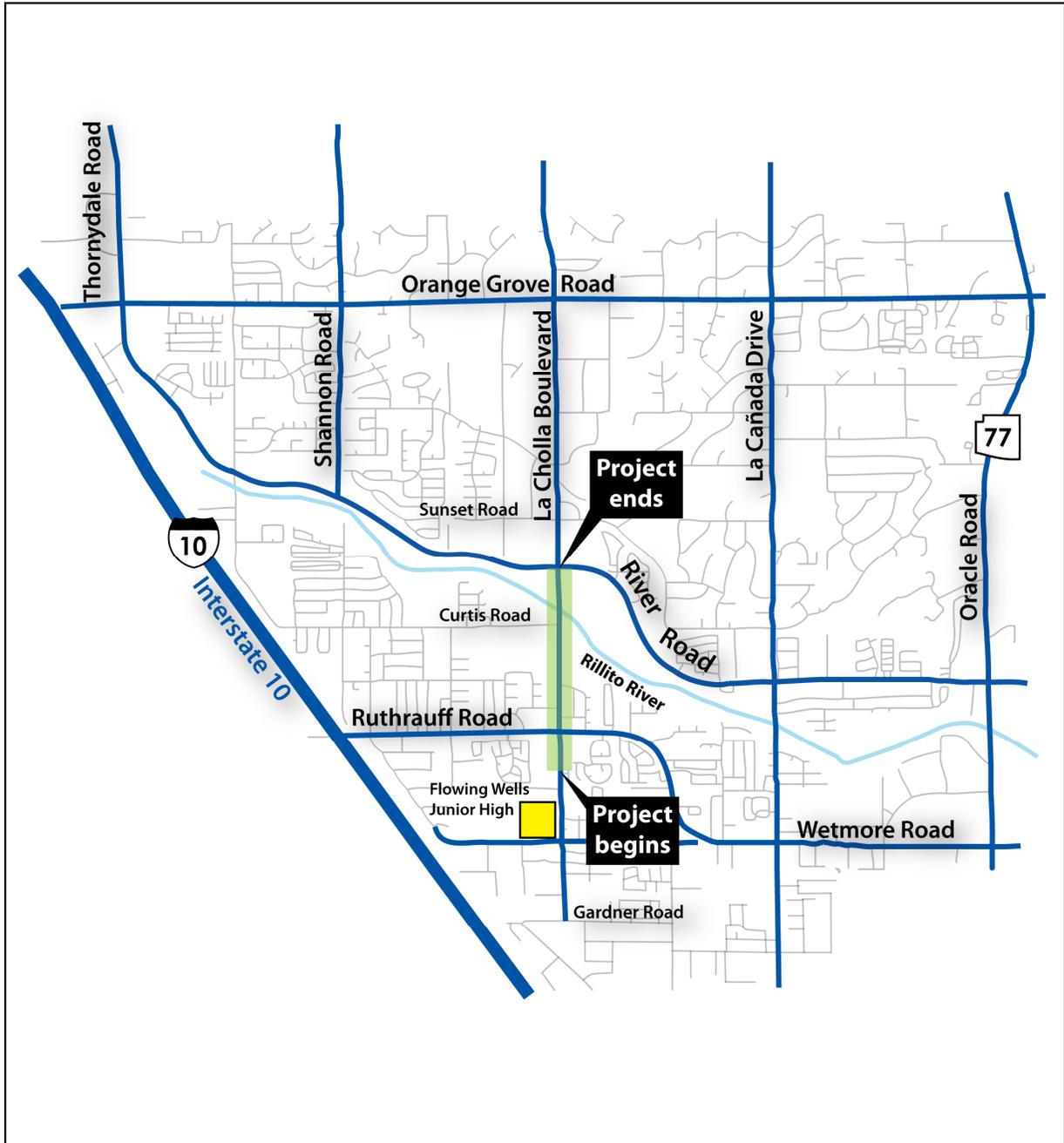




Figure 2. Vicinity Map



Legend

-  Arterial street
-  Interstate highway
-  Project Area



La Cholla Boulevard: Ruthrauff to River
Figure 2. Vicinity Map



2.0 Project Description

2.1 Project Type and Termini

This project involves the complete reconstruction of the road. The new roadway will have six travel lanes, a multi-use lane in each direction, a raised and landscaped median, and sidewalks from La Cholla Boulevard to the north bank of the Rillito River. Pedestrian trail improvements will be made to connect the La Cholla Boulevard sidewalks to the Rillito River trails on both the north and south banks. The project will also include replacing the existing two-lane bridge over the Rillito River with a six-lane bridge. The intersections of La Cholla Boulevard with Curtis Road and Ruthrauff Road will include additional right and left turn lanes. The total length of project is about one mile from River Road south to about 0.3 miles south of Ruthrauff Road. The improvements to Ruthrauff Road will extend about 1000 feet both east and west of La Cholla Boulevard.

2.2 Major Features

The design speed for this project is 50 mph. It will be posted at 45 mph. The existing right-of-way width of 150 feet will be sufficient for the six-lane roadway. The typical roadway section is shown in Figure 3. Landscaping will be provided in the median and parkway areas. Vicki Scuri has been hired by PCDOT to design artwork as part of the improvements.

An alignment alternatives study was performed to determine how to handle access to the single family homes located at the southern end of the project. This study is documented in Chapter 10 of this report. The PCDOT recommended alternative consists of a centered alignment with one-way frontage roads on both sides of the main arterial. To enable these improvements to fit within the existing 150-foot wide right-of-way, the lanes and median were narrowed as shown in Figure 4.

Drainage improvements will include a new six-lane bridge over the Rillito River, and a storm drain system consisting of catch basins and pipe culverts capable of conveying a 50-year storm. No box culverts and only minimal channel work are anticipated.

Existing utilities include 6" and 12" water, 8" and 10" sanitary sewer, 4" natural gas, telephone, cable TV, and overhead electric. It is anticipated that minor relocation of the underground lines will be required along with the overhead electric lines.

Driveways will be provided to every property that currently has access to La Cholla Boulevard. Median openings will be provided at all side streets except Noreen Street which is too close to Ruthrauff Road to have an opening. Existing traffic signals will be modified at the intersections of River Road, Curtis Road and Ruthrauff Road. Street lighting will continue to be provided at signalized intersections only.

Certain features of the project are included to comply with the Americans with Disabilities Act (ADA). These will ensure the project meets the accessibility guidelines. These include sidewalks meeting the minimum width and maximum slope criteria. Curb ramps will be provided at every intersection with a maximum slope of 12:1 and truncated dome warning strips at the bottom of each ramp. Crosswalks will have a maximum cross slope of 2%.



Traffic signals will have wheelchair accessible push buttons. Pads for wheelchair lifts will be provided at each bus stop.

The project will include the installation of landscape improvements in the medians and the roadway shoulders to the right-of-way limit in accordance with the Pima County Department of Transportation Landscape and Irrigation Design Guidelines and the Pima County Roadway Design Manual. The Arizona Community Tree Council's Guide to Arizona Desert Shade Trees will be used as a resource in the selection of shade trees for placement near overhead electric power lines. Placement of landscape improvements will consider the extensive network of existing underground utilities in the project area and follow sight distance requirements.

Figure 3. Typical Roadway Cross-Section

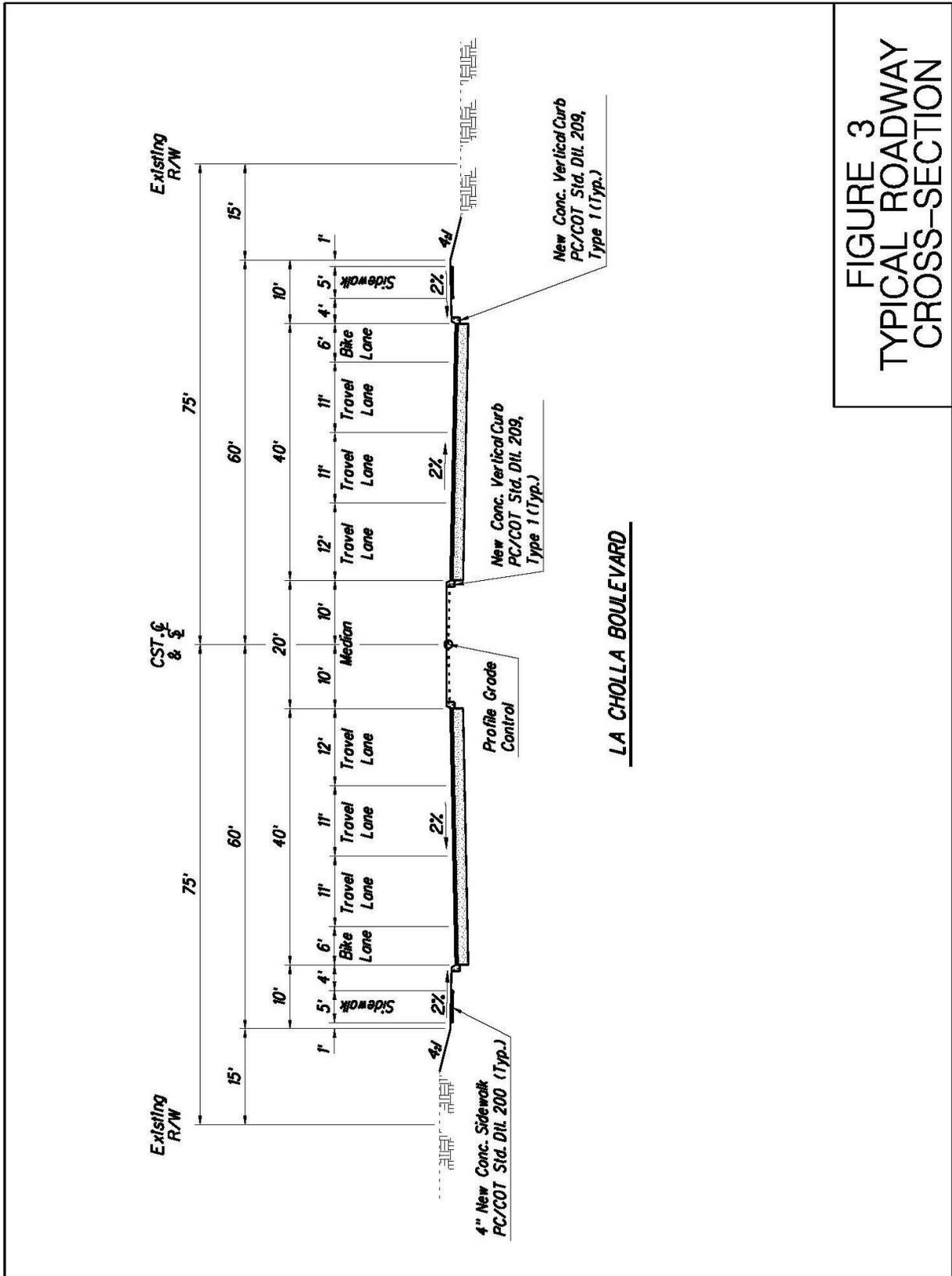
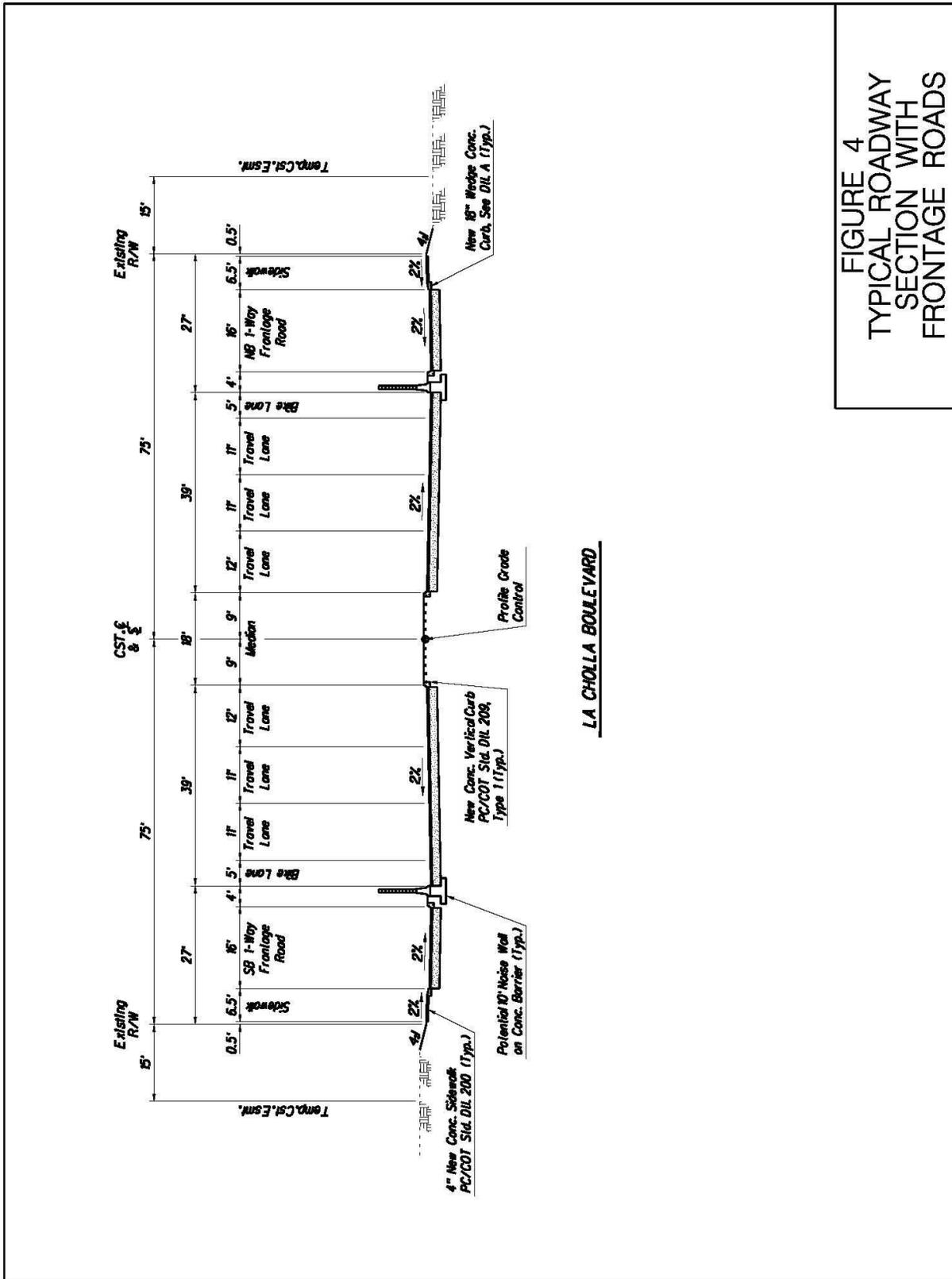




Figure 4. Typical Roadway Section with Frontage Roads





3.0 Project Area Characteristics

3.1 Existing Topography and Terrain

The existing terrain generally slopes gently to the northwest. Along La Cholla Boulevard, the road follows the existing terrain and slopes down from Ruthrauff Road to a low spot about 500 feet south of Curtis Road. From this point north, the road is built on an embankment which rises to a high point on the existing bridge over the Rillito River. The profile then falls to the intersection with River Road. The cross slope along La Cholla Boulevard is very mild with no major existing cut or fill slopes. There are shallow roadside ditches along La Cholla Boulevard for most of its length. The road widening can be accomplished using low 4:1 cut and fill slopes with no retaining walls or slope protection being anticipated.

3.2 Existing Roadway

The existing roadway is uncurbed and has two twelve-foot travel lanes with four-foot paved shoulders for most of its length. The existing surface is asphaltic concrete. The roadway also has a northbound left turn lane and southbound left and right turn lanes at Ruthrauff Road. At Curtis Road there is a northbound left turn lane. The south leg of the River Road intersection was recently built out with curb & gutter and includes six travel lanes, double left turn lanes, a right turn lane, bike lanes, sidewalks and a raised median island. The existing bridge over the Rillito River is 52-foot wide with two 12-foot lanes and two 10-foot shoulders. There is also a six-foot sidewalk on the east side of the bridge.

The existing horizontal alignment is straight with only one set of fairly flat (radius = 7600') reverse curves located just south of Curtis Road. The vertical profile contains gentle grades varying from 0.19% to 1.79%. There is a 350' sag vertical curve located south of Curtis Road and an 800' crest vertical curve over the Rillito River. Both vertical curves meet a 55 mph design speed. The posted speed is 35 mph.

3.3 Existing Rights-of-Way

La Cholla Boulevard has a 150-foot existing right-of-way with the following exceptions:

- On the west side, the first 400 feet south of the River Road centerline has a 100-foot half right-of-way.
- On the east side, from River Road to Curtis Road, there is a 100-foot half of right-of-way.
- On the east side, the first 300 feet south of the Ruthrauff Road centerline has a 100-foot half right-of-way.

Ruthrauff Road has a 150-foot existing right-of-way.



3.4 Existing Drainage

The Rillito River has a drainage area of approximately 900 square miles upstream of La Cholla Boulevard. The Rillito River drains the south portion of the Santa Catalina Mountains, the eastern portion of the Rincon Mountains, as well as several hundred square miles of desert. Watershed elevations range from 2200 to 9200 feet; watershed slopes range from less than 1% to almost vertical relief in the mountains.

The Rillito River, at its crossing with La Cholla Boulevard, is a 300-foot-wide ephemeral stream with lined banks. The streambed is comprised of medium to coarse sand with some gravel and cobbles. The bed is vegetated with a typical assortment of desert vegetation, including mesquite, cottonwood and other desert vegetation. The north and south banks of the existing channel are lined with soil cement. The existing bridge measures 353 feet long from abutment to abutment and is comprised of four equal spans. The bridge is 52 feet wide.

Other than the Rillito River, the main offsite watershed is roughly bordered by the Rillito River on the north, an unnamed wash that runs parallel to and 1300 feet east of La Cholla Boulevard on the east, Wetmore Road on the south, and La Cholla Boulevard on the west. Storm runoff generated within the watershed generally flows to the northwest in streets, roadside swales, and existing storm drains. The watershed is developed with single family homes, mobile home parks and light commercial developments. The vegetative cover consists of natural desert shrub, even in most of the residential areas where property owners have generally elected to maintain the desert appearance of their land in lieu of lawns or formal landscaping.

North of the Rillito River is a small drainage area currently being built into a commercial office center on the southeast corner of La Cholla Boulevard and River Road. The development plan shows that the runoff will be collected and conveyed to the south through the soil cement bank protection directly into the Rillito River. Before the construction of this development this area was drained under La Cholla Boulevard to the west through an existing 24" by 38" horizontally elliptical pipe. This pipe outlets into a drainage channel that flows to the west into the Rillito River. This channel is contained within a public open space & park easement and will be used to outfall the pavement drainage system built as part of this project.

The entire watershed is located on the southern alluvial plain of the Santa Catalina Mountains. The area consists of gently sloping profiles with gradients of less than 1%.

Pavement runoff is conveyed in roadside ditches to the north where it either enters the existing cross drainage system through catch basins (on the east side) or a pipe culvert (on the west side) which directly outfalls into the Rillito River. The ditches are typically quite shallow with minimal capacity.

In 1984, a major storm drain was installed in La Cholla Boulevard from Ruthrauff Road to the Rillito River. At the outfall, the storm drain consists of twin 66-inch reinforced concrete pipes. It was determined, from a discussion with the Pima County Flood Control District, that two ponding/flooding problems exist within the project area: at Noreen Street and Calle Narciso. Both of these problems will be fixed with this project.



3.5 Existing Utilities, Signals, and Lighting

The existing utilities were mapped by Inliner American. They found the following utilities and owners:

Table 1. Existing Utilities

Owner	Utility
Pima County Regional Water Reclamation Department	8", 10", and 15" sanitary sewer
Tucson Water	6", 12", and 16" water and well site
Pima County Parks and Recreation	3" & 6" irrigation (on bridge), Electrical for irrigation controllers (on bridge abutments)
Tucson Electric Power	46 kV and 14 kV overhead electric
Southwest Gas	4" high-pressure and 4" distribution gas
Qwest	Telephone (on bridge)
Xspedius Communications	Fiber optic telephone
Comcast	Cable television
AT&T	Fiber optic telephone
Pima County Traffic	2" Conduit for traffic signals (on bridge)

There are existing traffic signals at the intersections of River Road, Curtis Road and Ruthrauff Road. There are street lights at these intersections, but La Cholla Boulevard does not have continuous street lighting.

3.6 Existing Biology

The project area is located within the Arizona Upland Subdivision of the Sonoran desert scrub biotic community. However, most of the native vegetation has been removed from the right-of-way over the years. The Rillito River is identified by the Pima County Regional Flood Control District as an important riparian resource and serves as a potential wildlife corridor through the project area.

3.7 Archaeological and Historic Resources

There are two previously identified cultural resources within the project area, archaeological site AZ AA:12:18(ASM) which is also known as the Hodges site, and AZ AA:12:29(ASM) which is recorded as a prehistoric artifact scatter. A review of the County Assessor's web site indicates that there are no standing structures likely to be 50 years old or older within the project area.



3.8 Existing Visual Resources

The background views along the corridor consist of the Santa Catalina Mountains to the north-northeast, the Rincon Mountains to the east, and Tucson Mountains to the west. The middle-ground views are a mixture of medium- and low-density residential development interspersed with native and non-native vegetation. The foreground views are of residences, businesses, and a wide, unpaved, roadway shoulder that is predominately void of vegetation.

3.9 Existing and Future Land Use

As shown in Figure 5, the current land use along La Cholla Boulevard includes single family homes, mobile homes on individual lots as well as in mobile home parks, businesses and a linear park along the Rillito River. The businesses include gas stations, convenience stores, auto repair, auto sales, tavern, welder, and commercial offices. South of Ruthrauff Road, there is a fire station for the Northwest Fire District and south of that is Flowing Wells Junior High School. South of Curtis Road, on the west side of La Cholla Boulevard, are two large vacant parcels.

At the southeast corner of River Road and La Cholla Boulevard, a commercial and medical office development is currently being built. Just north of Ruthrauff Road, on the west side of La Cholla Boulevard, a development plan for a Family Dollar Store was submitted to Pima County, but the project is on hold. South of Ruthrauff Road, on the west side, a Walmart Neighborhood Market is currently under construction.

The existing zoning along La Cholla Boulevard is Multiple Use (MU) south of Curtis Road. The County and Tucson Electric Power parcels along the Rillito River are zoned Suburban Homestead (SH). The commercial office site on the east side north of the Rillito River is zoned under a Specific Plan (SP-RE1) while the gas station on the west side is zoned for Local Business (CB-1).

Public lands within the corridor include the Rillito River and two additional parcels located on the south bank, west of La Cholla Boulevard. These parcels are currently being developed into a park.

3.10 Intergovernmental Agreements

There are two existing Intergovernmental Agreements that affect this project. The first is between Tucson Water and Pima County which provides for a 50-50 sharing of expenses for any water lines that must be relocated due to the reconstruction of a road by Pima County. The second IGA is between the Pima County Regional Transportation Authority and Pima County. It provides for some of the funding of the design and construction of this project.

Figure 5. Land Use Map



Legend	Residential
	Commercial
	School
	Fire Station

La Cholla Blvd: Ruthrauff to River Road
Figure 5. Land Use Map



4.0 Traffic and Accident Data

4.1 Traffic

General

A traffic analysis report was prepared by Kimley Horn (Reference 1) to document existing and future traffic conditions, and to provide recommendations for number and length of turning lanes at intersections, location of median openings, and the need for additional traffic signals.

The current average daily traffic volumes along La Cholla Boulevard between Ruthrauff Road and River Road vary from 23,400 to 28,400. In the year 2030, the traffic volumes are expected to increase to between 41,000 and 44,000 vehicles per day. The proposed six lane roadway section will satisfactorily handle these quantities of traffic with Level of Service B.

Intersections

The three major intersections were analyzed in the Traffic Report. The existing turning movements are shown in Figure 6. The proposed turning movements are presented in Figure 7. The proposed intersection improvements for River Road will be included in a future River Road widening project. The entering traffic volumes for the River Road intersection in the year 2030 indicate the need for a grade separated intersection.

Parking, Pedestrians and Bicycle Movements

The existing wide right-of-way encourages parking between the existing two lane road and the adjacent homes and businesses. These informal parking spots will be lost when the road improvements are made. The wide right-of-way also currently serves several other uses including: a homeowner parks his large semi-tractor trailer in front of his house, private car sales and Tucson Water parks their maintenance vehicles in the right-of-way while servicing their well. Parking spaces that exist on private property will not be impacted by these improvements.

There is existing sidewalk along River Road and Ruthrauff Road. There is also a sidewalk along the east side of La Cholla Boulevard from River Road to the south side of the Rillito River bridge. Sidewalks along both sides of La Cholla Boulevard from Ruthrauff Road to River Road will be part of the proposed improvements. Sidewalks will be extended south of Ruthrauff Road to the point where the lanes begin tapering and the curb and gutter is terminated. This will include new sidewalk down to the bus stop located on the west side. While space in the right-of-way is reserved for sidewalks during a roadway improvement project, along vacant properties the sidewalk is typically constructed by the developer when they develop their property.

There are currently four to six-foot wide paved shoulders along La Cholla Boulevard which serve as bike lanes. The five to six-foot paved shoulders to be built with this project will continue to serve as bike lanes. “Bike Route” signs and pavement markings will be provided. Both pedestrian and bicycle access will be maintained to the Rillito River linear park.



Figure 6. Existing Traffic Conditions

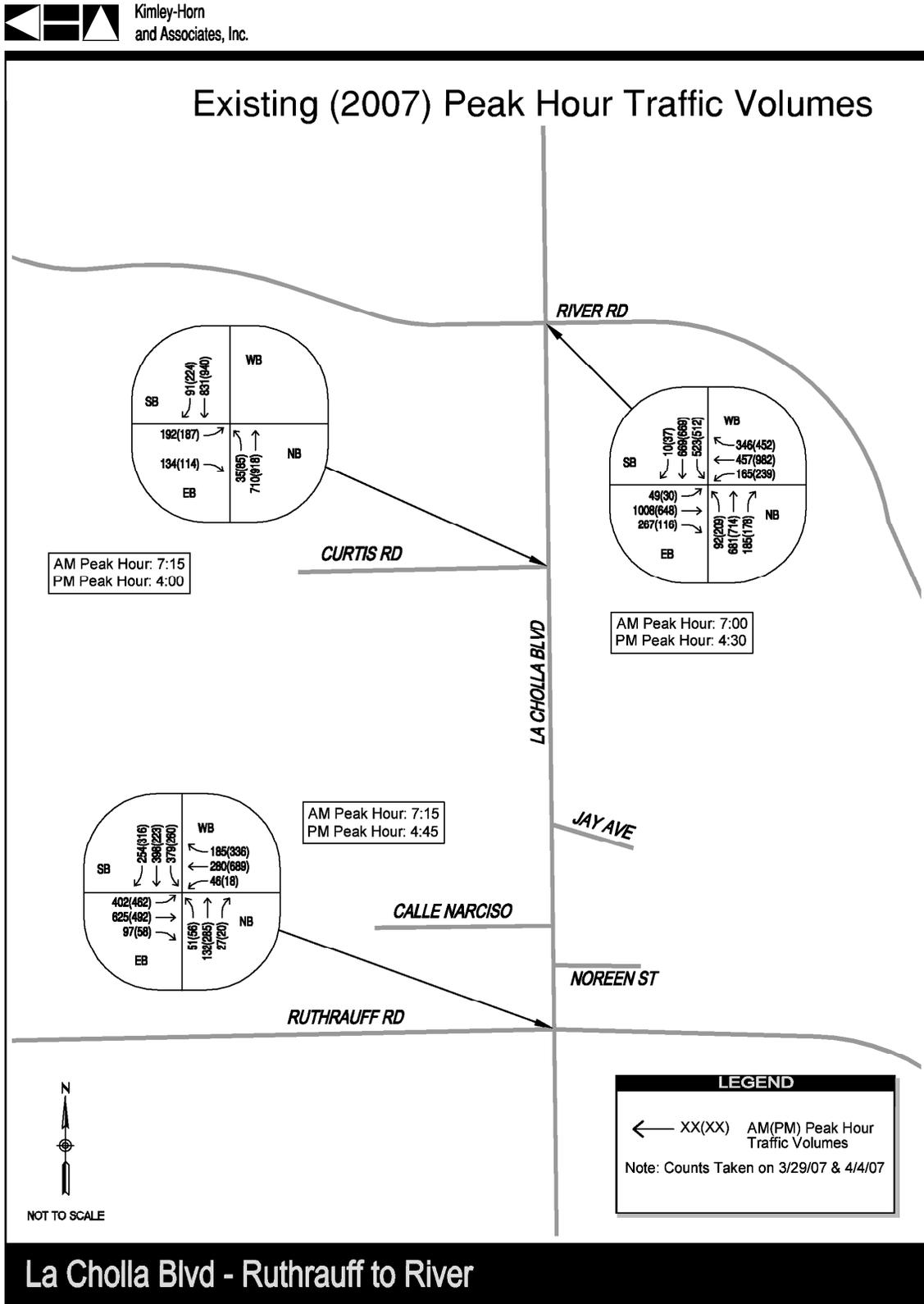
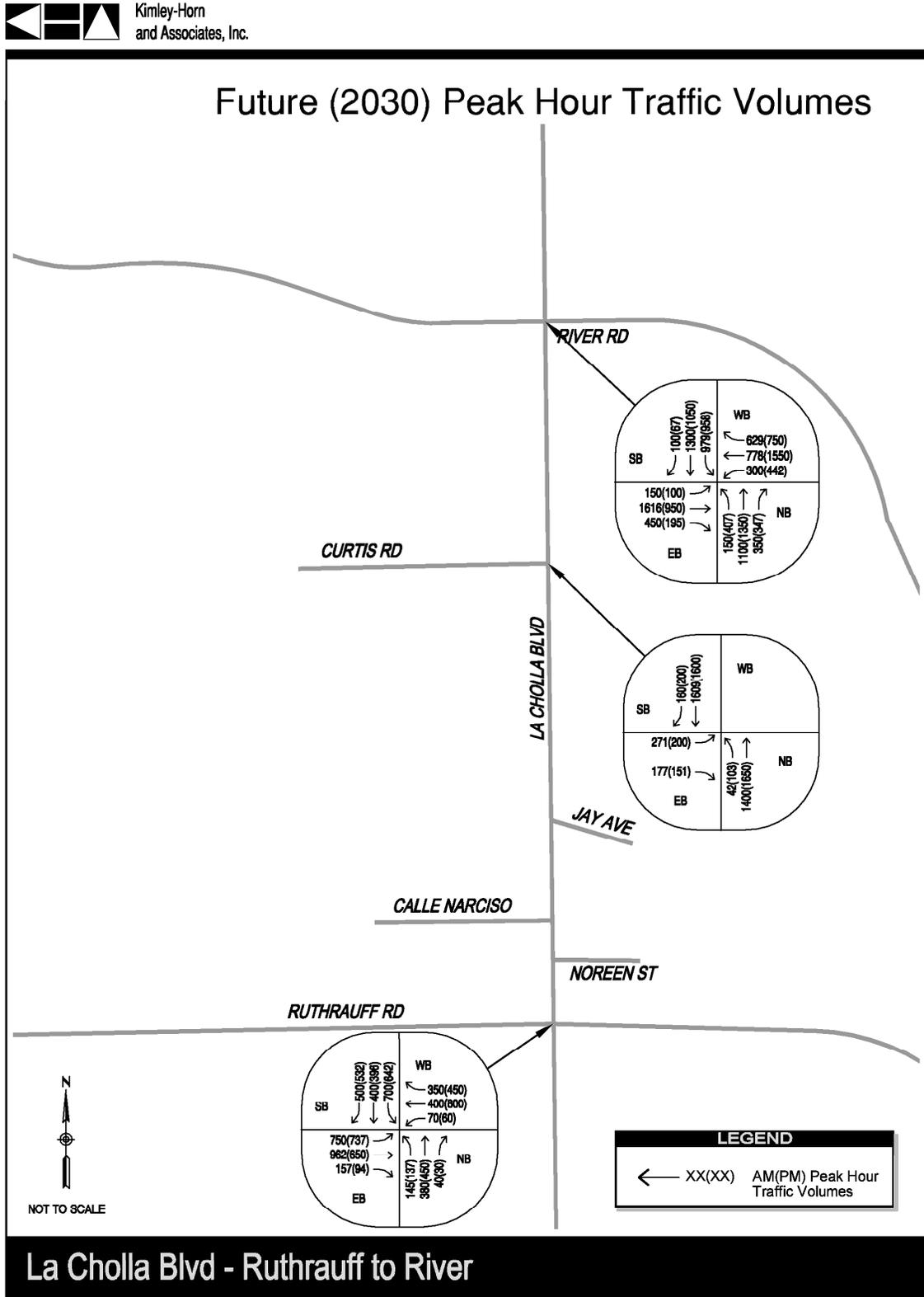




Figure 7. Future Traffic Conditions





4.2 Accidents

Crash data were obtained for the 3-year period from 2004 to 2006. During this period, there were 12 accidents on the roadway segment between Ruthrauff Road and Curtis Road, and 13 accidents between Curtis Road and River Road. The average crash rate for this roadway section of La Cholla was 1.29 crashes per million vehicle miles. Accident data for the intersections are kept separately and breakdown as shown in Table 2:

Table 2. Accident Data

Intersection with La Cholla Boulevard	Number of crashes (2004–2006)
Ruthrauff Road	50
Noreen Street	3
Calle Narciso	1
Jay Avenue	2
Curtis Road	34
River Road	105

4.3 River Road Safety Assessment

The Arizona Department of Transportation (ADOT) prepared a Road Safety Assessment for the River Road/La Cholla Boulevard intersection. PCDOT reviewed this report and determined that the following recommendations should be incorporated into this project:

- Restripe the southbound approach to place the bike lane between the right-turn lane and the painted gore.
- Restripe the southbound departure leg to place the bike lane against the curb with a painted gore between the bike lane and the through lane.
- Prohibit left turns out of the office park driveway on La Cholla Boulevard south of the intersection with a median.
- Construct a southbound right turn lane onto Curtis Road from La Cholla Boulevard.
- Provide ADA-compliant pedestrian signal push buttons with proper refuge locations at the Curtis Road/La Cholla Boulevard intersection.
- Provide bus pullout on La Cholla Boulevard, south of River Road by means of a widened shoulder.



5.0 Design Standards and Criteria

5.1 Geometric Standards

The roadway will be designed in accordance with AASHTO's A Policy on Geometric Design of Highways and Streets (Reference 2), the Pima County Roadway Design Manual (Reference 3), and AASHTO's Roadside Design Guide (Reference 4).

5.2 Design Standards

Design standards for this project include the Pima County Roadway Design Manual (Reference 3), the City of Tucson/Pima County Standard Details for Public Improvements (Reference 5), AASHTO's Guide for the Development of Bicycle Facilities (Reference 6), FHWA's Manual on Uniform Traffic Control Devices (Reference 7), and AASHTO's Standard Specifications for Highway Bridges (Reference 8).

5.3 Slope Standards

Most slopes behind the sidewalk will be 4:1. Where a steeper slope is needed to avoid or minimize right-of-way acquisition, a maximum slope of 2H:1V will be used. The geotechnical study (Reference 10) recommended that temporary cut or fill slopes not exceed 1.5H:1V for stability.

5.4 Pavement Structure

Using the information contained in Section 3.13 of the Pima County Roadway Design Manual (Reference 3), a Pavement Design Report (Reference 14) was prepared. The design criteria are as follows:

- Design period – 20 years
- Design R-Value – 45
- Resilient modulus – 21,120 psi
- Seasonal variation factor – 1.6
- Level of reliability – 95%
- Change in serviceability index – 1.4
- Minimum Structural Number for Arterial Roadway – 2.64

5.5 Design Speed

The design speed for this project is 50 mph. It will be posted at 45 mph.

5.6 Drainage Design

The drainage design criteria applied for this project has been compiled from the scope of work and Pima County's Roadway Design Manual, Reference 3. It has been summarized here:



Rillito River. The new bridge will be designed to convey the 100-year flow without raising the floodplain significantly nor allowing it to be higher than the existing bank protection. The low chord of the new bridge will be placed at least two-feet above the 100-year water surface. The depth of the bridge piers and abutments will accommodate scour from a 500-year event.

Offsite Drainage. Offsite drainage facilities will convey a 50-year storm under the roadway while containing the 100-year event with no more than a one-foot depth in flowing or ponded water within the roadway. Runoff from a 100-year storm shall not be allowed to overflow to adjacent basins.

Onsite Drainage. Onsite or pavement drainage facilities shall convey the 10-year flows. The Pima County standards require that at least twenty feet in each direction be free from flooding during the 10-year flood which allows for a flow spread of up to 20 feet. Storm drains that convey a combination of pavement drainage and cross drainage will be sized for a 50-year storm. During a 100-year storm, flowing or ponded water must not exceed one-foot in depth within the roadway.

5.7 Access Control

In general, this roadway will not be access-controlled. The only exception will be that driveways will not be allowed within 150 feet of the curb line of the major intersecting streets (River Road, Curtis Road and Ruthrauff Road). Exceptions may be made for existing driveways on a case by case basis. Also, frontage roads will be provided for the single family homes between Noreen Street and Jay Avenue so they will not have to back out directly into the La Cholla Boulevard traffic. See Subsection 10.1 for additional discussion on the use of frontage roads.

5.8 Cross Section Elements

The roadway classification for this section of La Cholla Boulevard is urban arterial. It will be designed to AASHTO and PCDOT standards as shown in Table 3.

Table 3. Cross Section Elements

Typical section	Width
Inside traffic lane	12 feet
Middle and outside traffic lane	11 feet
Right turn lane	13 feet
Left turn lane (against curb)	13 feet
Left turn lane	12 feet
Bike lane	6 feet (5 feet in frontage road area)
Median	20 feet (18 feet in frontage road area)
Clear zone	20 feet from vehicle travel lane per AASHTO (Reference 4)
Sidewalks	5 feet wide on each side of road.

Note: See Figure 3.

Turn lanes will be provided at the River Road, Curtis Road and Ruthrauff Road intersections as recommended in the Traffic Report discussed in Section 4 of this report.



5.9 Roadway Geometrics

Based upon the 50 mph design speed, the minimum radius that will be used with a 4% superelevation is 926 feet. The minimum horizontal curve length will be 500 feet. Angle breaks of 1° 08' or less may be used in lieu of a horizontal curve.

The maximum grade shall not exceed 3%. Since the new road will be curbed, the minimum grade shall be 0.5%.

5.10 Right-of-Way Width

The existing minimum right-of-way width of 150 feet will be maintained for the new roadway. The only new right-of-way that will be needed is at the Curtis Road intersection to provide room for the new turn lanes and near the new bridge to provide room for paths connecting the new sidewalk to the Rillito River Linear Park.



6.0 Major Design Features

6.1 Horizontal and Vertical Alignment

The construction centerline of La Cholla Boulevard will follow the section line. The only angle break will be at the quarter corner in Curtis Road where a 0° 0' 10" deflection will be required to keep the road centerline on top of the section line. The new roadway profile generally follows the existing grade except at the bridge where it has been raised to accommodate the deeper girders and provide more freeboard. Stage I (15%) construction plans are included as Appendix C.

6.2 Access Control

There are 19 residential driveways that currently access La Cholla Boulevard. An alignment study was performed to determine how they were to be maintained and is documented in Section 10.0 of this report.

Driveways will be provided to every property which currently have their primary access from La Cholla Boulevard. On any parcel which does not have sufficient area for a car to turn around, frontage roads will be provided. Backing out into La Cholla Boulevard will not be permitted for safety reasons. Access to vacant properties will be allowed through the normal Pima County permitting processes.

The Pima County design guidelines direct that median openings be placed no closer than 660 feet to other median openings. The preferred spacing is 1320 feet. The traffic report recommended four median openings along the project length. At three locations the spacing is closer than 660 feet. The distance from Ruthrauff Road to Calle Narcisco is 587 feet while the distance from Calle Narcisco to Jay Avenue is 579 feet. The median openings at both Calle Narcisco and Jay Avenue are vital to neighborhood circulation. The third location is 650 feet south of River Road and is being installed based on the approved development plan.

6.3 Right-of-Way

A copy of the right-of-way requirements plan is included as Appendix D. This plan includes the names and addresses of the affected property owners. A summary of the right-of-way acquisition is provided below:

<u>Assessor Number</u>	<u>Owner</u>	<u>Area (Acres)</u>
101-16-117A	La Cholla/Curtis Limited Partnership	0.0044
101-13-016C	Pima County	0.3022
101-13-015M	Pima County Flood Control	0.0181
101-13-015K	Unisource Energy Corp.	0.1868
None	Unknown (Rillito River)	0.3442



6.4 Drainage

An initial drainage report covering the cross drainage impacts has been prepared by HDR, Inc. (See Reference 9). The first step was to calculate the 50 and 100-year peak flows for each watershed. The results of these calculations as well as the approximate capacity of the existing storm drains are shown in Figure 8.

Each of these watersheds were analyzed to determine the type of drainage improvements that would be required to convey the 50-year peak flow under La Cholla Boulevard. Alternative storm drain systems were designed and analyzed. The recommended system is graphically shown in Figure 9. Note that the 50-year peak flows are slightly different than those shown in Figure 8. This is because the 50-year peak flows used for calculating the existing conditions were derived using ratios of the 100-year storm. For determining the size of the new storm drain, rainfall intensities were needed, so the hydrology model was rerun specifically for the 50-year storm resulting in some minor differences.

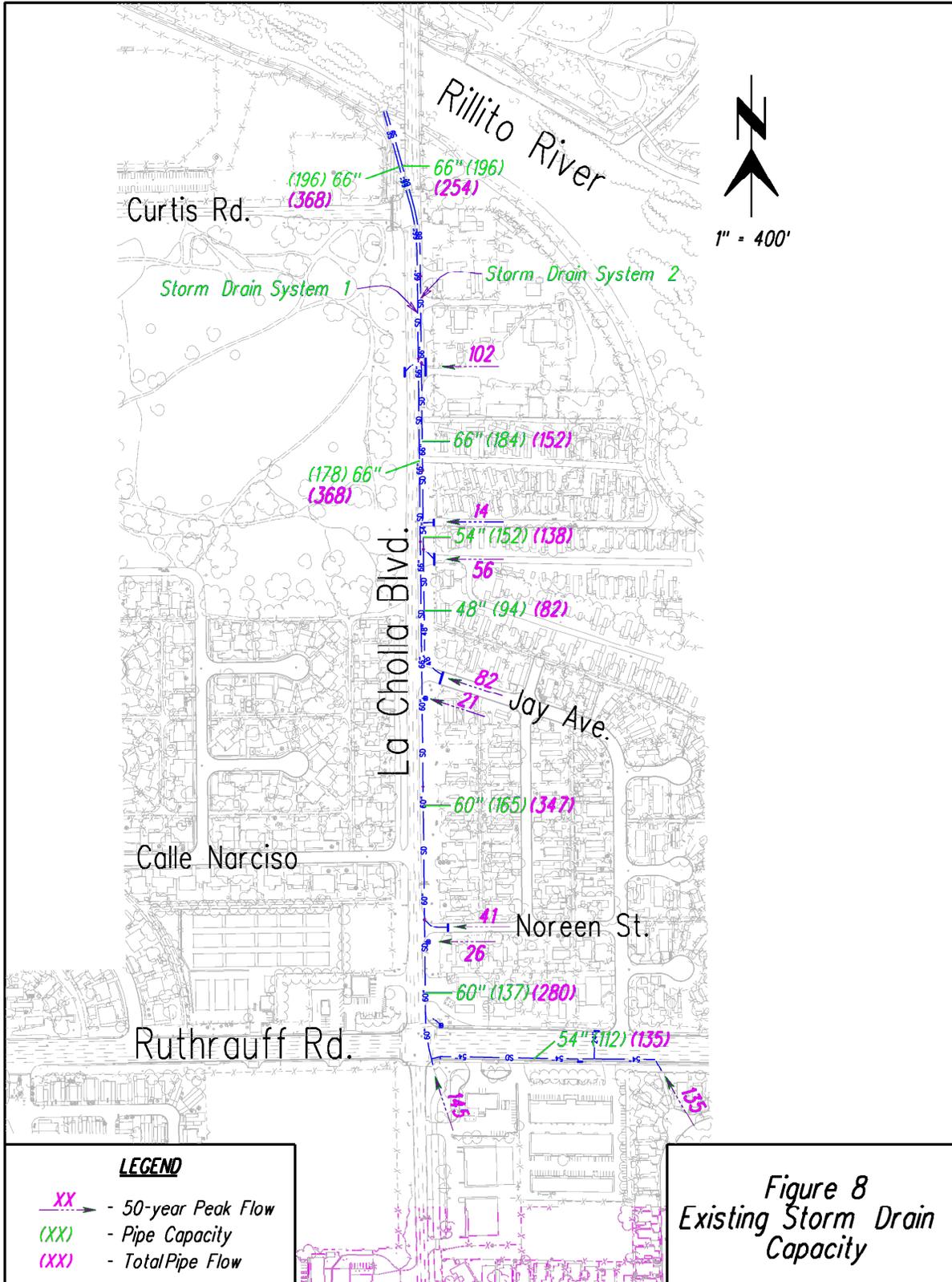
In addition to the new storm drain there will be a new three-span bridge over the Rillito River. The completed bridge will pass a 100-year storm of 32,000 cfs with two feet of freeboard. The new piers will withstand the scour from a 500-year storm (62,000 cfs). A complete summary of the new bridge is presented in Section 6.8 of this report.

If the United States Army Corps of Engineers determines that they have jurisdiction over the Rillito River, a Section 404 Nationwide Permit will be required for the bridge construction.

A pavement drainage system will also be designed to keep 20-feet of pavement width of La Cholla Boulevard open in each direction during a 10-year storm.



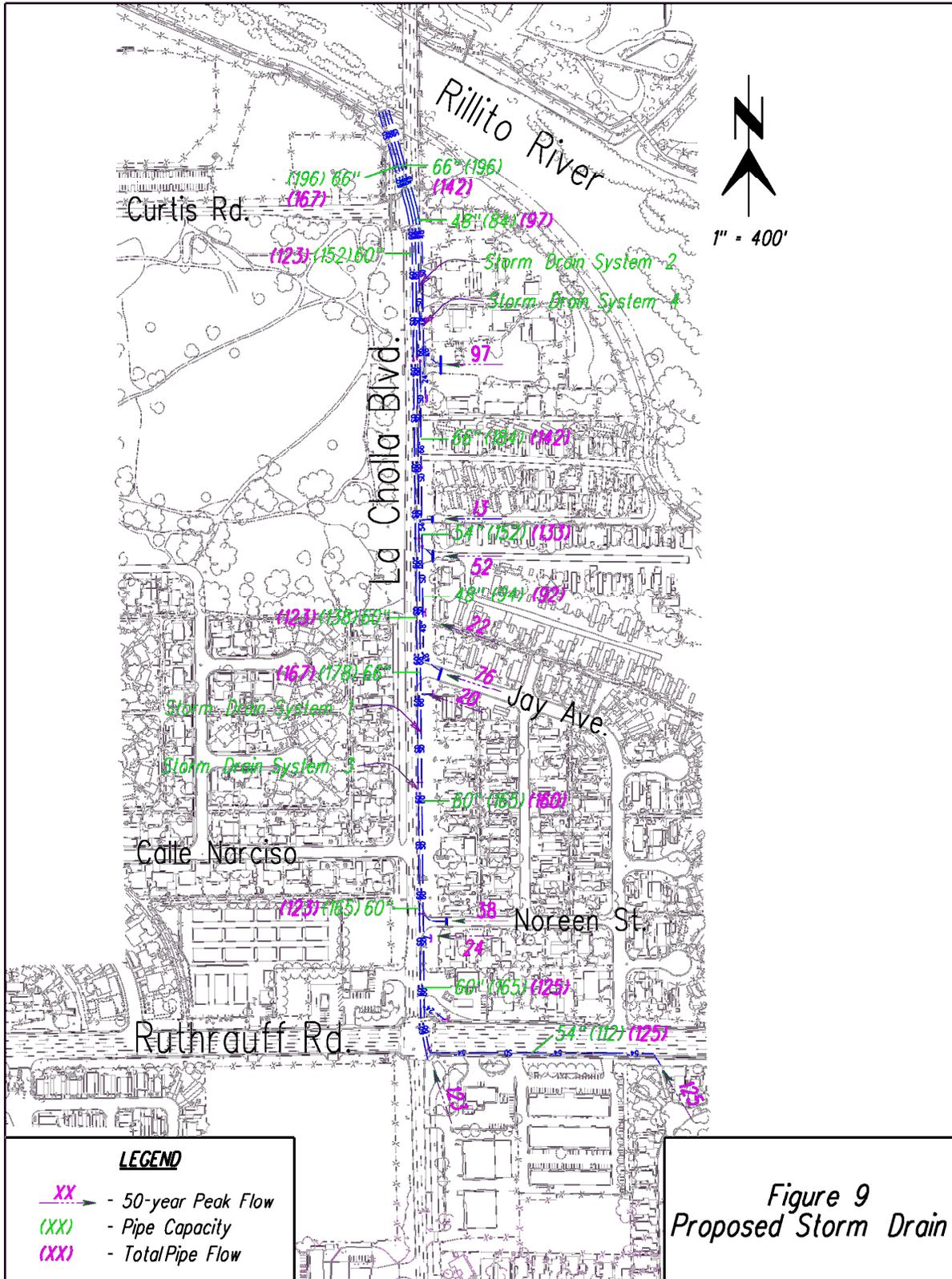
Figure 8. Existing Storm Drain Capacity



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Figure 9. Proposed Storm Drain



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6.5 Earthwork Considerations

The new roadway profile was based on two main constraints. The first was the need to raise the profile over the Rillito River so that the new deeper bridge structure could be accommodated with two-feet of freeboard over the 100-year floodplain. This will also increase the headroom over the bike paths that cross under the bridge. The second constraint was to match the existing grades along the single family homes with the new frontage road grades to minimize the impact to the front yards of the homes. The new profile generally resulted in excavation along the homes and embankment at the Rillito River bridge. The initial earthwork quantities are 18,300 cubic yards of excavation and 17,400 cubic yards of embankment. Therefore, approximately 900 cubic yards will need to be wasted or hauled out.

The draft geotechnical report (Reference 10) recommended that a design R-value of 45 be used for designing the new pavement structure. This should eliminate or minimize the amount of on-site material that is found to be unsuitable as subgrade. It also evaluated the on-site soils for their suitability as trench backfill for the new storm drains and determined that they are suitable both within and outside of the roadway prism. As far as structure backfill and shading material, there are some local areas that may provide suitable material but it is recommended that the contractor assume this material will have to be imported.

6.6 Intersections

The proposed improvements for the Ruthrauff Road intersection include dual southbound and eastbound left-turn lanes. Single left turn lanes will be provided for the northbound and westbound legs. Single right turn lanes will be provided at all four legs of the intersection. Of the three southbound travel lanes, one will be trapped into one of the left turn lanes while the other two will continue south through the Ruthrauff intersection.

At the Curtis Road intersection, left turn and right turn lanes will be provided for all three legs. Single through lanes will be provided for the east/west leg.

At River Road, the new roadway will tie into the existing south leg maintaining the current lane configuration which includes dual left turn lanes and a single right turn lane. The existing east, west, and north legs will remain unchanged.

6.7 Utilities

The existing utilities were summarized in Section 3.5. The water and sanitary sewer lines will be impacted by the new storm drain and will be relocated under the roadway contract. The relocation of the gas, telephone and cable TV lines will be performed prior to the road construction. The overhead electric lines will also need to be relocated and this will be accomplished prior to the road work. There is a well site operated by Tucson Water located on the west side of La Cholla Boulevard just south of Jay Avenue. A catch basin will be built in the frontage road to capture the waste water when the well is flushed. No other accommodations will be needed for the well site.



The high voltage electric line relocation must be performed between September and May. The high pressure gas line relocation must be performed between April and September. If the 16” water line in Ruthrauff Road must be relocated, this work must occur between October and April.

The existing bridge supports several utilities including Qwest fiber optic lines in four ducts, two reclaimed water lines used by the Pima County Department of Natural Resources Parks and Recreation (PCDNRP&R) and a Pima County electrical conduit for traffic signals. The PCDNRP&R also owns an electrical conduit with power lines that serve their irrigation controllers in the Rillito River linear park. The traffic signal conduit and the Qwest fiber optics lines will be relocated to the new bridge. No new utilities have been identified as needing to be carried by the new bridge.

No prior rights for the facilities located within the public right-of-way, owned by private utility companies, have been identified. Under an existing intergovernmental agreement, half of the water relocation costs incurred by Tucson Water will be paid for by Pima County. Also, all of the sewer relocation costs incurred by the Pima County Regional Water Reclamation Department will be reimbursed by project funding.

6.8 Structures

General

A Bridge Selection Report (Reference 11) was prepared for the La Cholla Boulevard Bridge over the Rillito River. The existing four-span bridge was constructed in 1981 and carries two lanes of traffic. This bridge will be removed. The new bridge will be constructed along a similar alignment as the existing bridge but will be constructed as two separate side-by-side structures, each carrying three lanes of traffic. A plan and elevation and typical section showing the new and existing bridge are presented in Figures 10 and 11 respectively.

The new bridge is located approximately 700 feet south of the River Road intersection. It is possible that this intersection will be rebuilt into a grade separated interchange at some point in the future. This reconstruction may result in a shifting of the traffic lanes on La Cholla Boulevard in the vicinity of the Rillito River Bridge. If this occurs, some retrofitting or reconstruction of the open median between the two bridges may be required.

Existing Bridge

The existing La Cholla Boulevard Bridge over the Rillito River is scour critical for the 500-year superflood event, and therefore, at risk of catastrophic failure during extreme flooding. There is also a significant amount of cracking in the bridge deck which will require the deck to be replaced or retrofit at a minimum in the next ten years. The abutment diaphragms and beams have cracks and spalls that will need to be repaired due to exposed rebar, but these cracks do not affect the overall structural capacity. Larger than normal differential settlement has been seen at the north abutment that causes rideability concerns. Approach guardrails do not meet current safety standards and the bridge barriers are not sufficient for pedestrians or bicyclists. The existing bridge would have to be strengthened for the superflood event and widened on both sides in order to make it viable with the proposed alignment. It has been determined that the existing bridge will be replaced.



Bridge Geometrics

The proposed cross section of both the northbound and southbound bridges will be comprised of a 1-foot-wide concrete parapet with pedestrian railing, a 6-foot, $\frac{3}{4}$ -inch sidewalk, a 1-foot, $4\frac{1}{4}$ -inch concrete parapet wall with bicycle railing, a 6-foot shoulder, three 11-foot travel lanes, a 2-foot shoulder and a 1'-7" concrete barrier 3'-6" high along the inside edge of the bridge deck. The north span on the northbound bridge will be flared from 51 feet wide to 61 feet wide in order to accommodate site distances. The open area between the southbound and northbound structures will vary from approximately 20 feet at the south abutment to approximately 21 feet at the north abutment. The distance between the two structures varies because of the 50:1 taper that is required to shadow the proposed dual left turn lanes to the north of the bridge at the River Road intersection.

The southbound bridge will have a length of 360 feet and the northbound bridge will have a length of 356 feet between centerline bearing of abutments. There will be three equal spans of 120'-0" and 118'-8 $\frac{1}{4}$ " in the southbound and northbound bridges, respectively. Although the structures have slightly different skew angles, the centerlines of the piers and abutments will be along the same line. The span arrangement has been set so that the new foundations will not interfere with the existing pile foundations of the existing bridge. The new abutments will be set behind the existing abutments and the piers for the new three-span configuration will be placed between the pile foundations of the existing four-span bridge, thereby minimizing conflicts with the existing piles. Fifteen-foot long concrete approach slabs will be constructed at both ends of the bridge.

The roadway centerline of La Cholla Boulevard, at the proposed bridge, is on a tangent alignment, bearing N 0° 38' 04" W. The southbound structure will follow this alignment. The northbound structure will not be parallel to the southbound structure, but will follow a tangent with a bearing of N 0° 28' 52" E to accommodate the 50:1 taper that occurs south of the River Road intersection.

The proposed vertical alignment will be higher than the existing bridge and will be on a 650-foot long crest vertical curve. The approach grade will be +2.22% and the departure grade will be -2.27%. The profile will result in just over two-feet of freeboard under the bridge during the 100-year storm event. The cross slope of the new bridge is 2%.

All substructure units will have a skew of approximately 30° to the right, which matches the existing bridge. This will result in hydraulic and scour characteristics which are similar to existing conditions.

The configuration of the roadway on the bridge is such that a large median area will exist between northbound and southbound traffic. Two options have been considered for the median area: a closed median and an open median. The closed median option is more visually open and can be easily modified to accommodate future traffic lanes by removing a portion of the raised median curb. However, preliminary cost estimates show that constructing this essentially unused portion of the bridge will cost an additional \$1.5 million. The open median option provides the minimum bridge structure required for travel lanes. In this alternative, the median is eliminated and concrete bridge barriers are placed along the inside traffic lanes for the full length of the bridge. These barriers will require impact attenuation devices at the bridge approaches, which will need to be maintained when struck



by errant vehicles. The cost of the additional barriers and impact attenuation devices, including probable future maintenance, is significantly less than the additional cost of the median bridge structure.

It is unlikely that the proposed bridge would need to be widened to the center in the future. The proposed widening will accommodate future traffic projections until at least 2030. In addition, there are no roads in Pima County today that are wider than six lanes. Therefore, because of the cost savings, the bridge will be designed with an open median.

Bridge Drainage

The watershed area upstream of the bridge is approximately 900 square miles. The 100-year and 500-year peak discharge rates are 32,000 cfs and 62,000 cfs, respectively.

The Rillito River in the vicinity of the bridge is approximately 300 feet in top width. The streambed is comprised of medium to fine sand with some gravel and cobbles.

A scour analysis for the proposed bridge was done using the Federal Highway Administration HEC-18 (Reference 12) model. Results of the 500-year scour analysis are as follows:

1. Contraction scour = 7.7 feet
2. Local pier scour = 18.3 feet
3. Total pier scour = 26 feet
4. Abutment scour = 23 feet

No new channelization will be required. The existing channel cross section at the bridge consists of a 240-foot bottom width with 2:1 soil cement side slopes. The depth of the channel is approximately fifteen feet. In constructing the new bridge the existing 8-foot thick soil cement bank protection will be trimmed as needed to construct the new abutments on both sides of the wash. Excavated soil cement will be replaced with 2000 psi concrete.

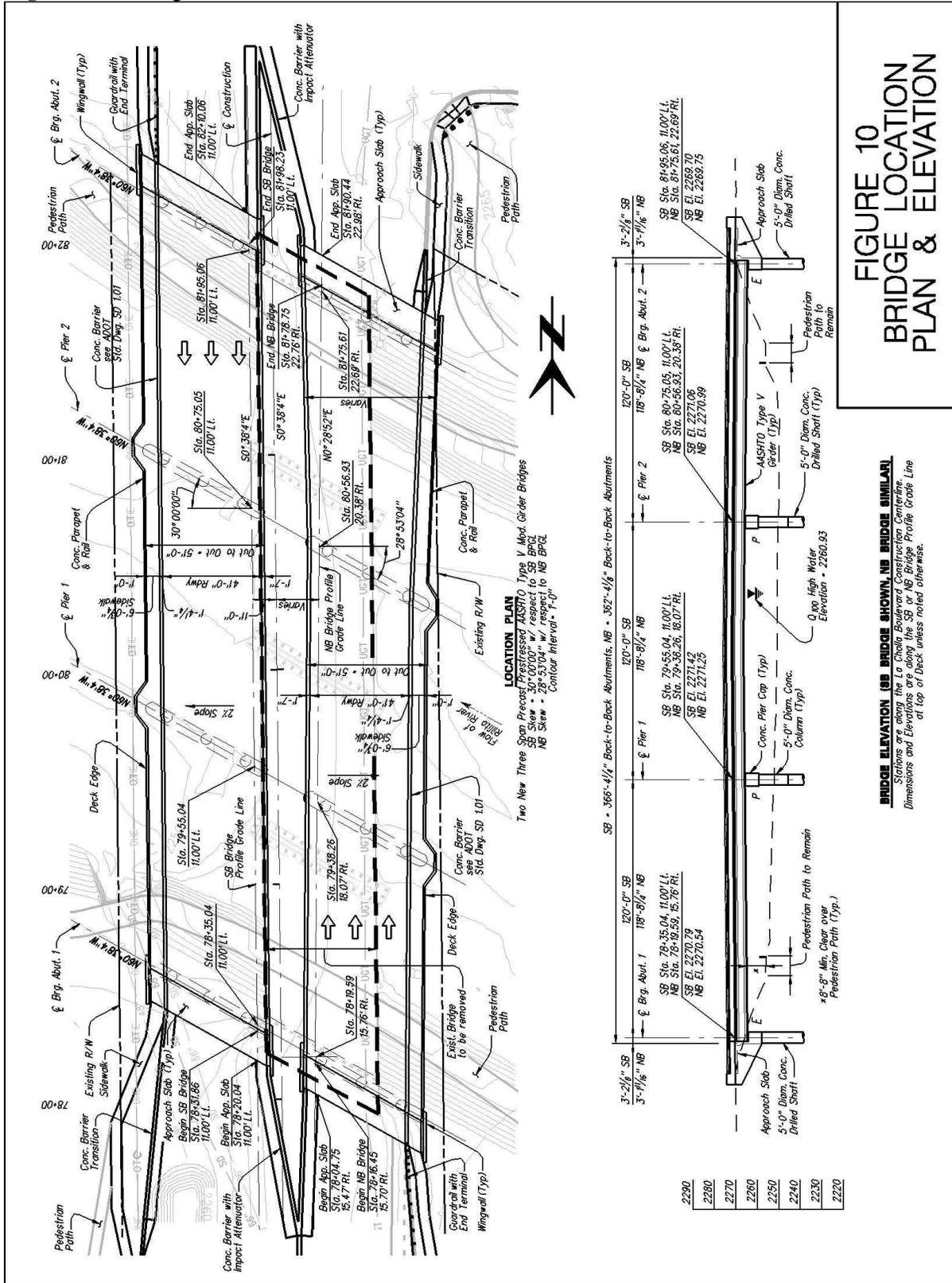
HEC-RAS (Reference 13) analyses of the proposed and existing bridges were conducted for the 100-year (32,000 cfs) and 500-year (62,000 cfs) flow rates. Results of the HEC-RAS analysis for the proposed bridge are as follows:

1. The 100-year water surface elevation at the upstream face of the bridge is 2260.93 feet.
2. The 100-year flow velocity is 10.0 feet per second.
3. The 500-year water surface elevation at the upstream face of the bridge is 2268.06 feet.
4. The 500-year flow velocity is 11.6 feet per second.
5. 100-year minimum freeboard = 2.0 feet. (Actual freeboard provided = 2.07 feet.)

The bridge slopes in two directions, which will drain water off of the roadway and into catch basins along the outside curbs just off the bridge and deck drains equally spaced along the span adjacent to the traffic barrier. The deck cross-slope along with the crest vertical curve will draw water out and off of the bridge deck. Deck drains will also be placed at the base of the concrete parapet at approximately 40 feet on center to allow water buildup on the sidewalks to drain off the edge of the bridge.



Figure 10. Bridge Location Plan and Elevation





Bridge Utilities

There are several utilities within the project limits that are to remain. Only those utilities located on the existing bridge will need to be relocated or deactivated in conjunction with construction of the new bridge. Disruption of affected utilities will be minimized by relocating them to the new bridge before demolition of the existing bridge. Additionally, precautions will need to be taken during construction near the Tucson Electric Power 46KV overhead line that is approximately six feet away from the proposed southbound bridge deck. Specific requirements regarding construction in the vicinity of the powerlines will be included in the construction documents. See Section 6.7 and Figure 11 for additional information regarding bridge utilities.

Bridge Substructure

Multi-column bents with transverse pier caps and formed concrete columns will be used to provide a structure with similar hydraulic properties to the existing bridge. Spill-through abutments, or stub abutments, will be used at both ends of the new bridge. Soil cement is presently in place to protect the banks of the channel. The existing soil cement will be removed and replaced as required for the construction of the new abutments. Since the new abutments fall behind the existing soil cement, it is anticipated that only a small portion of soil cement will need to be removed to construct the new abutments. Drilled concrete shafts will be utilized to support the abutments and piers. The drilled shafts will have a diameter of 5'-0" at the abutments and the piers.

Bridge Superstructure

Four different superstructure alternatives and span configurations were considered for the new bridge, including cast-in-place post-tensioned concrete box girders, steel plate girders with a composite deck, precast prestressed concrete box girders, and precast prestressed concrete AASHTO I-Girders with a composite deck. Based mainly on simplicity of construction and minimizing permanent disturbance within the Rillito River channel, the three-span bridge with AASHTO Type V Modified Girders was recommended. The estimated cost for the new bridge is \$4.7 million, not including contingencies.

6.9 Pavement Design

Based on Section 3.13 of the Pima County Roadway Design Manual (Reference 3), the Draft Geotechnical Report (Reference 10) and the Final Traffic Report (Reference 1), a Final Pavement Design Report (Reference 14) was prepared. The pavement structure recommended for both La Cholla Boulevard and Ruthrauff Road consists of two inches of asphalt-rubber asphaltic concrete over three inches of asphaltic concrete over seven inches of aggregate base. All minor side streets and the frontage roads will be paved with 2.5 inches of asphaltic concrete over four inches of aggregate base.



6.10 Signalization and Lighting

All intersections with side streets having an ADT over 2400 vehicles per day will receive lighting. This will include Ruthrauff Road and Curtis Road. The lighting at River Road will remain as it currently exists. Conduits for future continuous street lighting and ITS along both sides of La Cholla Boulevard between Ruthrauff Road and River Road will be considered during design.

The traffic signals at Ruthrauff Road and Curtis Road will be reconstructed to accommodate the proposed intersection widenings.

6.11 Construction Issues

The construction phasing scheme will be driven by the need to maintain traffic across the Rillito River. One proposed construction sequencing approach would be as follows:

1. Remove the existing steel railing along the west edge of the existing bridge deck and place temporary concrete traffic barrier a minimum of two feet away from the west edge of the deck. Traffic will remain on the existing bridge as it is today with one lane open in each direction.
2. Construct the new southbound bridge and the west half of La Cholla Boulevard from Ruthrauff Road to River Road. Traffic will be maintained on the existing bridge and on the existing two lane pavement. Additional paving will be added to the east side of La Cholla as required to maintain two lanes of traffic.
3. Relocate the utilities that are supported by the existing bridge onto the new southbound bridge. Move traffic to the new southbound bridge and the new roadway built in the previous phase. Remove the existing bridge and the existing pavement. Build the northbound bridge and the east half of La Cholla Boulevard from Ruthrauff Road to River Road.
4. Complete final striping and landscaping.

Access to all homes and businesses must be maintained during construction. While driveways are being built, temporary parking may need to be provided to the residents. Refer to Section 6.7 for seasonal considerations related to scheduling of utility relocations.



6.12 Design Exceptions

Introduction

The study team compared the geometric design elements of the proposed La Cholla Boulevard improvements and the recommendations contained in AASHTO's A Policy on Geometric Design of Highways and Streets (2004) and the Pima County Roadway Design Manual (RDM). Design exceptions will be discussed in each category. The roadway classification is urban arterial.

Lane, Shoulder and Median Widths

The proposed lane widths will vary from 11 feet to 12 feet. AASHTO recommends 12 feet for a lane width but finds 11 feet acceptable for a reconstructed arterial. The RDM recommends 12 feet.

The outside paved shoulder widths are 6 feet for the main section and 5 feet for the frontage road section. AASHTO recommends 8 feet minimum for an outside shoulder. The RDM recommends 6 feet. The inside paved shoulder width is one foot throughout the project per the RDM. AASHTO does not recommend a minimum as long as the median curb is mountable as it will be on this project.

The RDM recommends a median width of 20 feet. An 18-foot wide median width is used in the frontage road area.

Vertical Alignment and Stopping Sight Distance

For 50-mph, AASHTO recommends a minimum stopping sight distance of 400-475 feet. This project is being designed using the Pima County requirement for stopping sight distance of no less than 475 feet. All new vertical curves for La Cholla Boulevard meet this criteria.

Horizontal Alignment and Superelevation

AASHTO recommends the superelevation is not to exceed 12% for a high speed urban street. Pima County allows a maximum superelevation rate of 4% for urban streets. There are no curves used on La Cholla Boulevard. The maximum angle point of 1°08' specified by the RDM will be adhered to.

Design Speed

For urban arterial roadways, AASHTO recommends a design speed of 40 to 60 mph. Pima County has specified a design speed of 50 mph for this project.

Grades

AASHTO recommends a maximum grade of 6% for urban arterials in level terrain. The RDM specifies a maximum grade of 3%, which will be followed. Due to the use of curbs on this project a minimum grade of 0.5% will be maintained.

Cross Slope

The AASHTO recommended cross slope range for travel lanes is 1.5% to 2.0%. The travel lanes on this project will have a cross slope of 2.0%.



Bridge Width

AASHTO states that the minimum clear width for new bridges on arterial streets should be the same as the curb to curb width of the street. That criteria is met on this project.

Design Exceptions

No design exceptions from the AASHTO controlling design criteria will be necessary. The Pima County design standards that will require an exception are for lane, paved shoulder and median widths. Pima County granted these exceptions to help minimize the right-of-way impacts to adjacent properties.



7.0 Social, Economic, and Environmental Considerations

7.1 Biological Resources

The project has very limited vegetation, but does contain native plant species subject to the County's Native Plant Preservation Ordinance and the Arizona Native Plant Law. Plants will be preserved in place, salvaged and relocated, or replaced, consistent with the Ordinance and the project landscape plan. The project may affect nesting birds protected under the International Migratory Treaty Act. Bridge demolition outside of the nesting season is recommended, or nest removal and treatment, is recommended to avoid impacts to nesting birds. Preconstruction surveys for burrowing owl will also be needed in suitable habitat for this species.

7.2 Air Quality

The project is located in the Tucson Region Carbon Monoxide Limited Maintenance Area. The La Cholla Boulevard widening from River Road to Ruthrauff Road is in conformity and was analyzed for air quality impacts as part of the 2008-2012 Transportation Improvement Plan for Pima County.

7.3 Noise

Sensitive receptors in the project area include adjacent residences and the Rillito River Park/Trail. A noise study (Reference 18) was prepared to evaluate the increase in roadway noise resulting from the future change in traffic, year 2030 traffic volumes under the proposed roadway design, consistent with the Pima County Department of Transportation noise procedure (PCNAP, 2008).

The project will result in temporary noise impacts during project construction associated with the operation of heavy equipment. Mitigation measures are proposed to minimize short-term construction noise to the extent practicable; however, construction noise impacts will occur.

The project will result in an increase in traffic noise at adjacent residences. Rubberized asphalt will be used in roadway construction and will result in a noise reduction. The resulting noise levels will exceed acceptable noise levels, based on the Pima County Noise Abatement Procedure (PC NAP) criteria of 66 dBA, at 29 residential locations north of Ruthrauff Road and at the Rillito River Park.

Noise walls are recommended for installation in the medians between the roadway and the frontage roads on both sides of La Cholla Boulevard, and in front of the residences north of Jay Avenue, on the east side of La Cholla Boulevard. Noise walls at the Rillito River Park would not be effective.



7.4 Hazardous Materials

A Phase I Environmental Assessment (Phase I) was conducted to determine whether there are any hazardous materials conditions of concerns within the project area (Reference 16). The results of the Phase I identified potential hazardous materials concerns regarding existing and past service stations, and closed landfill sites, and recommended further evaluation of these sites. A preliminary site investigation involving subsurface investigation and testing was conducted at these sites to determine whether there were any implications for the project (Reference 17). Results indicate that the landfills do not extend into the project right-of-way and that sampling at the service stations did not reveal any contaminants at actionable levels. Therefore, these sites are not a further consideration for the project.

7.5 Historical/Cultural Resources

Two previously recorded archaeological sites occur within the project area: the Hodges Ruin (AZ AA:12:18 [ASM]), a large habitation site; and AZ AA:12:29 (ASM) an artifact scatter. Archaeological testing was conducted to assess the subsurface potential for sites and to determine the site boundaries (Reference 15). Testing did not reveal any subsurface features or artifacts associated with AZ AA:12:29 (ASM). The boundary of the Hodges Ruin near the project site has been further refined, and the project limits have been refined to avoid impacts to this site. Nonetheless, potential for subsurface features associated with the Hodges Ruin remain. As a result, archaeological monitoring with 100 feet of the Hodges Ruin will be required.

7.6 Visual/Aesthetic Resources

The project will have the greatest change in visual character between Ruthrauff Road and Curtis road by converting a two-lane roadway with unpaved shoulders to a six-lane urban roadway with raised medians, curb, noise walls and a section of frontage roads. Mountain views will remain largely unobstructed, except closest to noise walls. Background views will remain expansive on approach to the Rillito River. Foreground views will be improved by landscaping and other aesthetic improvements along the corridor.

7.7 Neighborhood Impact

A range of alternatives were developed for this project. Design alternatives were evaluated with the acquisition of homes on the east and west of La Cholla Boulevard, the acquisition of homes and partial takes on only one side of the roadway, and the roadway widening without the acquisition of homes. The recommended alternative is one that will not require the acquisition of homes.



The overall neighborhood effects are similar for all the alternatives because they relate to improving traffic flow and modifying access points. The addition of a raised median will require that some residents make U-turns to access side streets, which they presently access via left-hand turns. However, the medians also result in a safer roadway that results from better separating the opposing traffic. Current frontage roads are in poor condition and do not limit access to La Cholla Boulevard. One-way frontage roads will be provided for the residences that directly front La Cholla Boulevard for safe ingress and egress. The project will eliminate use of the right-of-way for parking, which will have to be absorbed on commercial and residential properties and local streets. Access to businesses and residences will be maintained throughout the construction process.

Access to the neighborhood schools, the new park at Curtis Road and the Rillito River Park is a concern expressed at the public open house. To address the access to the schools, sidewalks will be provided to the southern limits of the new curb. From that point to the end of the new pavement, graded shoulders will be built to provide pedestrians with a place to walk outside of the traffic. Access to the two parks will be provided via new pedestrian paths built on all four corners of the Rillito River bridge.

7.8 Community Resource Impact

Temporary effects are anticipated during construction and include a loss of access to the portion of the Rillito River Park that passes under the bridge and a loss of bicycle access to La Cholla Boulevard within the project boundaries. Pedestrian detours will direct path-users around construction activities.

Based on the traffic study, there is a possible cut-through route along Jay Avenue that will be monitored and, if needed, traffic calming measures should be considered. The project will improve overall connectivity through the provision of improved pedestrian, bicycle, and bus facilities.



8.0 Public Involvement

8.1 Public Participation Plan

A Public Involvement Plan was prepared by Gordley Design Group, Inc and submitted to the County on September 6, 2007. This plan features public information meetings, a Community Advisory Committee, ongoing contact with affected parties, media relations and the development of informational materials. The goals of the plan are to educate the public about the project's purpose and need, solicit the public's comments on the project, review public comments and adjust the roadway design concept to address the public concerns to the greatest extent possible and within the constraints of the project, including safety and cost.

8.2 Community Advisory Committee

Community Advisory Committee Meetings

There have been six Community Advisory Committee (CAC) meetings held to date. The first meeting was held to go over the responsibilities of the CAC. The second, third and fourth meetings were held to discuss the Alignment Alternatives Study. The fifth meeting was held to review the results of the noise study. The sixth meeting was held to review the Draft Design Concept Report and the Draft Environmental Assessment and Mitigation Report (EAMR). The minutes from these six meetings have been enclosed in Appendix B.

Future CAC meetings will be held to obtain their input on the construction plans. The CAC will also meet as needed to provide input on the artwork, aesthetic treatments, and other items for which they have jurisdiction.

Community Advisory Committee Concerns

The CAC has had an opportunity to review many of the features of the proposed roadway. Aspects of the County plan that the CAC is in agreement include: 6-lanes, wider bridge, turn lanes at intersections, inclusion of sidewalk, bike lanes, and bus stops. However, there are a number of significant concerns that the CAC has with the County's recommended plan. These concerns primarily center on an approximately 1000' length of La Cholla north of Calle Narciso on the west side and between Noreen Street and Jay Avenue on the east side. These specific concerns are listed on the next page.



- Travel lanes
- Sidewalk
- Bike lanes
- Access and u-turns
- Parking
- Frontage roads
- Proximity of roadway to existing buildings
- HAWK light
- Alley
- Accidents
- Noise
- Public art
- Landscape
- Well site
- Property values
- Crosswalks
- Bus bays

These issues are described in detail in the CAC’s letters dated January 15, 2008 and January 31, 2008, copies of which are included in Appendix B. PCDOT believes all of these are important concerns, and responded in the letters dated January 24, 2008 and February 11, 2008, which are also included in Appendix B.

The plan the CAC recommends (Alternative D) is a plan developed by PCDOT during the alignment study. Although this is not the plan that PCDOT recommends, it provides added benefits such as lane widths per Pima County’s Roadway Design Manual; increased buffer space for landscape, public art, and noise mitigation; and frontage roads are not required. The disadvantage is the disruption to property owners, renters, and businesses due to the property acquisition required, as well as the cost associated with that acquisition. If additional money is made available for property acquisition on La Cholla, then money must be taken away from other projects. There is also the opportunity cost. If millions of dollars are now made available for property acquisition on La Cholla, could that money be better spent by improving the River Road intersection, or building a closed median on the proposed bridge over the Rillito River, or improvements at Ruthrauff Road and I-10?

While the CAC has been fairly unified on their stance, there are varying opinions on these issues within the community. One person wrote at the first Open House, “There needs to be sound barriers between roadway and housing areas.” Similarly, another person noted, “The fact that Pima County sees no need for sound abatement bothers me. I do not believe that this project would be presented to neighborhoods where the “perceived” income of the residents is higher.” In response to what they like best about the project one person shared a contrasting opinion, “No wall being put up.” Another person stated, “Forget the right-of-way purchase!”



Since PCDOT does not have additional funds to allocate to La Cholla, the CAC would like to request that the Board of Supervisors allocate the needed money for property acquisition. This request will go to the Board when they receive the EAMR.

8.3 Public Meetings

A public open house meeting was held on March 6, 2008. Approximately 84 people attended this meeting. A summary of the written comments received has been included in Appendix B.

A second open house was held on September 11, 2008 to obtain community input on the Draft Design Concept Report and the Draft Environmental Assessment and Mitigation Report. Approximately 50 people attended this meeting. Appendix B also includes a summary of the written comments from the second meeting.

There will be at least one more open house public meeting for this project. It will be held after the Stage IV plans have been submitted.

The public meetings are publicized through news releases distributed to the appropriate media and through display advertisements placed with the *Daily Territorial* and *Arizona Daily Star* a minimum of 15 days prior to each meeting. Meeting announcements are mailed to the project contact list of approximately 750 addresses, which comprise impacted residents and businesses as well as elected officials. Corresponding information is posted on the dedicated project Web site. Sign-in sheets are provided to record attendance at the meetings, and attendees are asked to submit comments on the forms provided at the meetings. Comments submitted during a two-week period following each meeting are documented and summarized for the project team.

A public hearing will be held before a meeting of the Pima County Board of Supervisors upon completion of the EAMR.



9.0 Agency Coordination

9.1 Environmental Review

Coordination is anticipated with the following agencies: City of Tucson, Pascua Yaqui Tribe, Tohono O’odham Nation, State Historic Preservation Office (SHPO), Arizona Game and Fish Department (AGFD), U.S. Fish and Wildlife Service, U.S. Army Corps of Engineers, and the Arizona Department of Environmental Quality (ADEQ). Coordination with the tribes and SHPO will be initiated during the cultural resources consultation. Coordination with AGFD through the use of their online Environmental Review Tool was initiated on September 27, 2007. A list of species within three miles of the project vicinity was provided, as well as project related recommendations. During the preparation of the Phase I Environmental Site Assessment, a representative of ADEQ’s Tank Programs Division was contacted via telephone and the pertinent files regarding known underground storage tanks and leaking underground storage tanks cases were reviewed at ADEQ.

9.2 Intergovernmental Agreements

The City of Tucson has a proposal to annex an area named “River-La Cholla Annexation District 2”. This area includes La Cholla Boulevard from just north of Curtis Road to Sunset Road. No date has been set for this matter to go to the Mayor and Council. If it is successful, an Intergovernmental Agreement will probably be required.



10.0 Alternatives

10.1 Location

Alternatives Description

This section documents an alignment alternative study for La Cholla Boulevard between Ruthrauff Road and River Road. The reason that an alignment study is needed is due to the presence of single family homes located on both sides of La Cholla Boulevard at the south end of the project. These homes don't have sufficient room on their property to build circular driveways. Therefore, some approach is needed to prevent vehicles from having to back out directly into traffic. The alternatives investigated under this study include the following:

Alternative A

This alternative involves shifting the La Cholla Boulevard centerline to the east about 40 feet. This provides enough room to build a frontage road to serve the homes on the west side. It would require the purchase of 13 lots on the east side along with some partial takes from other properties. The layout for this alternative is shown in Figure 12 with the typical roadway section shown in Figure 13.

Alternative B

This alternative involves shifting the La Cholla Boulevard centerline to the west about 40 feet. This provides enough room to build a frontage road to serve the homes on the east side. It would require the purchase of 12 lots on the west side including a City well site along with some partial takes from other properties. The layout for this alternative is shown in Figure 14 with the typical roadway section shown in Figure 15.

Alternative C

This alternative involves maintaining La Cholla Boulevard centered on the section line as shown in Figure 16. The typical section, as shown in Figure 17, would include an additional pullout lane outside of the bike lane to provide space for the homeowners to back out a car without entering the La Cholla Boulevard travel lanes. Right of way would not be required from either side of the street, but temporary construction easements would be needed to rebuild the driveways.

Alternative D

This alternative involves maintaining La Cholla Boulevard centered on the section line as shown in Figure 18. However, as shown in Figure 19, no pullout lanes nor frontage roads would be used to provide space for the homeowners to back out a car. Therefore, the fronting residences on both sides of La Cholla Boulevard would need to be acquired. However, no other right of way would be needed.



Alternative E

This alternative was created to provide an option in which right-of-way was not required from either side of La Cholla Boulevard. It involves maintaining La Cholla Boulevard centered on the section line as shown in Figure 20. As shown in Figure 21, the median, travel and bike lanes have been reduced in width. According to the Highway Capacity Manual (2002) an 11 foot lane reduces the saturation flow rate by 3% when compared to a 12 foot lane. Also, 16 foot frontage roads are used on both sides as one-way streets. The medians between the frontage roads and La Cholla Boulevard are four feet. If noise walls are required, a median barrier could be placed in the four feet with a noise wall built on top. While no right-of-way must be purchased from the fronting residences, it will be necessary to acquire temporary construction easements to build the small slopes and reconstruct the driveways.

As stated in the 2004 AASHTO, A Policy on Geometric Design of Highways and Streets, lane widths typically vary from 10-ft to 12-ft. The 12-ft lane widths are most desirable and should be used, where practical, on higher speed, free-flowing, principal arterials. The AASHTO guide states that 11-ft lanes are normally adequate under interrupted-flow operating conditions at low speeds (45 mph or less). Research on the relationship between lane width and traffic crashes found no statistically significant relationship between lane width and crash rate on arterial streets (Transportation Research Board, NCHRP Report 282, Multilane Design Alternatives for Improving Suburban Highways 1986).

Alternative F

This alternative also maintains La Cholla Boulevard centered on the section line with the same typical section as Alternative D as shown in Figure 19. However, as shown in Figure 22, instead of purchasing all of the homes on both sides, only certain properties would be bought. These purchased properties would be used to build circular driveways for access to the remaining homes. Under this alternative, a total of eight properties would be purchased.

Variations on Alternative E

Several variations on Alternative E were also considered and rejected. These variations included the following:

- Replacing the median with a continuous two-way, left-turn lane. This was rejected because of the safety concerns of using a seven-lane section on a 50 mph roadway. AASHTO states on page 714 of their 2004 *Geometric Design of Highways and Streets*, “In general, two-way left-turn lanes should be used only in an urban setting where operating speeds are relatively low and where there are no more than two through lanes in each direction”.
- Replacing the frontage roads with direct driveways. Attached to each driveway would be a paved turnaround area built within the right-of-way.
- Providing additional openings in the frontage road medians along with left turn bays in the main median. This was rejected because it would force the narrow frontage roads to function as two-way rather than one-way.



- Widening the frontage roads from 16 feet to 20 feet so they could function as two-way. The median would then be reduced from 18 feet to 10 feet. This caused two problems. The narrowing of the median can't begin until the south end of the median nose for the left turn bay Calle Narcisco (approximately Station 55+50). This median geometry not only allows for the left turns onto Calle Narcisco, it provides for the required shadowing of the southbound left turns onto Ruthrauff Road. The taper must be at 50:1, therefore, the 10-foot wide median can't be achieved until approximately Station 57+50. As a result, we are forced to take right-of-way from six homes on the east side and two homes on the west side. Second, since we only have a 10-foot wide median at Jay Avenue, we can't provide a southbound left turn lane. Therefore, this movement would be forced down to Ruthrauff Road to make a u-turn.
- Reducing the four-foot separating medians to two feet and provide 18-foot frontage roads that are two-way. A sub-alternative to this option was using a concrete barrier in place of the two-foot median. The one-way frontage road was determined to be preferred over the narrow two-way for several reasons including:
 - It is safer operationally at the intersections with the side streets.
 - It allows the median width to remain at four feet instead of only two. AASHTO recommends a minimum width of four feet.
 - There is not the confusing headlight interaction between the traffic on La Cholla and the frontage roads.

A glare screen was discussed as a possible solution to the headlight problem, but it would be a significant maintenance problem.

Figure 12. Alternative A Plan View

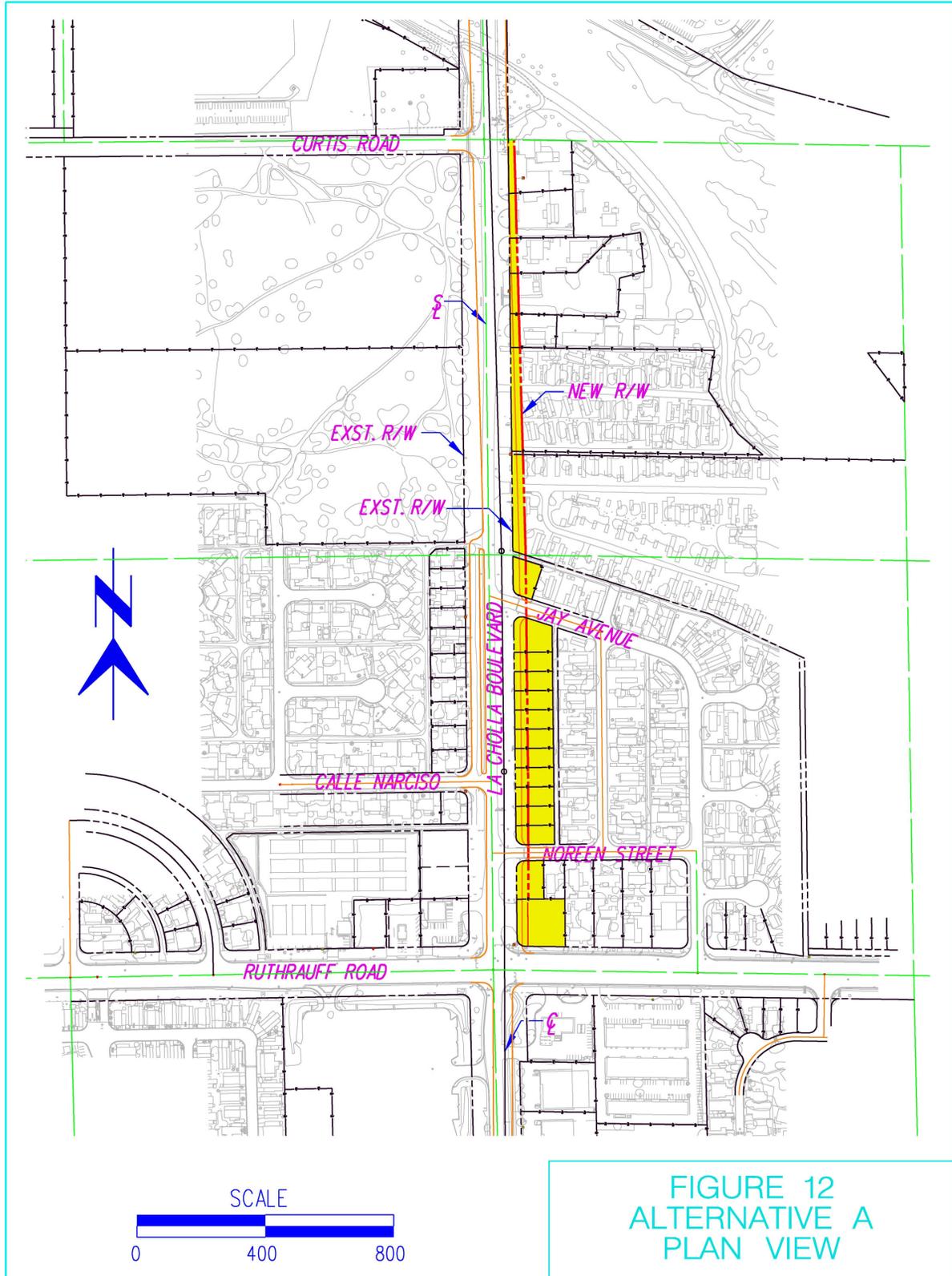




Figure 13. Alternative A Typical Section

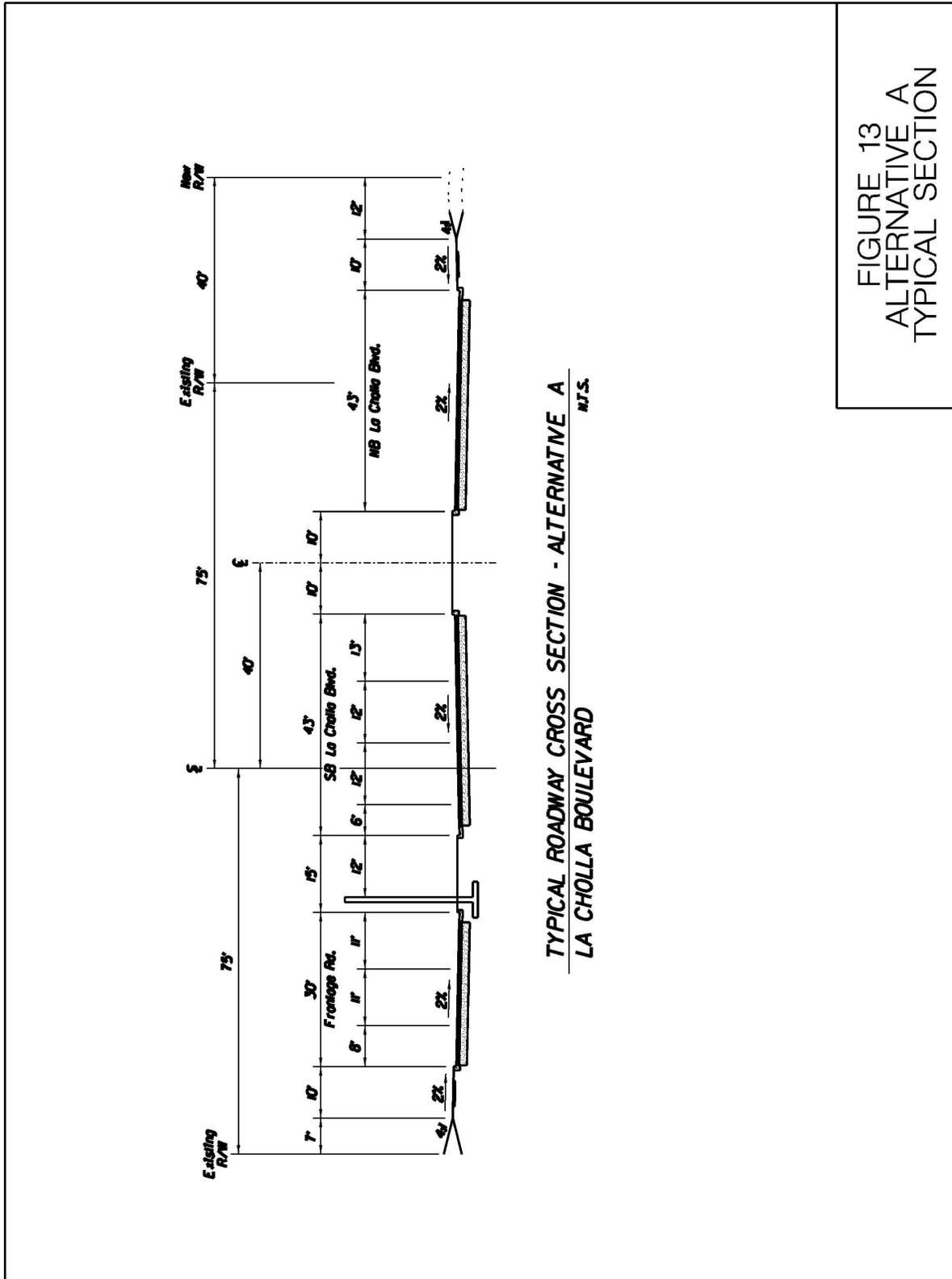


Figure 14. Alternative B Plan View

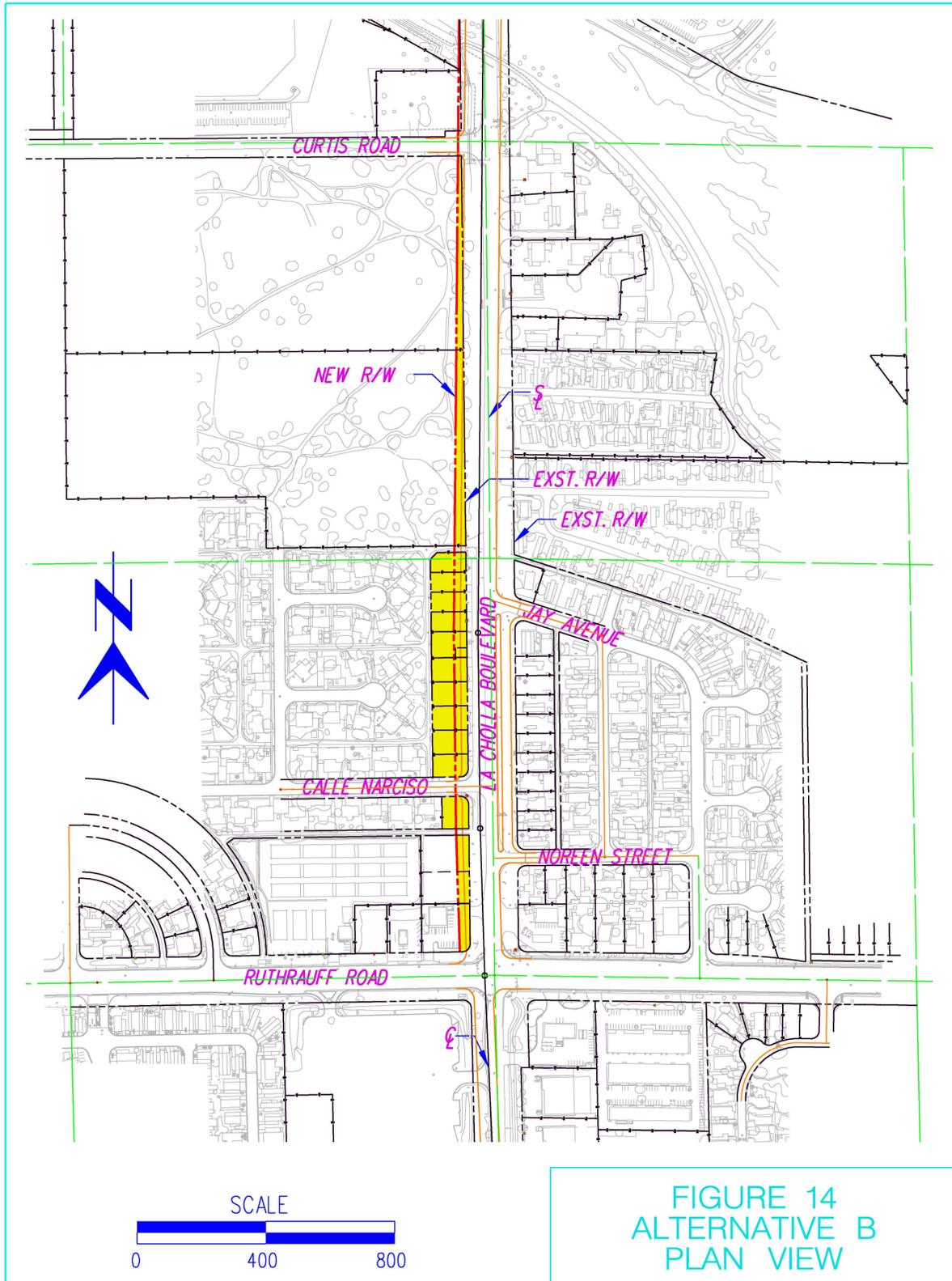


Figure 15. Alternative B Typical Section

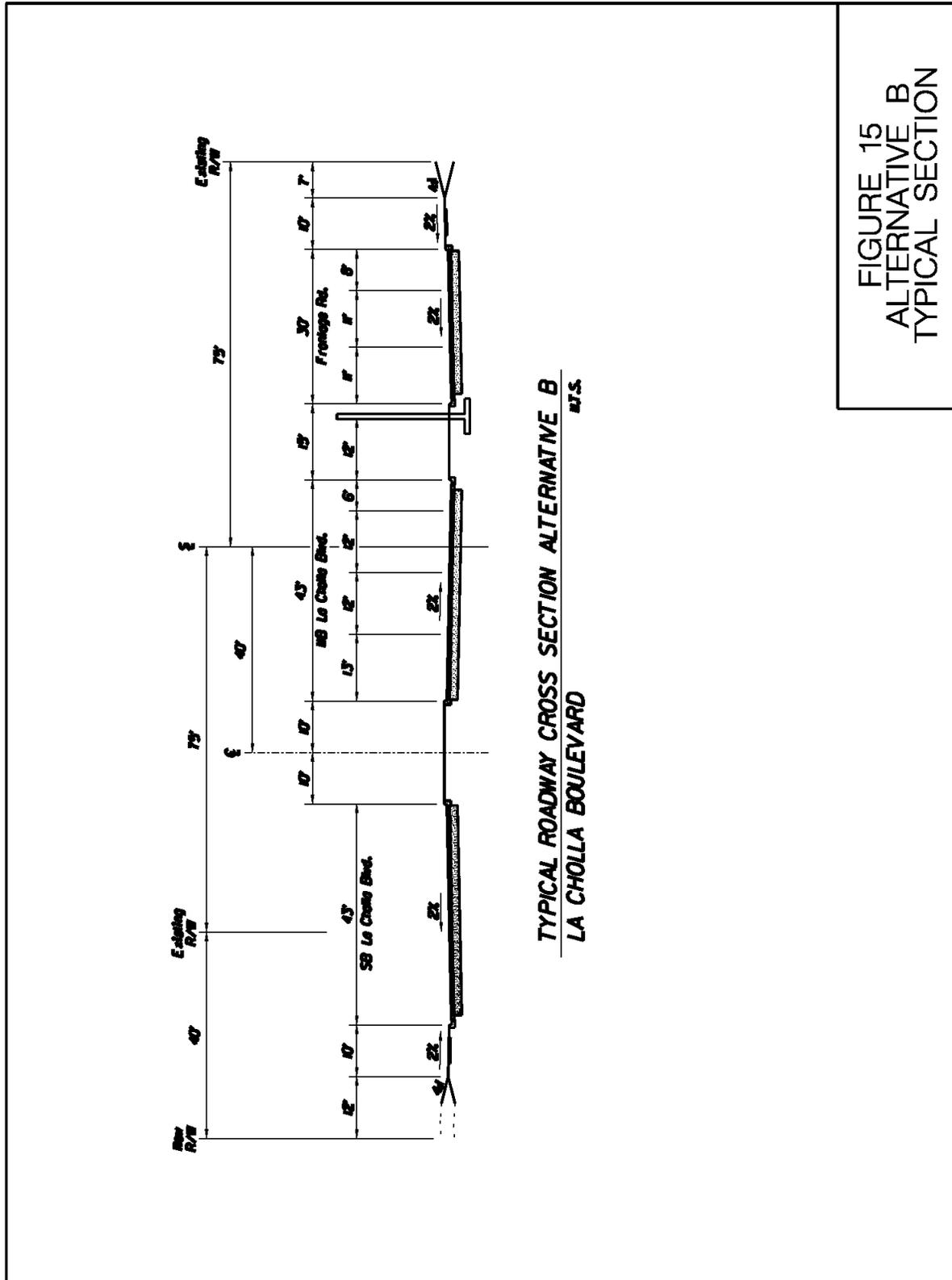


Figure 16. Alternative C Plan View

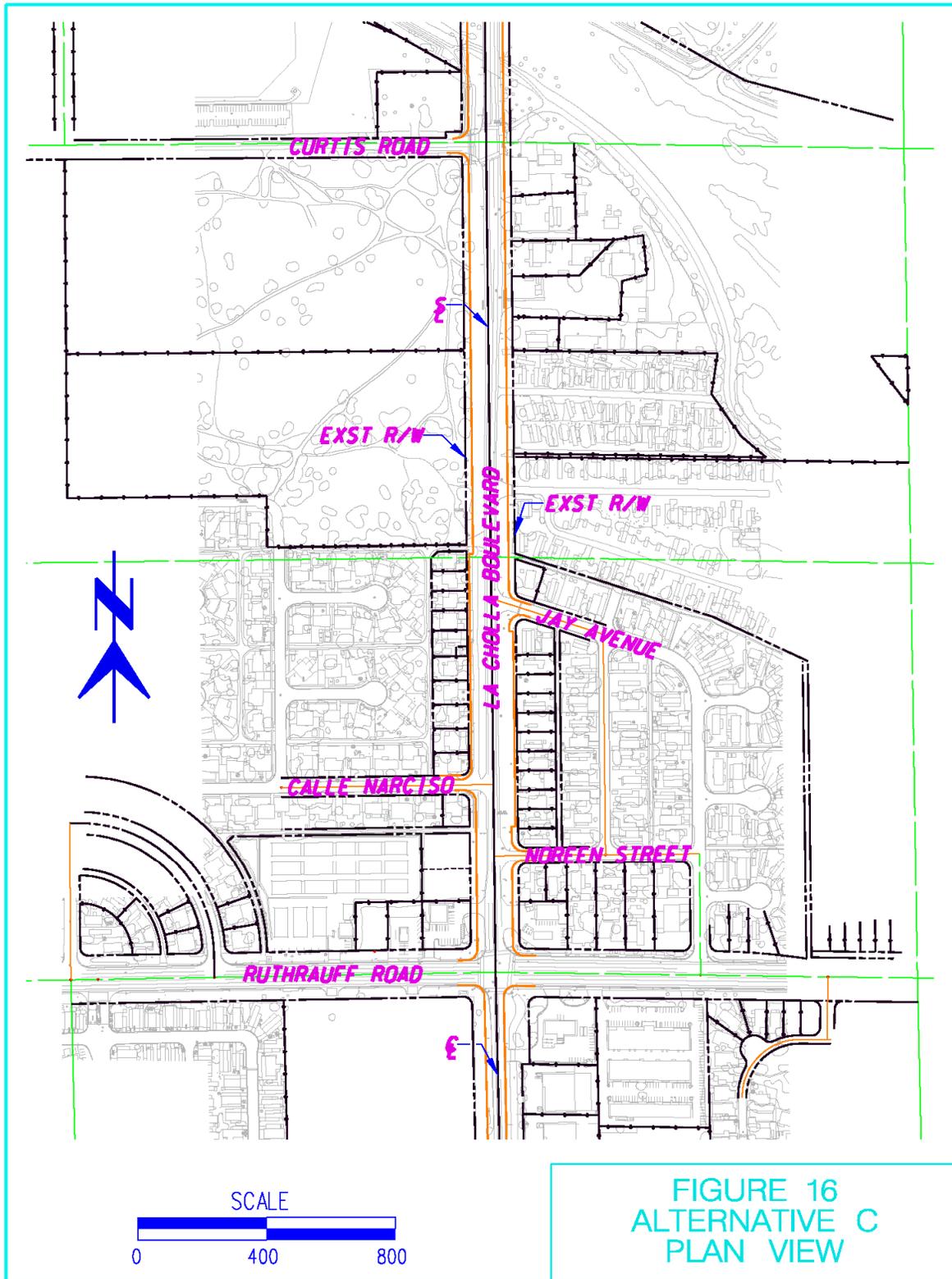


Figure 17. Alternative C Typical Section

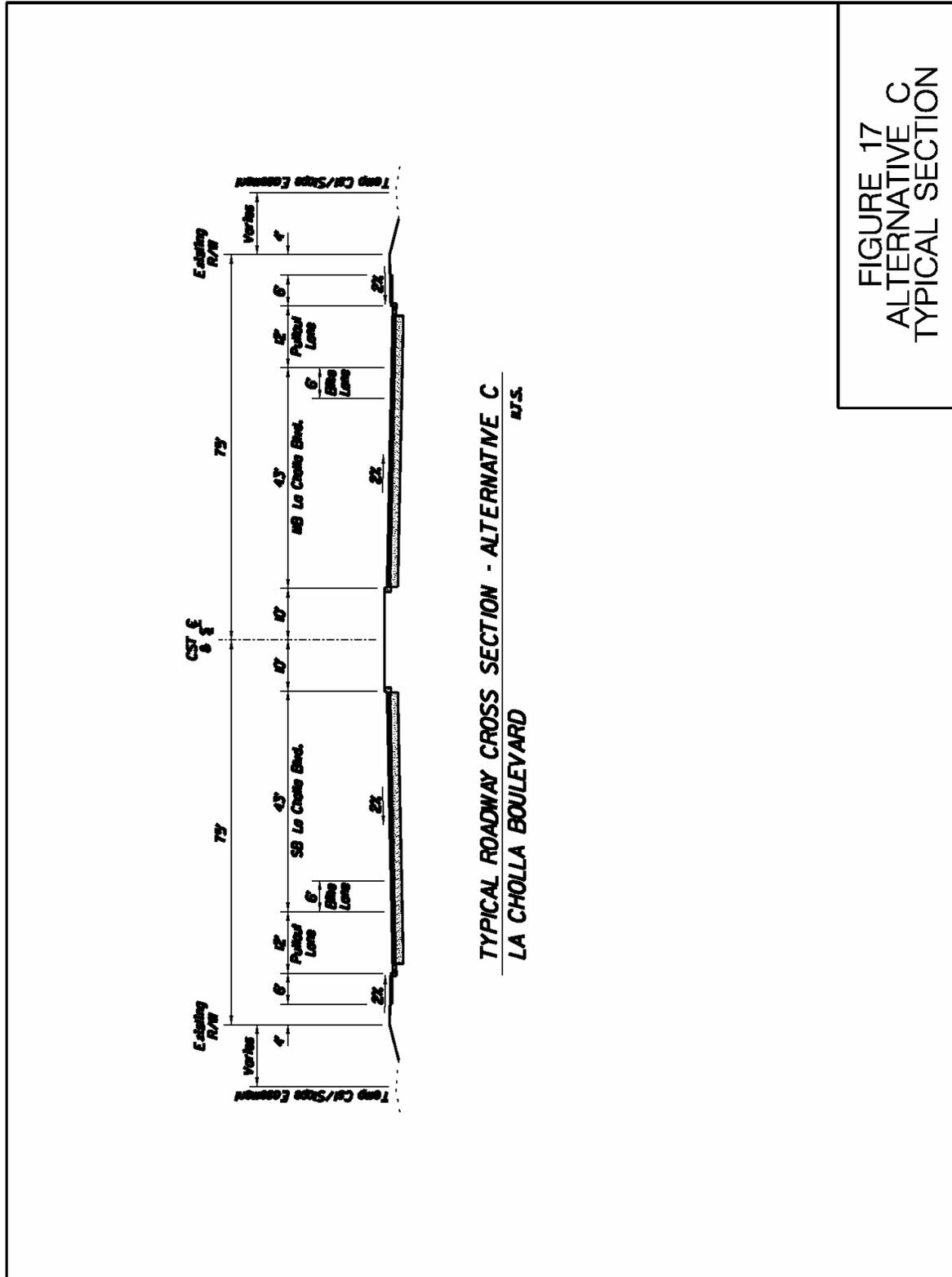


FIGURE 17
ALTERNATIVE C
TYPICAL SECTION

Figure 18. Alternative D Plan View

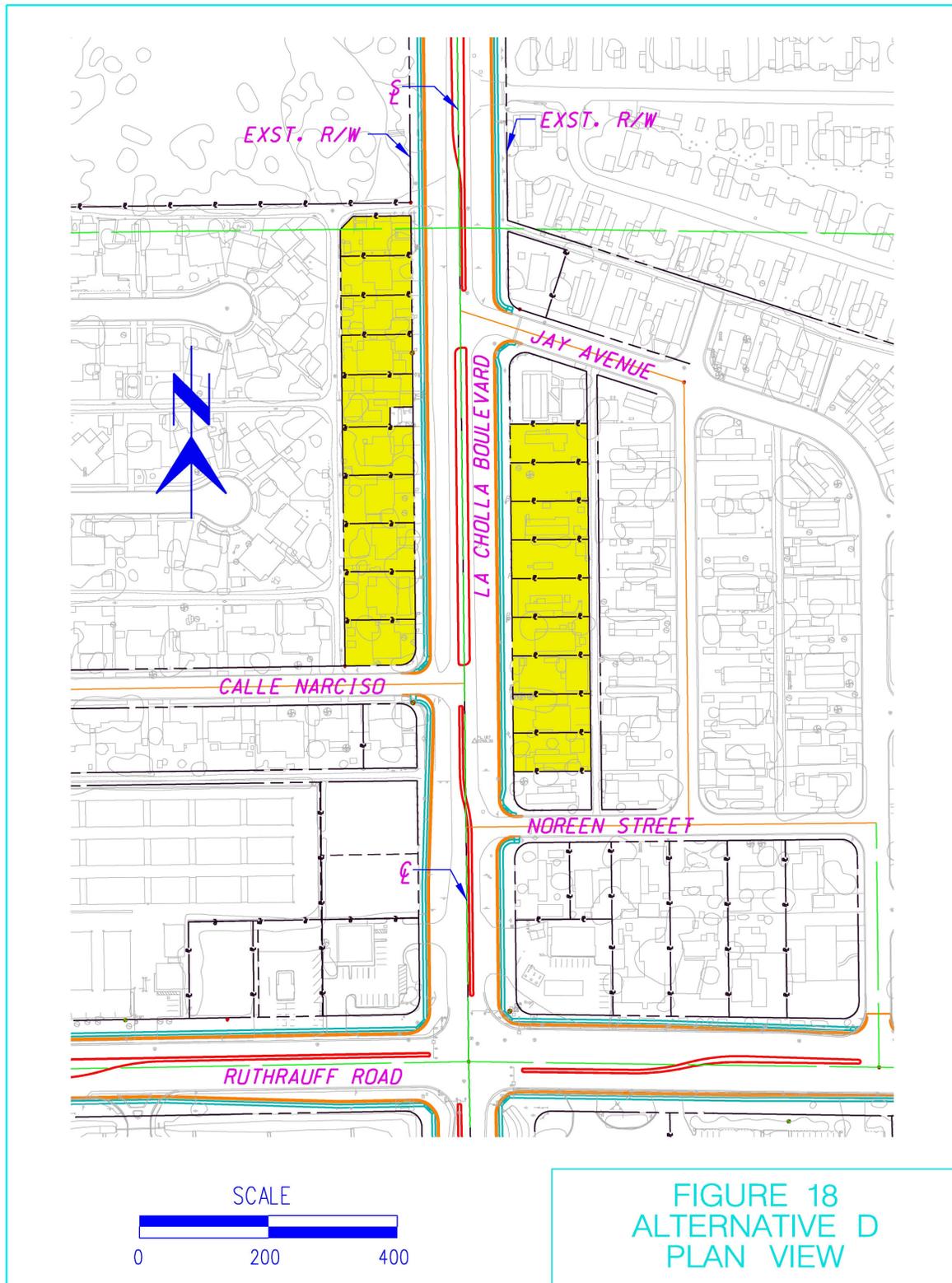


Figure 19. Alternative D Typical Section

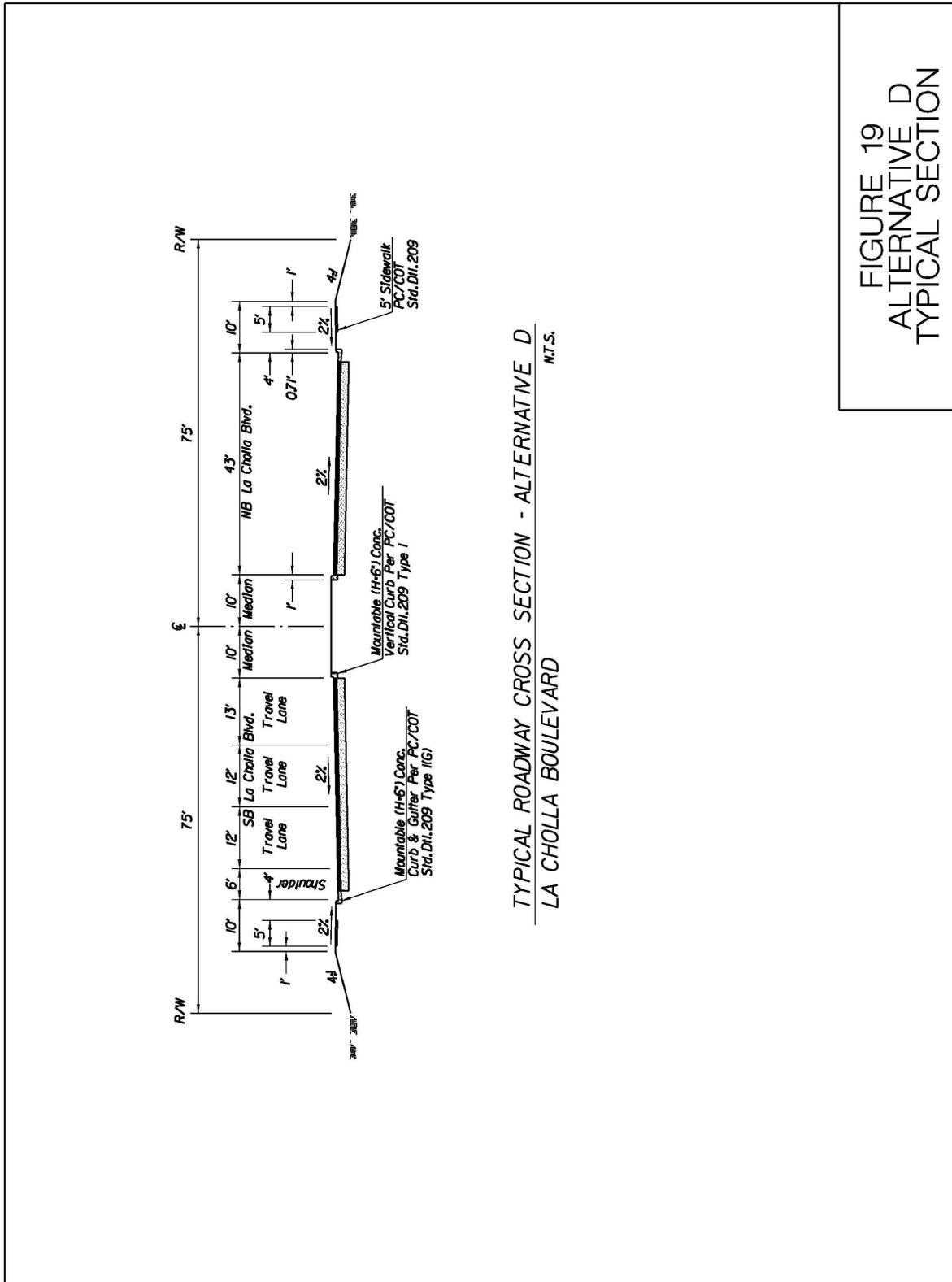


FIGURE 19
ALTERNATIVE D
TYPICAL SECTION

Figure 20. Alternative E Plan View

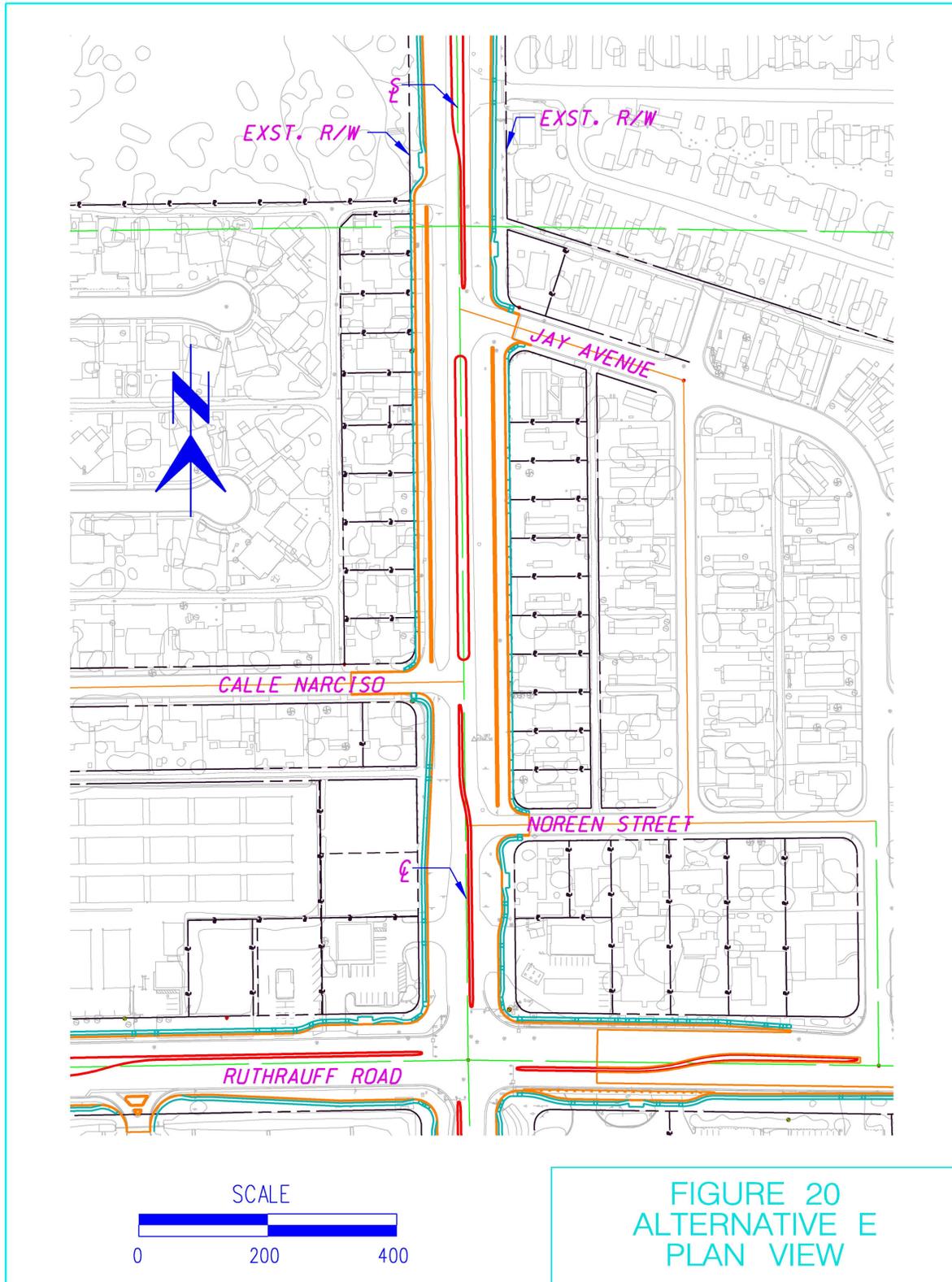




Figure 21. Alternative E Typical Section

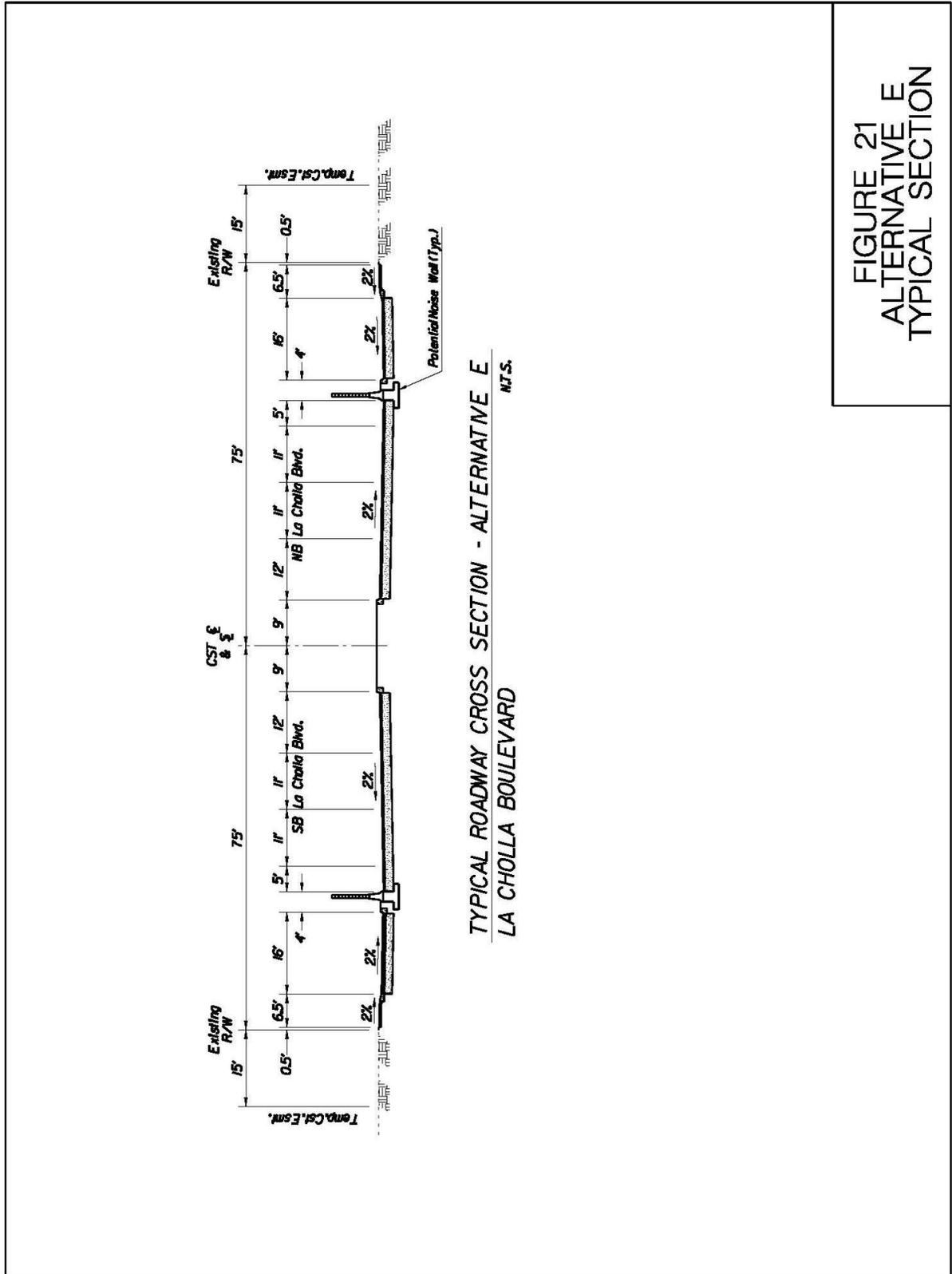
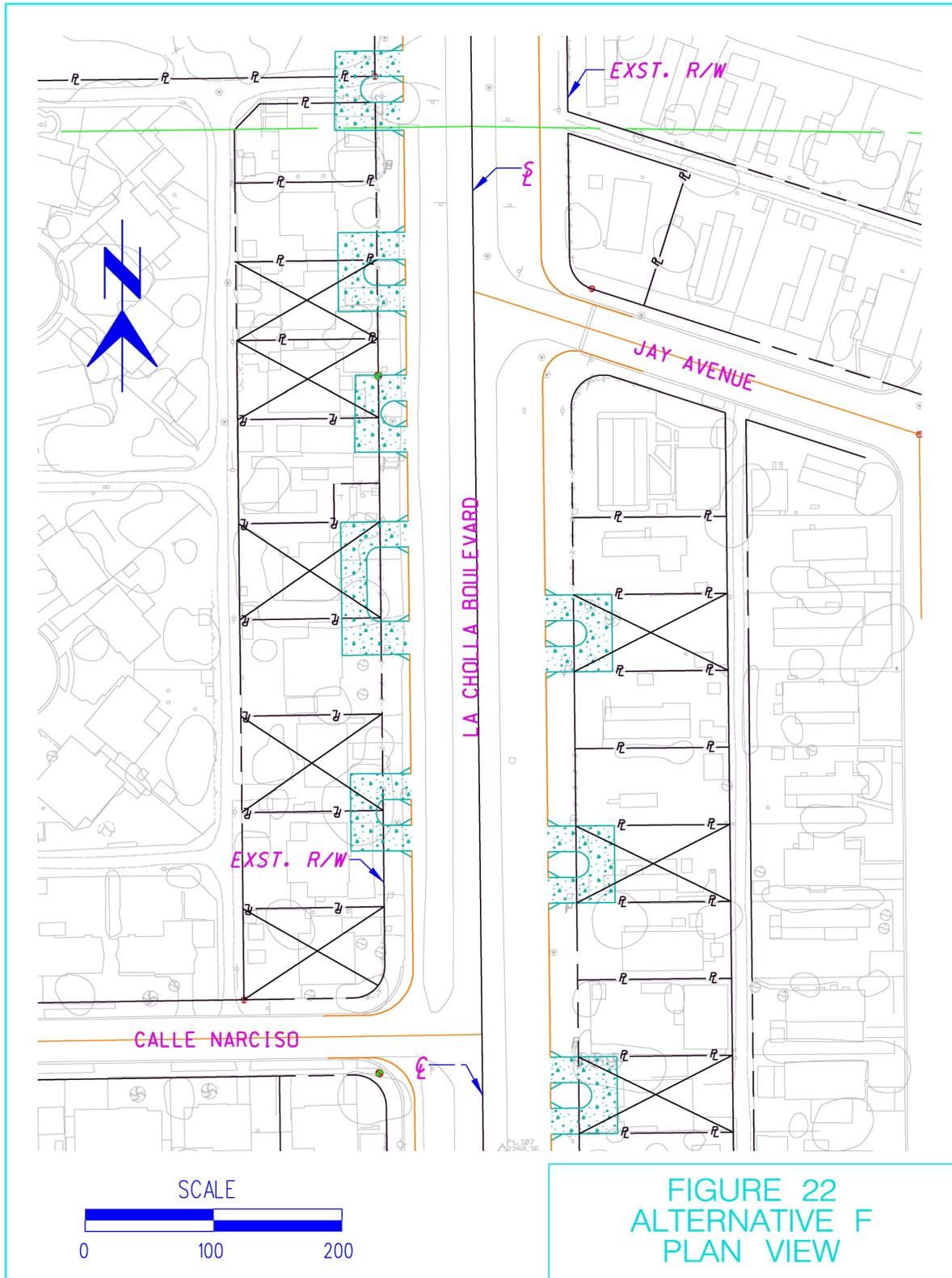


FIGURE 21
ALTERNATIVE E
TYPICAL SECTION

Figure 22. Alternative F Plan View





Right-of-Way Cost

The estimated cost of the right of way for each alternative was provided by Pima County Real Property. The estimated right of way costs for each alternative are as shown in Table 4:

Table 4. Right-of-Way Cost

Alternative	Right-of-way cost
A	\$4.3 million
B	\$3.6 million
C	\$0.3 million
D	\$3.8 million
E	\$0.3 million
F	\$2.0 million

The costs for Alternatives A, B, D and F may be mitigated to some extent since much of the acquisition will be total takes which will have unneeded areas that might be able to be reconfigured and resold. Alternatives A, B, D and F will result in excess property that could be reassembled and resold or it could be used for buffering the adjacent neighborhoods as discussed below. Since the remnant properties are so narrow, Real Property estimates that they would have minimal resale value. Alternatives C and E have no right of way cost, however, they will have some cost for temporary construction easements because the new sidewalk would be only one to four feet from both the east and west existing right of way lines in the area of the single family homes. Alternative F also includes the cost for temporary construction easements on those parcels that are not total takes.

Environmental

Environmental issues common to all alternatives:

All alternatives would require archaeological monitoring for construction activities within 100 feet of the Hodges Ruin, including utility work. Alternatives A, B, D and F would acquire residences along the project area. As a result, any buildings acquired would be subject to asbestos and lead surveys and demolition permitting through the National Emission Standards for Hazardous Air Pollutants (NESHAP) program with Pima County Department of Environmental Quality (PCDEQ).

Alternative A – The shifting of the alignment to the east would place the roadway closer to a construction waste landfill located in the southeast quadrant of the intersection of La Cholla Boulevard and Curtis Road. Subsurface hazardous materials investigations at the edge of the right-of-way adjacent to the landfill, and revealed minimal material; however, additional material could be present further east, and would require additional investigation. Nonetheless, construction waste landfills are normally less of a concern than general residential landfills.



A Circle K gasoline station in the northeast quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road would be impacted. Hazardous materials testing did not reveal any contaminants in the subsurface of the project right-of-way; however, this does not preclude the presence of contaminants in soil beneath this site, and would warrant additional investigation.

By acquiring the first row of mobile homes along La Cholla Boulevard, traffic noise would be buffered by distance to the remainder of the mobile homes in the area. Noise mitigation walls would be possible for the remainder of residences.

Alternative B – The shifting of the alignment to the west would place the roadway closer to a general residential landfill located in the southwest quadrant of the intersection of La Cholla Boulevard and Curtis Road. Subsurface hazardous materials investigations at the edge of the right-of-way adjacent to the landfill, and revealed minimal material; however, additional material could be present further east, and would require additional investigation. General residential landfills are normally more of a hazardous materials concern than construction waste landfills.

A former gasoline station in the northwest quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road would be impacted. Hazardous materials testing did not reveal any contaminants in the subsurface of the project right-of-way; however, this does not preclude the presence of contaminants in soil beneath this site, and would warrant additional investigation.

By acquiring the first row of residences along La Cholla Boulevard, traffic noise would be buffered by distance to the remainder of the residences in the area. Noise mitigation walls would be possible for the remainder of residences.

Alternatives C and E – Maintaining the existing alignment and eliminating right-of-way acquisitions would prevent the roadway from approaching either the general residential landfill located in the southwest quadrant of the intersection of La Cholla Boulevard and Curtis Road or the construction waste landfill located in the southeast quadrant of the intersection of La Cholla Boulevard and Curtis Road.

Neither the former gasoline station in the northwest quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road nor the Circle K gasoline station in the northeast quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road would be impacted.

No buildings would be acquired; therefore, asbestos and lead surveys and demolition permitting through the NESHAP program with PCDEQ would not be required.

Noise mitigation through the use of noise abatement walls could not be accomplished under Alternative C. For Alternative E, noise walls could be built on top of the frontage road median barriers. However, the length of the noise walls will be limited by the need for an attenuator at one end and the sight visibility required at the side street intersections.

Alternatives D and F – Maintaining the existing alignment and eliminating right-of-way acquisitions in areas other than the residential areas would prevent the roadway from approaching either the general residential landfill located in the southwest quadrant of the intersection of La Cholla Boulevard and Curtis Road or the construction waste landfill located in the southeast quadrant of the intersection of La Cholla Boulevard and Curtis Road.



Neither the former gasoline station in the northwest quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road nor the Circle K gasoline station in the northeast quadrant of the intersection of La Cholla Boulevard and Ruthrauff Road would be impacted.

By acquiring the first row of residences along both sides of La Cholla Boulevard under Alternative D, traffic noise would be buffered by distance to the remainder of the residences in the area. Noise mitigation walls would be possible for the remainder of residences, including those north of Jay Avenue on the east side of La Cholla Boulevard. Under Alternative F, since only a few of the residences would be acquired, noise mitigation through the use of noise abatement walls could not be accomplished along the single family homes. Noise walls would be possible north of Jay Avenue.

Displacement of Residences

Some of the alternatives would require either single family homes, duplexes or mobile homes to be purchased with their residents being required to relocate. Some of the homes are owner occupied while others are rentals. The number of relocations for each alternative is as shown in Table 5:

Table 5. Home Acquisition Requirements

Alternative	Single-family home acquisitions	Duplex acquisitions	Mobile home acquisitions
A	0	3	16
B	11	0	0
C	0	0	0
D	10	3	8
E	0	0	0
F	5	2	2

Impacts to Businesses

Impacts to existing businesses consist of either complete acquisition or a partial take in which the business can continue to operate. Under Alternative A, the Circle K on the northeast corner of Ruthrauff Road and La Cholla Boulevard would be a total take. Partial takes would be required from the businesses located along the east side of La Cholla Boulevard, just south of Curtis Road, but they would be able to continue to operate. These businesses include Finish Line Collision Repair Center, Specialty Auto, Montgomery Auto Repair, and Montgomery Automotive & Trailer Sales.

For Alternative B, the Family Food Store on the northwest corner of Ruthrauff Road and La Cholla Boulevard would be a total take. There is also a parcel for which a development plan has been submitted to Pima County for a Family Dollar Store. This development is currently on hold. Alternatives C through F do not directly impact any businesses.



Bridge Location

Moving the bridge further west under all of the alternatives will create the need to lower the bike path under the bridge and thereby require the reconstruction of the storm drain outfall. Alternative B causes the greatest shift to the west, but the bridge location is similar with all of the alternatives.

Impacts to Utilities

The impact to the underground utilities will be similar for all of the alternatives. This impact will mainly occur due to the construction of the new storm drain system. This will require the relocation or lowering of water, gas, telephone and traffic signal conduits. Sanitary sewer lines will need to be relocated or replaced with ductile iron pipe at the storm drain crossings.

It is the impact to the above ground facilities, including overhead Tucson Electric Power (TEP) lines, in which the alternatives differ. Alternative A will require the relocation of the overhead joint 46 kV & 14 kV electric lines along the east side of La Cholla Boulevard from Ruthrauff Road to just north of Jay Avenue. Between Jay Avenue and Curtis Road, there are another four distribution poles that may need to be moved. However, there will be new right-of-way into which TEP can relocate. This alternative will also impact a valve station for a 4" high pressure gas line owned by Southwest Gas located on the northeast corner of Ruthrauff Road and La Cholla Boulevard.

Alternative B will require the relocation of an overhead 46 kV electric line from Calle Narciso to Curtis Road into new right-of-way. This alternative will also move the bridge closer to the large steel TEP poles and overhead lines at the Rillito River. While this shouldn't require the relocation of the TEP line it will make drilling the new bridge piers more difficult. Alternative B will also impact the Qwest telephone line from Ruthrauff Road to the north end of the single family homes. Although the underground line probably won't be affected there will need to be relocation of some pedestals. Finally Alternative B would require the relocation of a Tucson Water well site, which must be within 500 feet of the existing location.

Alternative C will require the relocation of the overhead TEP lines on both sides of La Cholla Boulevard along the single family homes. However, since new right-of-way is not being purchased, this will put TEP in the position of having to either buy their own easements across the private properties or put their lines underground both of which will be expensive propositions. TEP recently put 1000 feet of 46kV and 14 kV lines underground at a cost of \$750,000. Also, the transformer for the Tucson Water well site located on the west side will need a 15' by 15' transformer easement on private property if the poles can't be maintained. Qwest will need 5' by 5' easements on private property for their pedestals. The large expense for undergrounding the TEP lines makes it an improbable option.

For Alternative E, TEP will need to relocate their overhead lines. This will have to be accomplished by using high, long spans since undergrounding will probably be too expensive. Qwest will need 5' by 5' easements on private property for their pedestals.

Alternatives D and F will have the least impact to the above ground utilities.



Traffic Operations

Alternatives A, B, D and F use standard roadway typical sections that have been successfully used in Pima County. The frontage road provides a secure separation between the vehicles backing out of the driveways and the high speed traffic on La Cholla Boulevard. The roadway section for Alternative C is not typically used locally nor is it described in the AASHTO Roadway Standards. Therefore, PCDOT will probably be assuming an increased liability in using Alternative C. Also, the pullout lane built in Alternative C will be an attractive area for on-street parking which would defeat its ability to be used for its intended purpose.

Alternative E uses narrower traffic lanes (11' versus 12') and bike lanes (5' versus 6'). As described previously, while these narrower lanes meet AASHTO requirements, the wider lanes are preferred on high speed arterials. Alternatives C and F are the only alternatives that have residential driveways that connect directly to La Cholla Boulevard.



Alternatives Matrix

A summary of the impacts for each alternative is provided in Table 6.

Table 6. Alternatives Matrix

Criteria	Comments	A	B	C	D	E	F
Cultural resources	There are no known cultural sites in the area of the single family homes.	□	□	□	□	□	□
Hazardous materials	Alternatives A and B require the acquisitions of existing or old gas stations. They also move the new road closer to old landfills.	⊗	⊗	□	□	□	□
Noise impacts on neighborhood	Alternatives A, B, D, and E allow for noise wall mitigation, although the extent of walls for E may be limited by sight distance. Alternatives C and F do not allow for walls.	□	□	⊗	□	●	⊗
Residential displacements	Assumes that relocations from single-family homes are more difficult than mobile homes.	●	⊗	□	⊗	□	●
Business impacts	Alternatives A and B involve total business takes.	⊗	⊗	□	□	□	□
Utility impacts	These mainly involve costs to Tucson Electric Power.	●	⊗	⊗	□	⊗	□
Traffic operations		□	□	⊗	□	●	●
Bridge impacts		□	●	□	□	□	□
Aesthetic impacts	Alternatives A, B, and D have plenty of room for landscaping.	□	□	⊗	□	⊗	●
Construction cost (\$ millions)	The construction cost difference is minor.	17.8	17.8	17.5	17.3	18.2	17.5
Right-of-way cost (\$ millions)	Estimated cost provided by Pima County.	4.3	3.6	0.3*	3.8	0.3*	2.0
Planning and design cost (\$ millions)		2.4	2.4	2.4	2.4	2.4	2.4
Total project cost (\$ millions)	Budget from Regional Transportation Authority is \$16.3 million	24.5	23.8	20.2*	23.5	20.9*	21.9

□ = no impact or minor impact

● = moderate impact

⊗ = major impact

* = R/W cost is uncertain because severance damage is unclear.

These costs are based on preliminary design plans from the fall of 2007. As design progresses, the estimates are changing as well.

Final Direction

Considering safety, operations, cost and impacts, PCDOT recommends Alternative E.



11.0 Conclusions and Recommendations

This project should be designed as described in this report. The design criteria to be used are listed in Section 5.0. Other important criteria for this project include:

- Alignment Alternative E shall be used to prepare the final design.
- Noise walls shall be designed for those locations anticipated to have a noise level of 66.0 dBA or above in the next twenty years. If noise walls are not feasible, the use of rubberized asphaltic concrete may be considered as mitigation.
- Existing native vegetation shall be salvaged and replanted in accordance with the Native Plant Preservation Ordinance (NPPO).
- No new traffic signals are warranted for this project. Existing traffic signals at Ruthrauff Road, Curtis Road and River Road shall be upgraded as necessary to accommodate the proposed roadway improvements.
- A new bridge shall be designed to replace the existing bridge over the Rillito River. The spans and piers of the new bridge shall be offset from those for the existing bridge to ease construction of the new piers and eliminate the need to pull the old piles. The superstructure shall consist of a concrete deck over Type V (Modified) AASHTO precast concrete girders.
- Dual left turn lanes shall be designed for the northbound and eastbound legs of the Ruthrauff Road intersections. No additional improvements shall be made to the River Road intersection.



12.0 Cost Estimate and Budget Considerations

The project cost shown in Table 7 below is based on the preliminary design performed to date and will be refined as more detailed plans are prepared.

Table 7. Project Cost

Task	Cost
Construction Cost	\$19,000,000
Artwork	\$250,000
Right-of-way	\$500,000
Design and planning	\$2,400,000
Construction administration	\$2,800,000
Total	\$24,950,000

For a breakdown of the construction cost see Appendix A. Note that the construction cost differs somewhat from the costs given previously in Table 6. This is due to the alternatives study being performed in the fall of 2007 and the design has been refined since then. However, the difference in cost between the alternatives shown in Table 6 is still valid. The private utility relocation will be paid for by the individual utility companies, and therefore, is not included in the budget. The utility relocation costs that will be borne by this project have been included in the construction cost. The total cost to Pima County for this project is estimated to be \$25.0 million. The budget for the project is currently \$20.0 million.



13.0 References

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3. "Pima County Roadway Design Manual", Pima County Department of Transportation and Flood Control District, Second Edition, December, 2003.
4. "Roadside Design Guide", American Association of State Highway and Transportation Officials, Third Edition, 2006.
5. "Standard Details For Public Improvements", City of Tucson/Pima County, 2003 Edition, Revised April 19, 2004.
6. "Guide For the Development of Bicycle Facilities", American Association of State Highway and Transportation Officials, 1999.
7. "Manual of Uniform Traffic Control Devices", U.S Department of Transportation, Federal Highway Administration, 2003 Edition.
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9. "Stage I Drainage Report For La Cholla Boulevard, Ruthrauff Road to River Road W.O No. 4LCITR", HDR, Inc., December, 2007.
10. "Draft Geotechnical Report, La Cholla Boulevard – Ruthrauff Road to River Road", NCS Consultants, LLC, June 2, 2008.
11. "Draft Bridge Selection Report For La Cholla Boulevard, Ruthrauff Road to River Road W.O No. 4LCITR", HDR, Inc., February, 2008.
12. "Evaluating Scour at Bridges," U.S Department of Transportation, Federal Highway Administration, Hydraulic Engineering Circular No. 18, Fourth Edition, May, 2001.
13. "HEC-RAS River Analysis System User's Manual," Hydrologic Engineering Center, U.S Army Corps of Engineers, Version 3.0, January, 2001.
14. "Final Pavement Design Report, La Cholla Boulevard, Ruthrauff Road to River Road", HDR, Inc., July, 2008.
15. "Cultural Resources Assessment of the La Cholla Boulevard—Ruthrauff Road to River Road Project," Cook, Patricia; Desert Archaeology, Inc., 2008.
16. "Phase I Environmental Site Assessment, North La Cholla Boulevard: West Ruthrauff Road to River Road," HDR Engineering, Inc., 2007.
17. "Preliminary Site Investigation, North La Cholla Boulevard: West Ruthrauff Road to River Road," HDR Engineering, Inc., 2008.
18. "Final Noise Report, La Cholla Boulevard, Ruthrauff Road to River Road," HDR Engineering, Inc., 2008.



14.0 Abbreviation and Acronyms

AASHTO	American Association of State Highway and Transportation Officials
ADEQ	Arizona Department of Environmental Quality
ADOT	Arizona Department of Transportation
AGFD	Arizona Game and Fish Department
Corps	U.S. Army Corps of Engineers
County	Pima County
DCR	Design Concept Report
EAMR	Environmental Assessment and Mitigation Report
EA	Environmental Assessment
EPG	Environmental Planning Group (ADOT)
ESA	Endangered Species Act
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
GIS	geographic information system
LOS	level of service
Manual	Pima County Department of Transportation Roadway Design Manual (2003)
NEPA	National Environmental Policy Act
NPDES	National Pollutant Discharge Elimination System
PAG	Pima Association of Governments
PCRWRD	Pima County Regional Water Reclamation Department
PLSS	Public Land Survey System
PS&E	plans, specifications, and estimates
ROE	right-of-entry
R/W	right-of-way
TDM	transportation demand management
USFWS	U.S. Fish and Wildlife Service



Appendix A

Construction Cost Estimate

PRELIMINARY
NOT FOR CONSTRUCTION

STAGE I COST ESTIMATE

LA CHOLLA BOULEVARD
RUTHRAUFF ROAD TO RIVER ROAD

Project No. 4LCITR

October 1, 2008

ITEM NO.	QTY	UNIT	ITEM DESCRIPTION	UNIT PRICE	AMOUNT
1070001	1	L. SUM	NPDES (ORIGINAL)	\$ 60,000.00	\$60,000
1070011	40,000	F.A.	NPDES (MODIFICATIONS)	\$ 1.00	\$40,000
1090001	50,000	F.A.	FUEL AND MARKET ADJUSTMENT	\$ 1.00	\$50,000
2010011	8	ACRE	CLEARING AND GRUBBING	\$ 1,000.00	\$8,000
2020001	1	L. SUM	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	\$ 20,000.00	\$20,000
2020002	1	L. SUM	REMOVAL OF BRIDGE	\$ 250,000.00	\$250,000
2020020	2,500	L.FT.	REMOVAL OF CURB	\$ 3.00	\$7,500
2020029	38,500	SQ.YD.	REMOVAL OF BITUMINOUS PAVEMENT	\$ 2.00	\$77,000
2020034	1	L. SUM	REMOVAL OF SIGNS AND DELINEATORS	\$ 5,000.00	\$5,000
2020040	500	L.FT.	REMOVAL OF PIPE	\$ 15.00	\$7,500
2020061	30	EACH	RELOCATE MAILBOX	\$ 200.00	\$6,000
2020071	400	L.FT.	REMOVAL OF GUARDRAIL	\$ 6.00	\$2,400
2020101	500	L.FT.	REMOVAL OF FENCE	\$ 5.00	\$2,500
2020200	1	L. SUM	REMOVE & SALVAGE TRAFFIC SIGNAL EQUIPMENT AND CONDUCTORS	\$ 15,000.00	\$15,000
2020300	18,300	CU.YD.	ROADWAY EXCAVATION	\$ 8.00	\$146,400
2030401	5,000	CU.YD.	DRAINAGE EXCAVATION	\$ 10.00	\$50,000
3030003	13,000	CU.YD.	AGGREGATE BASE	\$ 30.00	\$390,000
4040111	16	TONS	TACK COAT	\$ 1,000.00	\$16,000
4060001	10,500	TONS	ASPHALTIC CONCRETE (NO. 1)	\$ 80.00	\$840,000
4060002	1,000	TONS	ASPHALTIC CONCRETE (NO. 2)	\$ 80.00	\$80,000
4140040	6,200	TONS	ASPHALTIC CONCRETE (ASPHALT-RUBBER)	\$ 125.00	\$775,000
4140042	620	TONS	ASPHALT RUBBER MATERIAL (FOR AR-AC)	\$ 400.00	\$248,000
4140044	62	TONS	MINERAL ADMIXTURE (FOR AR-AC)	\$ 100.00	\$6,200
5011012	320	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 18"	\$ 90.00	\$28,800
5011023	250	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 24"	\$ 100.00	\$25,000
5011033	450	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 30"	\$ 130.00	\$58,500
5011053	850	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 48"	\$ 250.00	\$212,500
5011058	40	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 54"	\$ 350.00	\$14,000
5011063	3,030	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 60"	\$ 400.00	\$1,212,000
5011068	1,530	L.FT.	PIPE, REINFORCED CONCRETE, CLASS III, 66"	\$ 450.00	\$688,500
5030718	32	EACH	CATCH BASIN, TYPE 3, L=16' (D=<8')	\$ 10,000.00	\$320,000
5030775	6	EACH	CATCH BASIN (SPECIAL NO. 1)	\$ 15,000.00	\$90,000
5050000	2	EACH	STORM DRAIN MANHOLE (DTL. 300)	\$ 7,000.00	\$14,000
5050001	10	EACH	STORM DRAIN MANHOLE (DTL. 301)	\$ 9,000.00	\$90,000
508XXXX	1	L.SUM	SEWER RELOCATION	\$ 300,000.00	\$300,000
510XXXX	1	L.SUM	WATER RELOCATION (PCDOT SHARE)	\$ 200,000.00	\$200,000
5110001	50,000	F.A.	MISCELLANEOUS UTILITY RELOCATION	\$ 1.00	\$50,000
511000X	100	EACH	UTILITY POTHOLING	\$ 500.00	\$50,000
601XXXX	37,800	SQ.FT.	BRIDGE	\$ 125.00	\$4,725,000
6016087	1	EACH	CONCRETE HEADWALL (LARGE)	\$ 40,000.00	\$40,000
601XXXX	2	EACH	CONCRETE HEADWALL (PC/COT STD DTL 313)	\$ 5,000.00	\$10,000
608XXXX	1	L. SUM	SIGNING	\$ 35,000.00	\$35,000

7010001	1 L.SUM	MAINTENANCE AND PROTECTION OF TRAFFIC	\$ 200,000.00	\$200,000
7010006	900,000 F.A.	CONSTRUCTION ARE ELEMENTS (PREDETERMINED REIMBURSEMENT RATES)	\$ 1.00	\$900,000
704XXXX	1 L. SUM	PAVEMENT MARKING	\$ 15,000.00	\$15,000
731XXXX	1 L.SUM	STREET LIGHTING (AT SIGNALS ONLY)	\$ 20,000.00	\$20,000
7320045	8,000 L. FT.	ELECTRICAL CONDUIT (PVC) (4" FIBER OPTIC W/ 4-1" INTERDUCTS)	\$ 20.00	\$160,000
733XXXX	2 EACH	TRAFFIC SIGNALS	\$ 200,000.00	\$400,000
7370100	1 L.SUM	TEMPORARY TRAFFIC SIGNALS	\$ 50,000.00	\$50,000
80XXXXX	1 L.SUM	LANDSCAPE & IRRIGATION	\$ 300,000.00	\$300,000
9010001	1 L.SUM	MOBILIZATION	\$ 1,500,000.00	\$1,500,000
9050001	400 L. FT.	GUARD RAIL, W-BEAM, SINGLE FACE	\$ 45.00	\$18,000
905XXXX	2 EACH	GUARD RAIL TERMINAL	\$ 4,000.00	\$8,000
9080001	19,600 L. FT.	CONCRETE CURB(PC/COT STD. DTL. 209)(TYPE 1)	\$ 20.00	\$392,000
9080006	8,000 L. FT.	CONCRETE WEDGE CURB(PC/COT STD. DTL. 209)	\$ 20.00	\$160,000
9080090	10 EACH	CONCRETE CURB TERMINAL SECTION (PC/COT STD. DTL. 210)	\$ 200.00	\$2,000
9080201	57,000 SQ.FT.	CONCRETE SIDEWALK	\$ 7.50	\$427,500
9080280	12 EACH	CURB ACCESS RAMP, PC/COT STD. DTL. 207(TYPE I)	\$ 1,500.00	\$18,000
9080301	11,000 SQ.FT.	CONCRETE DRIVEWAYS	\$ 10.00	\$110,000
9090002	10 EACH	SURVEY MONUMENT	\$ 200.00	\$2,000
9100000	1,240 L. FT.	CONCRETE BARRIER (FULL)	\$ 140.00	\$173,600
910XXXX	4 EACH	MEDIAN BARRIER ATTENUATOR	\$ 10,000.00	\$40,000
9130100	5,000 SQ. YD.	CONCRETE CHANNEL LINING	\$ 70.00	\$350,000
9140001	16,500 SQ.FT.	NOISE BARRIER WALL	\$ 25.00	\$412,500
9200401	300 CU.YD.	SOIL CEMENT BANK PROTECTION	\$ 150.00	\$45,000
9260001	1 L.SUM	ENGINEER'S FIELD OFFICE	\$ 20,000.00	\$20,000
9300001	200,000 F.A.	INCIDENTAL ITEMS	\$ 1.00	\$200,000
9300004	9 EACH	BUS SHELTER PAD	\$ 1,500.00	\$13,500
93000XX	5 EACH	BUS PULLOUT (9" PCCP)	\$ 25,000.00	\$125,000
9310010	720 SQ. YD.	DECORATIVE CONCRETE PAVEMENT	\$ 60.00	\$43,200
9330001	1,000 L. FT.	BARRICADE RAILING (PC/COT STD. DTL. 105)	\$ 30.00	\$30,000

TOTAL		\$17,251,100
	10% CONTINGENCIES	\$1,725,000
	CONSTRUCTION COST	\$18,976,000



Appendix B

Community Advisory Committee and Public Involvement Information

Includes:

- Community Advisory Committee meeting minutes
- Open House No. 1 Meeting Summary
- Open House No. 1 Comment Summary
- Open House No. 2 Meeting Summary
- Open House No. 2 Comment Summary
- Summary of Public Participation Plan
- Letter from PCDOT to the CAC dated December 7, 2007
- Letter from the CAC to PCDOT dated January 15, 2008
- Letter from PCDOT to the CAC dated January 24, 2008
- Letter from the CAC to PCDOT dated January 31, 2008
- Letter from PCDOT to the CAC dated February 11, 2008



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC)
Tuesday, August 7, 2007, 6 to 7:30 p.m.
Metropolitan Domestic Water Improvement District Board Room

CAC Members Present at Meeting:

- Fred Bass
- Ellen Clark
- Jason Kai
- Ann Girvin
- Norma Metz
- Robert Schwartz
- Ellie Towne

CAC Members Not in Attendance:

- Humbert Arce
- Carol Gawrychowski
- Andy Hernandez
- William Mattausch
- Gretchen Ochoa
- Kaye Swinford
- Ian Stewart
- Edythe Walther
- Juergen Walther

Attending from Project Team:

- Pima County Department of Transportation: Carol Brichta, Rick Ellis, John McManus, Dean Papajohn
- HDR Engineering: Larry Barela, Ted Buell, Scott Stapp, René Tanner
- Gordley Design Group: Barb Alley, Jan Gordley, Arizeder Urreiztieta

Materials Distributed:

- Agenda
- Fact Sheet
- Binder for CAC members
 - Welcome Letter
 - Project Features
 - Project Area Maps
 - Pima County Community Participation and Mitigation Ordinance

Dean Papajohn, Pima County Department of Transportation (PCDOT) Project Manager, welcomed the Community Advisory Committee (CAC) members to their first meeting. Dean mentioned that there would be presentations from Rick Ellis, PCDOT Engineering Division Manager; Ted Buell, HDR Engineering Project Manager and René Tanner, HDR Engineering Project Scientist. Dean spent a few

minutes introducing himself to the group and what his role would be throughout this project. All of the project team members then introduced themselves and the committee members followed suit.

Dean began his portion of the presentation by telling the members that the essence of this project will be to “enhance life for people in Pima County.” Dean stated that roads build community and by community he meant getting to homes, schools, hospitals, shopping, friends, family and work. The CAC meetings are intended to analyze and discuss issues throughout the project and for the members to not only be the eyes and ears for the community, but to also educate those located in the project area to become more informed citizens.

Dean stated that the agenda for the meeting consisted of introductions, a brief presentation on how a CAC operates, an overview of the project, and receiving comments from the members.

Dean introduced Rick Ellis, PCDOT Engineering Division Manager. Rick started out thanking the members for their commitment to this project. He stated there would be a lot of work and a lot of value with some key elements to come. Rick said there are three roles for the CAC on this project and wanted the members to know what to expect. First of all, the project team would be looking for feedback, real-life observations, and would be hearing from the CAC members about what is going on out in the community affected by the project. Secondly, the members were chosen because of the different interests they represent, from homeowners to business owners to community groups, and the project team would be looking for those perspectives. And third, Rick said this group needed to be advocates – allies to the project out in the community. He reiterated Dean’s comment regarding educating the public and portraying a positive attitude.

Rick turned the floor back to Dean who introduced Carol Brichta, from PCDOT Community Relations. Carol gave a brief overview of what the CAC members would be responsible for during their time on the committee. Carol first went over what each member would find in his or her notebook. She went on to explain: 1) Each member needs to provide Pima County with feedback from the community; 2) CAC members would be responsible for preparing a collaborative letter hopefully of acceptance of the project that will accompany an Environmental Assessment and Mitigation Report (EAMR) to the Board of Supervisors for approval; 3) Carol referenced the Pima County Participation and Mitigation Ordinance that was in each member’s notebook. She stated that each member should take time to read through this document so that they would fully understand his or her role as a part of this committee. Carol also wanted the members to know that they would also have the opportunity to comment on the artwork that would be a part of the project.

Carol described how each member was chosen. People within the project area were mailed an application. A notice was also in the newspaper. From the signed applications, Dean and Carol plotted each applicant on a map, and then members were chosen in a way that assured that different areas of the project would be represented.

Carol said that it was important that all CAC members read page eight of the Ordinance. This page outlines what the CAC members are responsible to cover during their time on the committee. Carol also stated that although only eight meetings will be scheduled, there would be the possibility of the group meeting on their own when necessary. She also said that it was important for the group to choose a chairperson or co-chairpersons for this committee. It will become more important when the members start to write the letter that will go to the Board of Supervisors.

Carol stated that the next CAC meeting would be in about a month. The CAC members and the public would be notified about the date, time and place when that information becomes available. The suggestion was made that future meetings be held at the new Community Center that will open September 15, 2007. It is closer to the project area and one CAC member thought there might be more community involvement if the meetings were in a more central location.

Carol concluded by emphasizing how community outreach is extremely important to this project.

A question was asked about how soon the meeting summary would be ready to review. Barb Alley, Public Involvement Coordinator for Gordley Design Group, stated that she would start putting them together and they should be up and on the Web site in approximately two weeks. Carol also stated that the Web site was on the bookmark included in their binders. The members would be able to view not only information on this project, but they could get information on all Pima County projects.

A member asked how to give information out to neighbors. Carol stated that the member could make copies of what was passed out at the meetings to distribute or to point residents to the County Web site, which will have additional information for interested parties.

Dean introduced Ted Buell, Project Manager for HDR Engineering, to start the PowerPoint presentation that would take the members down La Cholla Boulevard for a project overview.

Ted started out by informing the group that La Cholla Boulevard between Ruthrauff Road and River Road would be widened from the two existing lanes to six lanes. Other project features:

- Total length of the project will be 1 1/2 miles with .7 of those miles being on La Cholla Boulevard and the rest would be intersection work at Ruthrauff Road and Curtis Road
- Bike lanes (6 feet wide), also referred to as "multiuse lanes"
- Close coordination with Sun Tran regarding the bus stops in the project area
- Drainage issues will be addressed at the Rillito River
- Storm drains will be addressed where La Cholla Boulevard meets the Rillito River (built in 1984)
- Ponding problems will be addressed

Dean spoke on available right-of-way (ROW) on La Cholla Boulevard. There is a 150-foot ROW; 75 feet on each side of the center line of the street. There is a mixture of residential and commercial properties in the area and the goal of the project is to make sure there is safe access into and out of these areas. Dean also touched on the fact that there will be landscaping in the project area. The decisions on what type of landscaping that would be needed would have to wait until it is decided on how the configuration of the roadway will unfold.

Dean also discussed utilities. He asked the group what utilities they thought were in the project area. Dean furthered the discussion by stating that multiple utilities were in the area; sewer, electric, gas, water, cable, etc. Dean also told the group that there is a gas regulator station in the area. Ideally, this should not be a problem, but if it should become an issue, Southwest Gas can only work on one regulator station at a time, and they are currently working on a station in Marana. The next one is scheduled for an area south of the project in Tucson. If work needed to be done on this station, Southwest Gas' schedule would have to be accommodated. Again, this is not anticipated work at this time, although it could become an issue in the future.

Ted introduced René Tanner, Environmental Planner for HDR, to give a short report on the status of the environmental findings. René stated that one of the tasks of the CAC members would be to review cultural resources as a part of the EAMR. During the research of the project area, there were two cultural sites identified. The next step would be to determine if those sites were within the project limits. Desert Archeology will be surveying the property in order to make that determination for the project team and advise them accordingly. They will also be looking at biological resources, endangered species and wildlife. René informed the group that no bats were located under the bridge, as the current structure was not built in a way so as to support bat colonies. There were swallow nests found, but they were not active and they were deteriorating; however, they will continue to be monitored.

René also told the CAC members that there were a couple of old landfills in the area. They were currently looking at historic photos and documents to see what the limits are, and that would take some further investigation. There would also be soil testing done at the intersection of Ruthrauff Road and La Cholla Boulevard since there are some gas stations in the area. Noise level is another area that will be monitored and studied. The monitors used by HDR Engineering are calibrated each year to ensure their accuracy. HDR Engineering uses the Traffic Noise Model, which was developed by the Federal Highway Administration (FHWA), to assess levels of noise in an area.

Ted talked about the Rillito River Bridge. This bridge was built in 1980 and is a four-span bridge. The design of bridges was changed in 1983 to include drill shaft foundations, which are deeper and more robust. Ted showed a rendering of what the bridge may look like. It would have three lanes in each direction along with a sidewalk and bike lanes on both sides. Ted also mentioned that it is possible to build the bridge one side at a time so that the bridge would remain open to traffic during construction.

Dean took a few minutes to talk to the members about the public art that will be present along the project area. The ideas are wide open at this point and there is nothing to present to the group today. An invitation was sent to members of the Tucson Pima Arts Council (TPAC), to apply for the artist position on this project, and the team received more than 60 applications. The list was cut to three finalists by a panel of citizens in which CAC member Ellie Towne was a part of. The person chosen was Vicki Scuri. Vicki had done other art along La Cholla Boulevard, but she was chosen because of the variety of experience she has in this area and professionalism she would bring to this project. Vicki would have one percent of the total budget of design and construction per Pima County policy to use for her art projects. Dean mentioned some areas that may include art along the project area; the bridge itself, the Rillito Park entrance, sidewalks, bus stops, etc.

Dean again mentioned the ongoing data collection and activities that have already begun on this project. Aerial photos have been taken of La Cholla Boulevard in its current state. There have also already been survey crews taking a look at the utilities and checking records. Pima County's Real Property division has already contacted residents and businesses with Right of Entry letters so that surveys could be made. Traffic engineers are already surveying the traffic flow and the noise levels will be measured once school is back in session.

Dean concluded the presentation portion of the meeting by stating that the design phase of this project will take a minimum of two years. It takes time to look at and resolve all the issues that will arise during the planning stage of the widening project improving La Cholla Boulevard between Ruthrauff Road and River Road. He pointed out that in the early fall of 2007 the team should have the Design Concept Report (DCR) and the EAMR ready for the CAC members to review. Once the committee reviews those documents, they will go to the Board of Supervisors for approval and upon approval the team can then go into the design phase of the project.

The floor at that time was opened up to questions and comments:

Ellie Towne: Concern about heading south on La Cholla Boulevard and making a right-hand turn onto Curtis Road; when vehicles are in the right-hand turn lane, there will be some cars that will go around them to turn in front of them: Dean said that traffic engineers are studying intersections and any problems they currently are experiencing.

Fred Bass: Concern about how close the new road will come to the houses in that area; also a concern about the safety of the middle-school kids who walk to and from school; cars do not always yield to the children, and often speed in the school zone. Dean reiterated that the traffic engineers would be surveying that area. One suggestion was that a median be put at the school crossing so that kids would have a place to stop if unable to make it all way across the new lanes on La Cholla Boulevard.

Jason Kai: Concern about how to access homes that are in the path of the widening project. He stated that on La Cañada Drive, those residents were given access to their homes from a street behind the main street. Dean talked about some of the options of what the widening may look like from narrowing the median to only putting a sidewalk on one side of the street. There are many alternatives to look at during this design phase to come up with the best one that would meet the needs of everyone involved. This may involve acquiring properties, building a frontage road for safe home and business access, etc.

Norma Metz: Concern about her home specifically. She is on the corner of La Cholla Boulevard and Curtis Road. Her concern was the amount of property the County would have to take in order to widen this stretch of road, leaving her home dangerously close to the busy intersection. She also referred to how difficult it was for her to get to and from her home turning from La Cholla Boulevard in a safe manner. Dean commented about how wide the intersection would be once four more lanes and turn lanes were added.

Robert Schwartz: Concerns about drainage problems. He has major problems on his own property on La Cholla Boulevard north of River Road with the vertical road profile that the County contractor did not build according to the plans has caused major issues on his own property. Dean said that that side of the road would have to be examined to determine what occurred.

Jason Kai: Concern about the additional three lanes in each direction causing back-ups due to the trains crossing Ruthrauff Road. Dean said the County is aware of the bottleneck in that area and they are taking the improvements one step at a time. Ruthrauff Road is on the long-range plan for improvements as well.

There was some discussion about how property might be acquired along the project area. It was stated that different options would be investigated, and the county would make sure if they needed to purchase property, it would be a fair transaction for all parties involved.

Dean adjourned the meeting at 7:30 p.m. letting the group know that the team would honor and respect the time of the group. He concluded with the opening statement: that the goal of this project is to: "enhance life for the people in Pima County."



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC)
Tuesday, Oct. 2, 2007, 6 to 7:30 p.m.

CAC Members Present at Meeting:

- Humbert Arce
- Fred Bass
- Ellen Clark
- Ann Girvin
- Andy Hernandez
- Norma Metz
- Wayne Metz
- Robert Schwartz
- Ellie Towne

CAC Members Not in Attendance:

- Carol Gawrychowski
- William Mattausch
- Gretchen Ochoa
- Kaye Swinford
- Ian Stewart

Attending from Project Team:

- Pima County Department of Transportation (PCDOT): Carol Brichta, Rick Ellis, John McManus, Dean Papajohn
- HDR Engineering: Larry Barela, Bob Brittain, Ted Buell, René Tanner
- Pima County District 3 Representative: Kiki Navarro
- Regional Transportation Authority (RTA): Britton Dornquast
- Gordley Design Group: Barb Alley, Jan Gordley

Materials Distributed:

- Agenda
- La Cholla Boulevard Project contact information bookmark
- Map of Alternative E
- CAC Member individual contact sheet
- Binder for CAC members:
 - Welcome Letter
 - Project Features
 - Project Area Maps
 - Pima County Community Participation and Mitigation Ordinance

Dean Papajohn, Project Manager, PCDOT, opened the meeting by welcoming everyone to the second CAC meeting. Dean made a brief statement about what was discussed at the previous CAC meeting and again stated to the group that this project is being done to “enhance life in Pima County.” The purpose of this meeting was to focus on the alignment and potential configuration of the roadway. Dean pointed out the rough draft of Alternative E that Bob Brittain, Design Engineer, HDR Engineering, would be presenting to the group.

Dean asked everyone to introduce themselves to the group. He started with the design team, consultants, then CAC members.

The first speaker was Carol Brichta, Community Relations Department, PCDOT. Carol briefly touched on the roles and responsibilities of the CAC members, and again, asked for someone to step forward to be chairperson. She stated that the chairperson or chairpersons would be the point of contact when it came time to write the letter summarizing their opinions regarding the Environmental Assessment and Mitigation Report (EAMR). Carol stated that it is helpful to have one person as the point of contact in this process and she would offer her assistance. Her presentation ended with no one volunteering for the open position.

Dean gave a brief overview of the project. He stated there are many disciplines in the design of a roadway including but not limited to, traffic, landscaping, art and bridge design. Dean went on to talk about the five key criteria of this project. They are: 1) safety, 2) function, 3) right-of-way, 4) aesthetics, and 5) budget and schedule. They are described as follows:

Safety: This includes drivers, pedestrians, buses and cyclists.

- a. Adequate timing for traffic flow and pedestrian crossings
- b. Sidewalks – safe passage for pedestrians
- c. Paved shoulder – safe riding for cyclists
- d. Driveway access – safe entrance and exit
Options include:
 1. Dedicated lane for entrance and exit into driveways
 2. Frontage roads: Two-way frontage road on one side or one-way frontage roads on both sides
- e. Medians
- f. Bus pullouts
- g. Storage lanes – cueing up for turns
- h. Adequate sight distance
- i. Bridge safety

Function: The operations of the project.

- a. Looking at traffic needs – current and future traffic patterns
- b. Turn movements off of La Cholla Boulevard onto cross streets
- c. Adequate lane width
- d. Accommodation of multiple users
- e. Median openings to access cross streets
- f. Frontage roads – reducing friction of vehicles entering the mainline
- g. Utilities – maintaining access to them
- h. Drainage

Right-of-Way

- a. Limit property easements
- b. Limit property acquisitions – leave property owners where they are

Aesthetics

- a. Landscape design (currently limited from River Road to Ruthrauff Road)
- b. Urban design/public art (will go into detail at future meeting)
- c. Bridge – modern design, clean lines
- d. Roadway profile– smooth design (rubberized asphalt – for noise control)

Budget and Schedule – funded by the RTA and Pima County

- a. The public voted for the RTA La Cholla Boulevard project and its budget
- b. Limit acquisitions due to budget constraints
- c. Bridge – careful where placed – keep away from utilities

Dean commented that the main goal of this meeting was to discuss alignment, roadway and planning. All the options need to be researched so that a balance can be found and the team can move forward according to the schedule. This is important in order for this project to stay on course and on its projected time line.

Questions:

Ellen Clark: With the occasional high water in the Rillito, is there a way to deepen the riverbed or elevate the bridge?

Dean Papajohn: Those issues will be researched and addressed when the design is being done on the new bridge

Ted Buell: The girders are one foot above the 100-year flood level currently; however, they will be looking into options when reconstructing the bridge.

Ellen: There is a lot of debris in the riverbed. What can be done about that?

Carol Brichta: That is a separate issue and an order can be placed with the county to have that area cleaned up.

Ellen Clark: Is there anything planned for Curtis Road like bike lanes?

Dean: Curtis Road is not a part of this project.

Bob Brittain talked about Alternative E, which is the leading option for La Cholla Boulevard. He distributed a small version of the display map. Ann Girvin asked whether the traffic study had been done prior to the closing of the exit and entrance ramps on Interstate 10. Bob stated that the study is done mostly on projected traffic patterns into the year 2030. [Note: current traffic volumes were collected in Spring 2007 before school was out for the summer.] While current traffic patterns are observed, the overall study is over a 23-year period. Bob went into detail on what the map showed and the points are as follows:

- a. Lanes would be narrowed one foot from 12 feet to 11 feet – this still meets lane width standards
- b. The median has been reduced two feet from 20 feet to 18 feet from the County standard detail.
- c. There will be double left turn lanes at Ruthrauff Road – they would be as long as possible
- d. There will be left turn median openings at Jay Avenue and northbound Calle Narcisco
- e. Ruthrauff Road will need to be widened at the intersection

- f. South of Ruthrauff Road the lanes would be narrowed down to tie back in with the three lane section heading south
- g. Sidewalks would be included on both sides of the road through the entire project at a width of five feet

A study will be done, if the time comes that cars cannot turn through traffic from the left turn bays onto cross streets, to see if a light is warranted.

Fred Bass: What about the bus stops? Will there be pullouts in order to get the buses out of traffic?

Bob: Stated that it was not shown in these preliminary drawings to have pullouts; however he felt there was enough room to put them in.

There was some discussion about just how close the sidewalks would come to resident's front doors.

[Note: There is approximately 25' from the back of sidewalk to most front doors on the west side.]

There was also discussion about the single, one-way frontage road. Residents would have to U-turn in order to get back to their homes. There was concern about the lack of visitor parking on La Cholla Boulevard on the frontage road, and there was also a comment about enhancing everyone's life by this improvement project except the people who live along the project area.

Dean stated that they will try to balance all the elements and that maybe a stake survey should be done for each resident to show the right-of-way so each resident can see where their property lines are and where the project would begin. [Note: pink whiskers were placed in the ground on the east and west side right-of-way lines on Oct. 8.]

Ellie: Where will the center line of the roadway be?

Bob: The center line will not change. The improvements will be added out from the original roadway's center.

Fred: What will happen to the noise level as the road moves closer to the houses?

Rick Ellis: The roadway paving material will be rubberized asphalt to help reduce the noise in the area.

Dean asked the CAC members to go around the table and make any comments they wanted so that each member had a chance to voice their concerns.

Andy Hernandez: It sounds like a sound plan – some issues, but we are in the planning stage. There will need to be more discussions and there will be time to keep talking.

Ellen: Since there will be two years prior to construction, there is time to discuss other options.

Dean: They can study the alignment; however, the more time the process takes with the public, the further the project is pushed out, leading to increased costs.

Norma: There is a two-year time frame before construction will begin on the roadway.

Fred: I would like to see all the affected properties taken by the county so that the construction can take place without impacting anyone as described; however, I understand budget concerns.

Ann: She has concerns about the current condition of the bridge.

Fred: Asked about the total cost of the project.

Dean: The total cost of construction is approximately 17 million dollars. The bridge will be made mainly of concrete, which is very expensive and has gone up in price since the original estimates. In order to

purchase property in the project area, several million dollars would be needed around the order of magnitude of three to five million dollars.

Humbert Arce: What is going in on the corner of La Cholla Boulevard and Ruthrauff Road?

Dean: There is a WalMart Market store going in at that corner.

Dean went over briefly what he heard the CAC members saying about the proposed Alternative E: positive reaction to additional lanes, wider bridge, turn lanes and lighting at intersections, sidewalk and bike lanes; concerns over proximity of residences that front La Cholla Boulevard. He stated that it was important for everyone to be on the same page during this process; the process is a collaboration between the County, consultants, and citizens. He thought it would be best for the members to meet back in a week or two. This would give the team a chance to discuss some possible changes and the CAC members will get a chance to see what their neighbors have to say about the proposed improvements.

A meeting date of October 9, 2007 was agreed upon and Carol stated she would check on the availability of the room and notify everyone to confirm the date.



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC)
Tuesday, Oct. 9, 2007, 6 to 7:30 p.m.

CAC Members Present at Meeting:

- Humbert Arce
- Fred Bass
- Ann Girvin
- Norma Metz
- Wayne Metz
- Ellie Towne

CAC Members Not in Attendance:

- Ellen Clark
- Carol Gawrychowski
- Andy Hernandez
- Jason Kai
- William Mattausch
- Gretchen Ochoa
- Kaye Swinford
- Ian Stewart
- Robert Schwartz

Attending from Project Team:

- Pima County Department of Transportation (PCDOT): Carol Brichta, Rick Ellis, Dean Papajohn
- Pima County Department of Public Works; Real Property: Greg Foster, Kelley Hall
- HDR Engineering: Larry Barela, Bob Brittain, Ted Buell
- Kimley-Horn and Associates: Mary Rodin
- Gordley Design Group: Barb Alley, Jan Gordley

Materials Distributed:

- Agenda
- Map with Alternative E
- CAC Member individual contact sheet
- Binder for CAC members:
 - Welcome Letter
 - Project Features
 - Project Area Maps
 - Pima County Community Participation and Mitigation Ordinance

Dean Papajohn, Project Manager, PCDOT welcomed the CAC members and the public to the meeting. He stated that this meeting was being held as a follow-up to the previous week's meeting and the format

would be a round table discussion rather than a presentation. The team members, CAC members, and the public made introductions.

Ted Buell, Project Manager, HDR Engineering, gave a brief update on the status of the project. The following tasks have been completed or are in progress:

- Mapping and surveys on adjacent properties along the project corridor
- Testing for hazardous materials
- Utility mapping
- Traffic reports
- Noise monitoring (monitoring the existing noise levels)
- Roadway alignment study
- Drainage
- Bridge research

Ted stated that a draft of the Design Concept Report (DCR) would include the information from these tasks.

There are several activities dependent on roadway alignment according to Ted, and they are as follows, along with the timeframe that has been planned for these activities to take place:

- Roadway alignment – as soon as possible
- Open house to present the alignment to the public – about a month after an alignment has been identified
- Environmental Assessment and Mitigation Report (EAMR) – drafted by February 2008
- Soil testing for the bridge and roadway – targeted for February 2008
- Roadway plans for design and elevation – end of 2009 or January 2010

Dean stated that choosing an alignment is critical for this project to be able to move forward.

Dean went on to talk about the positives that the team heard from CAC members at the Oct. 2, 2007 meeting. He stated that he heard the CAC members liked proposed sidewalks, bus pullouts, turn lanes, median openings and bridge improvement. The area of concern seemed to be the approximate 1,000-foot stretch where homes are adjacent to the widening project. Dean stated he had wanted the CAC members to have time to think about the proposed alignment for a while and have a chance to talk with neighbors to get their input on Alignment E, the proposed alignment.

Dean asked Carol to comment on the debris in the Rillito River that was a concern brought up by some CAC members from the previous meeting. Carol stated that she needed to know the specific area and the debris that needs to be cleaned up, and then she would contact the Pima County Flood Control District. They would send out a representative from their department to survey the area and put in a request for cleanup. She asked that the CAC members approach her after the meeting, so that she could take down the information and start the process.

Mary Rodin, Traffic Planner, Kimley-Horn and Associates, gave a brief report of the traffic study. She stated that the report was based on traffic forecasts for the year 2030, which were obtained from the Pima Association of Governments (PAG). PAG does travel forecasting for the entire Tucson region. The PAG model, based on the Regional Transportation Plan (RTA), assumes that La Cholla Boulevard would become a major north/south parkway from Tangerine Road south to Interstate 10 (I-10). The I-10 connection would be made using Ruthrauff Road.

Dean pointed out that in the Community Participation and Mitigation Ordinance there is a section that states the project must follow the PAG model. The design team is doing its best to balance the guidelines set forth by PAG with the County and team's ideas with input from CAC members.

Questions were brought up regarding funding, and Dean stated that there were no additional funds for this project. The 17 million dollars that was budgeted for this project is what the team has to work with.

Dean also stated that he had pictures to pass around that showed the Right-Of-Way (ROW) lines staked by whisksers (a pink fuzzy on the top of a stake driven into the ground). Since the CAC members wanted to know exactly where the ROW lines were in relation to their property, the team felt the ROW being staked for the affected properties along the project area would be beneficial to the homeowners.

Dean opened up the discussion to the CAC members for their comments, and then expressed the desire to hear comments from the public that came to share their thoughts and ideas.

Fred Bass requested hearing the other options that were not presented.

Bob Brittain, HDR Engineering, gave a brief overview of the alternatives that were not discussed at earlier meetings. They are as follows:

Alternative A: Buys the adjacent residential properties on the east side of the road, portions of some business properties and shifts the roadway to the east

Pro – this option allows for a 30-foot wide two-way frontage road, potential noise wall and extra room on the west side of La Cholla Boulevard, and a 16-foot median

Con – the cost to purchase these properties would involve an additional cost of 4.3 million dollars or more

Alternative B: Buys the adjacent residential properties on the west side of the road and moves the roadway to the west

Pro – same as option A – except the extra room would be on the east side of La Cholla Boulevard

Con – the cost to purchase these properties would involve an additional cost of 3.6 million dollars or more. The number of properties needed to buy on this side would be less than on Alternative A.

Also, this option would have to take the existing well and move it (It can only move within 500 feet of its existing site).

Alternative C: Instead of a frontage road, this option would simply add an additional lane to the roadway for residents to turn directly in and out of their homes

Pro – none

Con – not considered a safe option if vehicles back out onto La Cholla

Alternative D: Buys residences only on both sides that have driveways directly on La Cholla Boulevard and widens the road from its existing center line

Pro – roadway centerline can remain in the center of the existing right-of-way

Con – still expensive at a cost of 3.6 million dollars or more that is not in the budget

Alternative F: Buys every other residential property in order to have room to put circular drives in at the homes that are left; this will allow for those residents to safely exit and enter their properties off La Cholla Boulevard without a frontage road

Pro – safe access, reduces number of residences to purchase

Con – additional budget still required; every other property would be County-owned; question remains as to who would maintain that property.

Bob briefly went over Alternative E – Not purchasing any property. City of Tucson well site is not disturbed. This option provides adequate lane width of 11 feet and median width of 18 feet, one-lane frontage roads and allows for safe access to residences.

Humbert Arce: Which alternative is more schedule-friendly?

Bob stated that they all have their issues, so they all involve about the same time frame. Those alternatives that would require purchasing property could potentially take a little longer due to the acquisition process.

Dean made the statement that the RTA's plan was for a six-lane roadway, which was voted on, and the six lanes are what are needed for future growth in the area. He also said that acquisitions on this project were not possible due to the budget constraints.

Some of the CAC members and others in attendance were concerned about the noise, reduced speed needed for six lanes of traffic and the safety of children playing in their front yards so close to the road, and the loss of parking; residents are currently using the street in front of their homes and that will no longer be available. The team stated that the noise would be buffered by the fact that the frontage road would be between the homes and the throughway. It was also stated that René Tanner, HDR Engineering, would be reporting at a future meeting on the noise study and the reduction of noise by using rubberized asphalt.

The other concerns brought up by the CAC members had to do with the socioeconomic status of the residents in the project area. The CAC members feel that because they are at a lower income level than those on some of the other County projects, the decision makers at upper levels in the County are not hearing their concerns. The CAC members feel that the County set precedents because they have purchased homes throughout the County on other projects – but it was not provided for in the budget for this project. The CAC members are also concerned about the safety of children crossing La Cholla Boulevard from the middle school.

Dean suggested that if the CAC members wish to communicate their concerns to others at a higher level in the County, they could draft a letter, outlining their concerns, which he could present to his superiors. The CAC members agreed that would be a good idea.

The issues brought up by the public were as follows:

- Why were they not told of these proposed plans? The team stated that this project has been talked about for many years, has been of public record and had been voted on by the public.
- The bridge was not widened as originally planned because funds were shifted to other projects. The team stated that the County is no longer operating in that manner, and funds allocated for a project will stay for that project and within the budget that was set forth.

Fred Bass was chosen as the chairman for the CAC. The members decided to meet Monday, Oct. 15, 2007 to draft their letter.

Dean adjourned the meeting.



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC)
Thursday, Dec. 6, 2007
6:00 to 7:30 p.m.
Ellie Towne Flowing Wells Community Center

CAC Members Present at Meeting:

- Fred Bass
- Ellen Currey
- Ann Girvin
- Norma Metz
- Wayne Metz
- Ellie Towne

CAC Members Not in Attendance:

- Carol Gawrychowski
- Andy Hernandez
- Jason Kai
- William Mattausch
- Gretchen Ochoa
- Kaye Swinford
- Ian Stewart
- Robert Schwartz

Attending from Project Team:

- Pima County Department of Transportation (PCDOT): Carol Brichta, Dean Papajohn
- HDR Engineering: Larry Barela, Ted Buell, Bethy McGehee, Scott Stapp, René Tanner
- Gordley Design Group: Barb Alley, Jan Gordley

Attending from the public:

- Timothy & Jamie Barrett
- Bonny Bass
- Marsha Brendlinger
- James Brendlinger
- Bill Erickson
- Norman Franzen
- Robert Gaona
- Marvin Horn
- Steve Schweska

Materials Distributed:

- Agenda
- Fact Sheet
- Travel Demand Volume Data for the project area
- Meeting Minutes from 10/09/07 CAC Meeting

Dean Papajohn, Project Manager, PCDOT, welcomed the CAC members and the public to the meeting. All who attended made introductions and Dean stated that the purpose of this CAC meeting was for the committee members to focus on the aspects of the project and the tasks that the CAC is charged with and responsible for. Dean told the group that the team members would stay after the meeting was adjourned in order to answer individual questions including those from members of the audience. Dean turned the meeting over to Jan Gordley, Gordley Design Group, to review the agenda and facilitate the meeting.

Jan stated the purpose of her facilitating the meeting was so that Dean and the rest of the team could really focus on what the members were saying about the issues and concerns. Prior to the meeting, Jan had checked with Dean and Fred Bass, chair of the CAC, to see what their goals were for the meeting.

In Jan's discussion with Fred, she found that he had three concerns. Those concerns were traffic, noise and drainage. The team was prepared to give an update on those three areas, and to address other concerns the members had.

Jan took this time to go over an exercise that would allow individuals, including the public, to participate and voice their concerns. Categories of concerns were written on white paper and taped to the wall. The categories chosen were based on discussion at the previous CAC meeting, namely: Safety, Noise, Access, Parking, Visual and Other. Each CAC member was given pink paper while the public received blue paper. Each person was given the opportunity to write down their major concerns and tape them on the relevant white concerns paper. After that exercise was complete, the group was given three dots to put on the issues that were most important to the individual. Once this exercise was over, everyone took their seats and Jan went over the results.

Jan asked Ted to speak a few minutes on lane width, which was a concern under safety. Ted Buell, Project Manager, HDR Engineering, stated that the width of the lanes met the requirements of the American Association of State Highway & Transportation Officials (AASHTO), which governs the design of the roadway. The lane width acceptable by AASHTO's standards is 10-12 feet. In this project, the projected lane width would be 11 feet, which is within the guidelines. Fred's issue with the 11-foot lane width relates to the large number of semi tractor-trailers that travel La Cholla Boulevard and Fred feels the 11-foot lanes will be a safety issue. Fred stated that he is aware of the budget constraints and voiced that this stretch of road needed to be built with the safety of drivers and pedestrians in mind. Fred also stated that this area should be widened to match what had been done farther north on La Cholla Boulevard in another widening project. Dean stated that the traffic projections for the year 2030 suggested a pavement cross-section between four and six lanes. Because of this, a six-lane section provides extra space for vehicles resulting in less benefit for 12-foot lanes. La Cholla Boulevard north of River Road has 10-foot wide paved shoulders for bikes. However, the County has learned that vehicles start driving in the shoulders or using it for turn lanes if the shoulders are that wide, which introduces conflicts between bicycle use and motorized vehicle use. The new bike lanes would be limited to five to six feet in order to avoid that problem in the future. At that point, Jan asked to move forward with some other concerns.

Dean commented on safety and asked Ted to talk about a High-intensity Activated crossWalk (HAWK) crossing. This is a crossing signal that is activated when there is a pedestrian present. They push a button that begins a yellow flashing light that turns to a red light so that a pedestrian can cross at the specified location. One of the guidelines for installing a HAWK crossing is 20 pedestrians per hour crossing the street during a peak time of day. Ted stated that to install a HAWK crossing is approximately \$100,000 and if the volume of pedestrians is not what was anticipated, drivers will learn to ignore the crossing, causing another safety issue. Ellie Towne asked when a decision would be made about the crossing and Dean stated that would be determined after the roadway was built. Ellie wondered how pedestrians would cross the street to get to the southbound bus stop. A study would be done to determine what would be best for that area; however, the team could also recommend what they feel would work best. Dean stated that no matter which roadway alignment is chosen for this project, pedestrians would have a safe place to cross the street at the signalized intersections.

Scott Stapp, Environmental Manager, HDR, gave a brief overview of noise and how it is measured. He reviewed some basic noise concepts including dBA – decibels within the range of human hearing, Leq – average sound level and NAC – Noise Abatement Criteria. Scott stated that to require consideration of mitigation with sound barriers, the sound level must be above 66 dBA. Scott explained that up to 66 dBA, people could hold a normal conversation without having to raise their voices. Once above that number, mitigation is generally sought to help reduce the noise level. There are three places where roadway noise comes from: tires hitting the pavement, engine noise and exhaust. Criteria for assessing noise mitigation includes whether it is Feasible in terms of topography, geometry, drainage and safety, whether it is Reasonable in terms of cost per benefited receiver and if it is wanted by the affected property owners.

In Scott's presentation, he stated that landscaping was not enough to substantially lessen noise levels and noise walls only work where drainage, safety (sight distances) and continuous walls are provided. When a wall is not continuous (i.e. breaks in a wall to allow people access to their driveways), the noise will enter through the opening and render the wall ineffective. The best method of lowering noise in this situation is through rubberized asphalt. The increase in noise that may occur through year 2030 due to the increased traffic should not amount to more than three-dBA. A three-dBA reduction is generally allowed when using rubberized asphalt. Scott also let the group know that monitoring of existing noise levels had already been done and a report will be prepared projecting the noise levels to the year 2030. All of the data will be analyzed and a recommendation will be made based on the information that was determined by the noise study. Scott stated that he couldn't move forward with his study until the final alignment and roadway profile are determined.

Ted stated that the traffic report was in draft form and would be completed shortly. He passed out the travel demand numbers so that the committee could see where the volume is now and what the projected numbers would be for 2030. On this section of La Cholla Boulevard, traffic volumes currently range from 23,000-28,000 vehicles per day, with traffic volumes for 2030 predicted at 41,000-44,000 vehicles per day.

Ted and Dean reported on the status of the drainage study letting the members know that drainage goes hand in hand with the design, so that process is on hold until they are able to move forward in the design process.

Jan suggested the members each take a turn to go over their main concerns, one more time, for the team.

Norma Metz: No more comments at that time.

Wayne Metz: Voiced displeasure with Alternative E and wanted the County to look at some of the other alternatives that he feels are a better fit for this improvement project. Wayne feels the county should pick the best option for this project, and if the money isn't available, they should wait until more funds could be allocated.

Fred Bass: The road should be built with the best option for the project.

Ellie Towne: She voiced some concerns about where the residents would have to U-turn safely in order to get on the frontage road to access their homes. Ellie also had a question about the bridge and its height and width.

Ann Girvin: Her comment was to restate that she was not a homeowner and would not be directly affected by the project. However, she voiced her concern for the residents that will be directly affected by the widening of La Cholla Boulevard and stated she would support the decision they felt was right.

Ellen Currey: She stated that she had lived in Pima County since 1969 and wanted to see this project done correctly.

Ted stated that they would take specific questions from the public following the CAC portion of the meeting.

Jan discussed what the CAC's role was in moving this process forward. She reiterated that it was extremely important for anyone who had a concern to write a letter to Pima County so that the County was aware of specific

concerns that either the group or individuals had. Jan let the members know that the public process was important and the County had made a commitment to this project.

Carol Brichta, Community Relations, PCDOT, wrote the contact information for PCDOT's management on a flip chart for members and the public at the meeting to write their letters to Priscilla Cornelio, Transportation Director, PCDOT, 201 N. Stone Ave., Tucson, AZ, 85701, so that she could see their concerns and issues in writing.

Jan indicated the project was ready for a public meeting and that one would be scheduled after the first of the year.

Dean distributed a project fact sheet and the meeting was adjourned.

The team stayed for individual questions and comments from the public as well as CAC members.



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC) Meeting
Thursday, July 24, 2008
6 to 7:30 p.m.
Ellie Towne Flowing Wells Community Center

CAC Members Present at Meeting:

- Ann Girvin
- Norma Metz
- Wayne Metz
- Ellie Towne

CAC Members Not in Attendance:

- Fred Bass
- Ellen Currey
- Carol Gawrychowski
- Andy Hernandez
- Jason Kai
- William Mattausch
- Gretchen Ochoa
- Robert Schwartz
- Ian Stewart
- Kaye Swinford

Attending from Project Team:

- Pima County Department of Transportation (PCDOT): Carol Brichta, Rick Ellis, Dean Papajohn
- Representative for Supervisor Sharon Bronson's office: Kiki Navarro
- HDR Engineering: Larry Barela, Ted Buell, Christine Jacobs-Donoghue, Scott Stapp
- Regional Transportation Authority (RTA) – Mainstreet Program: Britton Dornquast
- Gordley Design Group: Barb Alley, Arizeder Urreiztieta

Attending from the Public:

- Timothy Barrett
- Marvin Horn
- Jerry Jones

Materials Distributed/Presented:

- Agenda
- Comment forms
- RTA booklets
- Cross-section map and noise wall layout
- Noise report
- Rendering of a 10-foot wall

Dean Papajohn, PCDOT Project Manager, welcomed the group to its fifth CAC meeting. Dean stated that the purpose of the meeting was to present the noise report for La Cholla Boulevard: Ruthrauff Road to River Road. Dean also reminded everyone that Barb Alley, Gordley Design Group, would be recording the meeting, and asked members to please speak clearly when addressing the group.

Dean briefly went over the agenda and asked everyone to introduce themselves, who they are affiliated with and their role as a part of the meeting. After the introductions, Dean referred back to the agenda pointing out that Ted Buell, HDR Project Manager, would give a project update. He also stated that there had been a public meeting in March and that there had been a lot of project activities. Ted would update the CAC members on those activities. After the project update, Scott Stapp, HDR, would present the noise study to the CAC along with the recommendations that resulted from the study. Dean stated that noise walls would be recommended in some locations along the project area, and Scott would go into further detail in his presentation. Dean also stated that the CAC wanted their concerns, regarding the roadway alignment, taken to upper levels of management within PCDOT and Pima County Supervisor Sharon Bronson's office. The project team has fulfilled that commitment and the CAC concerns have been shared as requested. Dean assured the members that all comments and concerns regarding the recommended alignment would be a part of the Environmental Assessment and Mitigation Report (EAMR). Dean jumped ahead on the agenda and asked the members to note some upcoming dates. Again, he stated that the purpose of the meeting was to present the noise study, and added that the team would like to meet again in approximately two weeks in order to distribute the draft EAMR and the draft Design Concept Report (DCR) to the CAC members. Dean asked if Aug. 7 would be a good date for the CAC to meet. He also stated that they would be scheduling a public meeting for possibly Sept. 9 or 11. Dean again asked that the CAC communicate available dates to the team.

The floor was turned over to Ted for the project update. Ted started out by stating that the partnering kick-off meeting was approximately one year ago and a lot of progress has been made to date. He mentioned that most likely the residents had been aware of some activity in the area. Some of the fieldwork consisted of environmental surveys, geotechnical reports, archaeological surveys, noise-level monitoring, native plant surveys, utility assessments, cultural resource surveys, land surveys, trenches for artifacts and the testing of soil for hazardous materials. The design team gathered necessary data on traffic, drainage and the bridge: whether the existing bridge should remain or be replaced. The traffic report stated the estimated vehicles per day on La Cholla Boulevard: Ruthrauff Road to River Road would be approximately 44,000 in the year 2030, and it was determined that a new bridge would need to be constructed.

Ted reported that the Environmental studies indicated that there were a few items that would need to be monitored: Biological Review (burrowing owls, birds, etc.) would need to be monitored during construction; Cultural Resources Survey – items will be monitored during construction. There were no hazardous materials noted except a very small amount of lead paint that was in such small quantities that no action would be required.

The biggest reports are the EAMR and DCR, which the team hopes to hand out to the CAC members at the next CAC meeting. The draft noise report is what Scott would be discussing, and a copy of the noise report would be handed out to the members at that time.

Currently, the team is working on the stage-two drainage report, which is at 30-percent design; approximately the same place the design team is on the roadway. The team is working on the final

structure selection report, final geotechnical report and the public art and landscape design. Ted turned the floor over to Dean who then introduced Scott.

Scott started his presentation by informing the CAC members that on Oct. 4 and Oct. 10, 2007, noise levels were taken in the morning from 7 - 8:30 a.m. and in the evening from 4:45 - 6:15 p.m. The monitoring was done approximately 40-70 feet off the roadway. The existing results of the levels taken during those time periods were 65-69 dBAs. The point where mitigation comes into play would be when the level is 66 dBA or higher. When the actual noise is gathered, those numbers are put into a computer model, and the noise levels generated from the computer should be the same as the actual readings. The reason actual readings are taken is to ensure the computer model reading is accurate. At that point, they plug the new roadway and the projected increase of traffic out to the year 2030 into the model and generate numbers based on those future conditions. The readings ranged from 56-72 dBAs; however, a three dBA reduction is given for the use of rubberized asphalt bringing that new figure to 53-69 dBAs. As a result of the study, there were 32 locations where mitigation, in this case sound walls, could be warranted.

The next stage of the study consisted of finding out whether sound walls would be feasible and reasonable. There is a set of criteria that must be met in order to justify the use of sound walls and they are as follows:

- Feasible: must work with existing drainage systems, provide adequate sight distance, provide adequate access to existing driveways and need to produce a reduction in sound of five dBA.
- Reasonable: more than one house must benefit from a sound wall and there must be an appropriate cost per benefited receiver. To find the cost per benefited receiver; the cost per square foot of the wall being constructed is multiplied by the square feet needed for the wall (height X length) divided by how many people a particular wall will protect. The dollar amount PCDOT is looking for is no greater than \$35,000 per benefited receiver to be considered reasonable.

In this analysis, the final number of walls that were warranted was five. Out of the five that were deemed necessary, only three fell into the feasible and reasonable category. Two of the walls only protected one house, and a wall must protect more than one dwelling. The last criterion that is considered would be whether the residents want walls (51 percent must be in agreement), and in this case, a 10-foot wall was found to be necessary in order to allow for the required five dBA reduction in sound.

A CAC member asked what the total cost would be to construct the walls that are being recommended.

Scott stated the estimated amount was roughly \$400,000.

Where would the walls be constructed?

The walls would be located in the right-of-way and on top of the raised medians that would be separating the frontage road from the actual roadway. Ted was able to show the CAC what the walls would look like from the cross-section that was handed out to the members at the beginning of the meeting.

What would the walls be made out of?

They can be constructed out of a variety of materials; however, the team is recommending eight-inch masonry block. The walls in front of the frontage road would be constructed on top of a 32-inch tall concrete traffic barrier. Ted let the members know that a similar wall was constructed at River Road and Flowing Wells Road if anyone was interested in seeing what a wall in their area would look like.

Would the walls be painted?

It was stated that those kinds of decisions would be answered at a later date. If walls were to be constructed, there would be a meeting in order to discuss how the residents would like the sound walls to look.

Since the walls on the east and west side of the street are not across the street from each other, wouldn't the sound bounce to the other side of the street off the sound wall?

Yes; it was stated that would be a small issue.

Are the instruments used to gather sound calibrated?

Scott stated that the machines are sent out yearly to be calibrated for accuracy. Scott did inform the group that the monitoring equipment is only used for a small portion of the study. A majority of the report is computer-generated using proposed guidelines.

A member of the public commented on the amount of pollutants that come from the idling cars.

Dean mentioned that the amount of pollutants coming from idling cars and trucks would be reduced due to the road being widened, thus reducing congestion and idling that is currently occurring.

A comment was made by a CAC member stating that the congestion would be alleviated somewhat when the construction was completed on Interstate 10. There has been an influx of traffic through the area due to the ramp closures on I-10.

Dean suggested that the members read through the report and bring any questions they may have before the next meeting. He said it was unfortunate that there would be some homes that would not be as protected as others in relation to sound; however, Dean said they were tied to the geometry of that segment of roadway. There is a desirability factor: some people will want noise walls, and others will think they hide their property, or may not be good for a business in the area. All factors need to be taken into consideration when reviewing this study. Dean also stated that this version of the alignment still shows frontage roads on both sides of La Cholla Boulevard. However, the alignment with two frontage roads as well as the alignment with one frontage road will be presented in the EAMR, which will be reviewed by the Board of Supervisors (BOS).

A CAC member asked whether this would be the information that would be presented to the BOS.

Dean stated that the BOS wanted PCDOT's recommendation and the CAC's recommendation to be complete. They do not want bits of information along the way. They prefer to have the information from both the CAC and PCDOT at one time. As an exception to that preference, Dean did state that as promised, the team did take the CAC's concerns to the County Administration and the Supervisor.

PCDOT was not directed to change their approach, but to continue with developing the DCR and the EAMR. At the time the recommendations go before the BOS, they would like all the information presented at one time.

A CAC member asked about the noise study and whether the CAC's recommended option was taken into consideration during this study.

Scott stated that they did determine what the sound level would be at the second row of houses if the first row of houses were removed. He said if the CAC's option were to be chosen, acquisitions would be made and the need for sound walls would no longer be an issue since the homes in question would either be gone, or far enough away from the roadway as to not warrant walls.

Were sound walls figured into the budget?

Yes, they were accounted for in the budget.

What is the date this will go to the BOS?

Dean stated that there was not a date set as of yet. The EAMR and DCR still have to be presented to the CAC members and there has to be one more public meeting to present all documents to the public, then the information gathered from the CAC and the public has to be integrated into the report and has to be publicly posted for a one month period prior to going before the BOS; Dean stated that a meeting with the BOS is not anticipated before December 2008 or January 2009.

A member asked for clarification on whether the CAC's option would still be able to be presented.

Dean stated that yes, the recommendation of the CAC would be considered.

What will happen to the Tucson Water well if sound walls are installed? There would not be enough room for the City to get their equipment into the narrow space in order to service the well.

Dean stated that there have been discussions with Tucson Water. Tucson Water has a number of options such as using smaller equipment, buy adjacent property, relocate the well or replace the well. The final decision on what will happen to the well will solely rest with the City of Tucson.

Dean brought the focus back to the issue of noise, which was the purpose of the meeting. Dean asked if anyone had any further questions and comments. There was some discussion on the noise walls with the understanding that the members would read over the report and bring any additional questions and comments to the next CAC meeting.

The meeting was adjourned at 6:55 p.m.



La Cholla Boulevard:
Ruthrauff Road to River Road
Community Advisory Committee Meeting



Community Advisory Committee (CAC) Meeting
Thursday, Aug. 12, 2008
6 to 7:30 p.m.
Ellie Towne Flowing Wells Community Center

CAC Members Present:

- Fred Bass
- Ann Girvin
- Norma Metz
- Wayne Metz
- Robert Schwartz
- Ellie Towne

CAC Members Not in Attendance:

- Ellen Currey
- Carol Gawrychowski
- Andy Hernandez
- Jason Kai
- William Mattausch
- Gretchen Ochoa
- Ian Stewart
- Kaye Swinford

Attending from Project Team:

- Pima County Department of Transportation (PCDOT): Carol Brichta, Rick Ellis, Dean Papajohn
- Representative from Supervisor Sharon Bronson's office: Kiki Navarro
- HDR Engineering: Larry Barela, Catherine Bolm, Bob Brittain, Ted Buell, Scott Stapp
- Regional Transportation Authority (RTA) – Jim DeGrood
- Gordley Design Group: Barb Alley, Arizeder Urreiztieta

Attending from the Public:

- Bonny Bass
- Timothy Barrett
- Marvin Horn
- Bob Iannarino
- Jerry Jones

Materials Distributed/Presented:

- Agenda
- Comment forms
- RTA booklets
- Draft Environmental Assessment and Mitigation Report and Draft Design Concept Report

Dean Papajohn, PCDOT Project Manager, welcomed the group to the CAC meeting, and again thanked the members for their commitment to the La Cholla Boulevard project. He took a moment to go around the room and have everyone introduce themselves and state their affiliation.

The purpose of tonight's meeting is to discuss two very important project documents. The CAC will be provided with the draft Design Concept Report (DCR) and the draft Environmental Assessment and Mitigation Report (EAMR). Although the DCR is in draft form, it is quite thorough at this stage and it includes all the documentation that has been presented to the CAC members over the past several months. The draft EAMR contains all the information to date and it summarizes all the investigations, analysis and design work that has been completed for this project.

With the amount of information in the reports, the team felt it would be beneficial to the members to present them with the documents in order to help them navigate through some of the major elements in the documents. It will be the role of the CAC members to respond to the DCR and the EAMR, and present that information back to the design team.

Dean took a moment to direct the CAC to the Community Participation and Mitigation Ordinance, section 10.56.200, which is located in their member notebooks. He stated that this section spells out the functions and the duties of the CAC. At this point in the process, the CAC is to provide written comments containing the CAC's recommendations on the DCR and the EAMR. The written summary and response will not only be presented to the design team, but will also be presented to the Pima County Board of Supervisors (BOS).

The CAC members on this project are further in the process than other CAC's; they had written a letter to the BOS in January. Dean stated that the team did their best to incorporate their concerns in the documents that the committee had received.

Dean stated that the group would need to come together after reviewing the documents in order to write their letter. He stated that PCDOT would be available to meet with the members if requested. Dean stated that the committee could use the comments that they submitted to the BOS in January, or they could write a new letter.

Dean informed the members that as a part of the process, the team would be getting the additional input from the community at a public meeting that has been scheduled for Sept. 11, 2008, at the Ellie Towne Flowing Wells Community Center from 5:30 – 7:30 p.m. The county will mail invitations to everyone that lives within one-half mile of the project area. The team will be there with displays and available to answer questions from the public. There will also be surveys that will be passed out to the attendees of the meeting as well as mailed to all who received the invitation for the public meeting. The results will be gathered and given to the project team and the CAC members so they can address the public's comments in their letter to the BOS.

Dean introduced Bob Brittain, HDR Engineering, who would present the draft DCR to the CAC members. The DCR is the report that is drafted to document the design approach on how to prepare the construction plans. This report summarizes all the reports, surveys, etc. that have been performed in the project area. Bob pointed out that at the front of the draft DCR there would be an Executive Summary and a Table of Contents. The following is a quick overview of each chapter:

Chapter 1 – Project Overview – how and why this project exists

Chapter 2 – Project Description – scope of work

Chapter 3 – Project Area Characteristics – existing conditions; environmental and physical

Chapter 4 – Traffic and Accident Data – summarizes traffic volumes, capacity, accidents, etc.

The Arizona Department of Transportation (ADOT) did a safety study of the intersection of La Cholla Boulevard and River Road due to the number of accidents at that intersection. Even

though that intersection is not a part of this RTA project, there will be some improvements under the recommendation of ADOT that will occur as a result of that study.

Chapter 5 – Design Standards and Criteria – 2003 Pima County Roadway Design Manual and the American Association of State Highway and Transportation Officials (AASHTO)

Chapter 6 – Major Design Features – horizontal and vertical alignment, access control, right-of-way, drainage, earthwork considerations, intersections, utilities, structures, pavement design, signaling, construction issues, design exceptions

Chapter 7 – Social, Economic and Environmental Considerations – summarizes all environmental studies

Chapter 8 – Public Involvement – includes the Public Participation Plan, information from the CAC and summarizes public open houses; documentation including meeting minutes, can be found in Appendix B; information and documentation from the Sept. 11, 2008, open house will be added to this document

Chapter 9 – Agency Coordination – environmental review and intergovernmental agreements

Chapter 10 – Alternatives – lists Roadway Alternatives A, B, C, D, E (recommended by PCDOT), F and variations of Alternative E

Chapter 11 – Conclusions and Recommendations

Chapter 12 – Cost Estimate and Budget Considerations

Chapter 13 – References

Appendix A – Construction Costs and Estimate

Appendix B – Community Advisory Committee and Public Involvement Information

Appendix C – Stage 1 (15%) Construction Plans – bound separately

Appendix D – Right-of-Way Requirement Plans – bound separately

There were several questions from the CAC regarding the DCR as follows:

Where will the money come from to improve the La Cholla Boulevard and River Road intersections?

The River Road intersection is a separate project with a separate budget. The County has allocated money separate from the La Cholla Boulevard project to build the River Road intersection improvements.

Where will the money come from for the installation of the new drainage improvements along this corridor?

The project cost estimate has included the additional storm drain since early planning and design identified this as a need.

Was the cost of sound walls added into the cost estimate?

Yes. When the noise study found walls to be warranted, the cost became a part of the estimate.

When will the CAC need to write the letter to the BOS?

After the open house comments are received and documented. The summary of comments will be made available to the CAC prior to writing the letter to the BOS. The public has two weeks after the open house to return comments to be included in the EAMR.

What about the alternative that the CAC is going to recommend?

Dean stated that the team had brought both alignments with them to the meeting tonight. PCDOT is aware that the CAC wants to choose an option that is not being recommended by the County. Dean said that all of the alternatives are summarized in the draft DCR, but the County will only develop one

set of plans. It is not cost-effective to design several roadway alternatives. At the 15-percent stage of plans, since the alternatives are not dramatically different, the team is able to show the CAC's alignment choice conceptually. However, further along in the process, it will not be feasible to continue to develop multiple alternatives in detail.

In Alternative B, how far would the road have to be moved over?

Bob stated that the lanes would be wider with Alternative B and he would most likely move the road over 40 feet. With Alternatives A and B, that would allow for wider lanes and a much wider frontage road since property would be purchased in those alternatives. In Alternative D, that alternative would be purchasing both sides of La Cholla Boulevard, reducing the need for frontage roads along the project corridor. This alternative would also keep the roadway on its current centerline.

Dean let the members know that on Page 60 of the draft DCR, there was a cost estimate of the different alternatives. Construction costs for Alternative B would be approximately \$23.8 million in comparison to Alternative D, which would be an approximate cost of \$23.5 million.

Would the CAC's alternatives be presented at the open house?

PCDOT plans to bring the county's recommended Alternative E to the open house. The team will also bring the CAC's alternative they will be recommending; however, they will not bring all of the alternatives to the public meeting.

How will south La Cholla Boulevard look with the intersection expanded at Ruthrauff Road and the possibility of La Cholla Boulevard being expanded past Wetmore Road?

After passing through the intersection, the roadway will gradually narrow back to two lanes. There are no plans in the 20-year plan to widen La Cholla Boulevard in that area. The traffic counts collected do not anticipate much growth.

If Alternative D purchases both sides of La Cholla Boulevard, there would not be frontage roads. Would the cost be reduced because frontage roads would not be needed?

No, there would be other costs incurred like wider lanes. Dean referred the members to Page 50 of their draft DCR to see which houses would be purchased and what the roadway would look like. Money is tight on this project and Dean stated that PCDOT already has the right-of-way needed to build Alternative E.

Dean stated that the team still had the EAMR to brief the members on. He said that the EAMR has a lot of the same information, as the DCR and Scott Stapp, HDR Engineering, would be presenting the EAMR to the members.

The EAMR begins with the executive summary. Scott stated that the summary only summarizes the information that is written in the report, so he is strongly urging everyone to read the entire report prior to reading the summary.

After the title page of the EAMR, there will be a table of contents that will outline what is in the report. The actual report begins on page one, after the executive summary. For your reference, the executive summary portion has ES before each page number.

Scott again stated that the draft EAMR has most of the same information as the draft DCR. This EAMR contains:

Chapter 1 – Background – project cost and funding, direction by BOS, and project design process

Chapter 2 – Purpose and Need

Chapter 3 – Project Setting

Chapter 4 – Proposed Project

Chapter 5 – Environmental Screening

Chapter 6 – Environmental Assessment and Mitigation – Natural/Physical Environment; biological resources, drainage and clean water act, floodplain, air quality, noise, utilities, hazardous materials, construction activities, cultural resources, visual resources; and Neighborhood/Social Environment; right-of-way acquisition, temporary and permanent access and parking impacts, neighborhood disruption, parks and recreational areas, consistency with other plans

Chapter 7 – Agency Coordination

Chapter 8 – Public Participation – public participation activities; community comments

Chapter 9 – Conclusions and Recommendations

Chapter 10 – References

Chapter 11 – Abbreviations and Acronyms

Scott told members to refer to Chapter 10 in order to get information on the documentation used in this report. He referred members to Chapter 11 for a list of acronyms that will be used throughout the documents. He also pointed members to the inside cover of their booklets where a CD is located that includes all the text and appendices in the document.

Scott referred everyone to table ES – 7, which is the summary for the environmental impacts and recommendations. He stated that in the summary it would show potential impacts, recommended mitigation, agency coordination and consultation and parties responsible for implementation. Scott mentioned that there were cultural sites found and the design of the roadway was slightly altered to avoid the sites. These sites will continue to be monitored during the course of construction. There was also a lot of past discussion regarding traffic noise. The entire study is contained in this report and what areas were found to warrant sound barriers as a result of the noise study. Another area of concern was hazardous material. Because of the potential of hazardous material from prior businesses, trenches were dug along the corridor and soil samples were gathered. The area was found to be free of any hazardous material. Borings were also done where known storage tanks were located once known to have been leaking. No concerns were noted with the extensive investigations that took place as a part of this report.

Scott took a moment to explain to everyone how to read the main report. He asked the members to turn to Page 18 of their report. Under Section 6.1.1, the CAC would find the Biological Resources section of the report. Under this section the members will find the existing conditions, what permits are needed, potential impacts and mitigation measures. Each chapter has this information available along with additional supporting documentation as warranted.

This report was based on Alternative E. Scott stated that some of this information would change if another alternative were to be chosen. For example, if the design moves out of the existing right-of-way, there may need to be additional contamination testing done to ensure the area is safe to work in.

There were several questions from the CAC regarding the EAMR as follows:

Where are the cultural sites located that were mentioned?

That information is excluded from the public record to ensure the areas stay undisturbed.

I was under the understanding that there were not any sites located in this project area?

There are no cultural resources within the project site; however, there are some sites that are adjacent to the project area that will be monitored.

Dean took a moment to point everyone to Pages ES – 12 and 13. This section contains a summary of the CAC and public concerns within the project area. More detail on this subject could be found in the main document beginning on Page 67.

There is also information in the Appendix on public art. Ellie Towne, CAC member, was on the selection committee to choose the artist that will work on this project. Vicki Scuri from Seattle Wash. was chosen as the artist on this project. There is not a lot of information at this point. She has some preliminary concepts she is working on. When the project is further along, she will come to Tucson and meet with the CAC to discuss her ideas on the public art that will be along La Cholla Boulevard. She is currently exploring art on the railing of the bridge and noise barrier walls. She will want the input from the committee when she is ready to move forward in her designs.

Will she be aware of all the safety features that will be a part of this project?

Yes, she will have all the necessary information when it is time to move forward.

Are there noise walls needed for Alternative D?

Noise walls will not be needed with Alternative D because both sides of the roadway would be purchased eliminating the need for sound mitigation. Dean stated that Scott would be available for any noise questions.

What if some residents don't want walls where walls are warranted?

If noise walls are still warranted upon approval from the BOS, there is a process the team will go through to find out who wants the walls in the areas where noise walls could be added. There has to be a majority of approval for each wall in order for the wall to be constructed.

Are all the documents in this report specific to Alternative E?

All the alternatives are presented in this report. PCDOT's recommendation is Alternative E, which stays within the current right of way, meets the mandates of the County and disrupts as few residents in the area as possible.

Dean stated that since there were no further questions, he wanted to remind everyone of the public open house which would be held on Thursday, Sept. 11, 2008, from 5:30 – 7:30 p.m. at the Ellie Towne Flowing Wells Community Center.

Dean reminded the CAC that their responsibility would include reading through the documents to get prepared to write their letter to the BOS. Dean stated that it would be up to the CAC if they wanted to use the letter that the CAC sent to the BOS in January; however, they could write a new one if they felt it was necessary. The public has a two-week period after the open house in which comments can be received. Those comments will be given to the CAC for their review. The members should have their letter completed near the end of September.

Dean stated that if there were any individual questions, the team would stay to answer those specific questions.

Can the CAC get a copy of the mailing list? The members would like to send information to the people who received the invitation to the open house.

Carol Brichta, PCDOT Community Relations, stated she would have to check to see if that would be possible. Dean stated that he would like the CAC and PCDOT to work together on any mailing that goes out to the community; it is important the CAC and PCDOT work as a team in this process. Dean stated the team would be happy to have questions on the survey if the CAC wanted specific concerns addressed with the public.

The meeting was adjourned at 7:05 p.m.

**Pima County Department of Transportation
La Cholla Boulevard:
River Road to Ruthrauff Road**

The Pima County Department of Transportation (PCDOT) held an informational public meeting to address roadway improvement conceptual design on La Cholla Boulevard from River Road to Ruthrauff Road. Area residents, property owners, stakeholders and jurisdictional representatives attended the public meeting. PCDOT representatives and the design team conducted a formal presentation at 6:15 p.m. Before and after the presentation, the meeting followed an open house format with maps, displays and other informational materials available for the public to view. Those in attendance were encouraged to complete and submit comment forms.

Public Meeting

- Thursday, March 6, 2008
 - Ellie Towne Flowing Wells Community Center
 - 6 – 8:00 p.m.; Presentation at 6:15 p.m.

Public Notification

- Invitation-postcard announcing meeting mailed the week of Feb. 18, 2008
 - Mailing area included businesses and residences one half mile north of Ruthrauff Road and one half mile south of River Road; and one half mile east-west of La Cholla Boulevard between Ruthrauff Road and River Road
- Newspaper advertisements ran in the Arizona Daily Star on Feb. 24, 2008 and the Daily Territorial on Feb. 15, 2008
- Regional Transportation Authority (RTA) made contact with more than 100 businesses along the corridor providing them with information about the RTA Mainstreet program and invitations to the public meeting

Team Attendance

- **PCDOT:** Carol Brichta, Priscilla Cornelio, Ali Fermawi, Dean Papajohn, Annabelle Quihuis
- **Pima County District III:** Kiki Navarro – Representative for Supervisor Sharon Bronson
- **HDR:** Larry Barela, Bob Brittain, Ted Buell, Scott Stapp, René Tanner
- **Kimley-Horn and Associates:** Scott Beck
- **McGann & Associates:** Darlene Showalter
- **Public Artist:** Vicki Scuri
- **RTA:** Britton Dornquast, Steve Taylor
- **Tucson Pima Arts Council (TPAC):** Jane Hallet
- **Gordley Design Group:** Susan Parcells, Arizeder Urreiztieta

Materials

- Fact Sheet
- Comment forms
- Sign-in sheets
- RTA brochures

Displays and Presentation

- Six information stations each with displays
 - Aerial photos showing alignment and access management
 - Typical cross-sections
 - Informational display board on noise
 - Notebook with examples of public art
 - Landscape design concepts and Rillito River Park access concepts
 - Brochures (RTA/Mainstreet; Pima County Real Property)
 - Concept bridge plans
- PowerPoint presentation

Public Attendance

- 84 attended

Public Comments

- Sixteen individuals submitted comment forms at the public meeting and during the two-week comment period ending March 20, 2008.

Open House No. 1 Comment Summary

Name	Address	Phone	E-mail	Add to Mailing List	1. What is your primary interest in La Cholla?	2. What do you like about this project?	3. Please list any questions or concerns about this project.	4. Please list any other comments you have concerning this project.
Comments Received at the Open House								
Betancourt, Celia	2121 W. Calle Narciso Tucson, AZ 85705			yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	Side walks; Sound Barriers	More Traffic; Access into Calle Narciso in and out	Have a sound barrier or wall between house and road; narrowing of lanes further down the road.
Carrig, Cheryl	4964 N. Jay Ave. Tucson, AZ 85705	888-5685		Yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	Kill the congestion.	If expansion is to the east, which I own property and reside in, what will happen to my property?	If I am concerned with what will happen to my property, if anything.
Chardukian, Doris	5242 N. Royal Palm Dr. Tucson, AZ 85705	690-0690		Yes	I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard. I own property off of Curtis Road.			When heading south on Curtis Road, there is a lane with a solid white line crossing out this lane. If this was made into a right-turn lane now, it would help traffic some. I have almost been hit by vehicles in that lane.
Gaona, Robert	4921 N. La Cholla Blvd. Tucson, AZ 85705	887-1395		Yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	Fix the traffic flow.	Drainage	Will we be bought out?
King, Gloria	2202 W. Calle Narciso Tucson, AZ 85705			Yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	Drainage problems to be corrected; sidewalks; landscaping	There need to be sound barriers between roadway and housing areas - this was not mentioned, but there is a need for them. Where is the art work going to go? Wider roads attract more traffic.	Questions from the floor were not encouraged. What is the plan for access to and from Calle Narciso? The lanes south of River Road need to be the same width as north of River Road - they should not be narrower.
Larsen, Svein Larsen, Carol	2125 W. Calle Fortunado Tucson, AZ 85705	690-9091		Yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	This will be a wonderful north-south roadway.	I live at 2125 W. Calle Fortunado. Access is on La Cholla Boulevard at Calle Narciso. When La Cholla is completed, I believe access will be very difficult because of the closeness to the traffic light at Ruthrauff Road, and just in general because of a major thoroughfare. I have lived at this address since 1964.	This was a good meeting and a very nice community center. I hope the traffic lights at Ruthrauff and Curtis Roads can somehow work as a team. The light at Ruthrauff Road now just makes traffic more congested instead of helping traffic.

Open House No. 1 Comment Summary

Name	Address	Phone	E-mail	Add to Mailing List	1. What is your primary interest in La Cholla?	2. What do you like about this project?	3. Please list any questions or concerns about this project.	4. Please list any other comments you have concerning this project.
Mattausch, William	2462 W. Kimberly Pl. Tucson, AZ 85705	293-2958		Yes	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	It is a good thing. It will move traffic quickly and safely.	Will we be able to pull heavy trucks off the residential streets, i.e., Kain Avenue (north and south) between Ruthrauff Road and Curtis Road?	The three companies who move very large and heavy equipment; can they be required to use Curtis Road and La Cholla Boulevard routes.
Plog, Patricia Plog, Terry	1965 W. Cusco Pl. # 2 Tucson, AZ 85705	235-0289	patriciaplog@yahoo.com	Yes	I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard. I live two blocks away from La Cholla Boulevard.	More space!! Traffic flow.	No questions.	I was hoping this project was further along. Need it now!!
Price, Robert	4950 N. La Cholla Blvd. Tucson, AZ 85705	888-2708	rprice15@comcast.net	Yes	I live in rented property adjacent to this section of La Cholla Boulevard. I own rental property in which I live in the area adjacent to this section of La Cholla Boulevard.	No wall being put up.	A wall.	How long will it take?
Schweska, Stephen	4846-4854 N. La Cholla Blvd. Tucson, AZ 85705	906-2961		Yes			Ingress/egress is needed for businesses on the east side of La Cholla Boulevard. I would like to enter straight in and straight out - no frontage road. I am concerned that the bus stop is too close to the intersection of Ruthrauff Road and La Cholla Boulevard.	

Open House No. 1 Comment Summary

Name	Address	Phone	E-mail	Add to Mailing List	1. What is your primary interest in La Cholla?	2. What do you like about this project?	3. Please list any questions or concerns about this project.	4. Please list any other comments you have concerning this project.
Steakman, Laura	5484 N. Bramble Brook Ln. Tucson, AZ 85704	888-9730	laurafree@comast.net	Yes	I regularly drive through this section of La Cholla Boulevard. I live in Riverside Place subdivision next to Bashas'.	Everything, especially relief from traffic congestion.	When is the widening of River Road to six lanes going to begin so we can make a U-turn at the intersection of La Cholla Boulevard and River Road to go east on River Road?	What is the tentative timeline to completion?
Van Echo, Jay	6750 W. El Camino del Cerro, Tucson, AZ 85745	917-4534	jay.vanecho@dmjmharris.com	Yes	I regularly drive through this section of La Cholla Boulevard; to and from work and to shop regularly at River Road and La Cholla Boulevard - also a regular at Dairy Queen!	Improve a.m. and p.m. peak turn movements; improve roadway safety and reduce congestion; new bridge at the river; improved access.	Please do not spend my tax money foolishly; if you can build it within existing Right-Of-Way and not spend \$4+ million more - do it! However, make sure landscape amenities and safe access are provided to homeowners - signal timing will be critical. Build it now - turn lane into a dedicated westbound to-westbound right-turn lane into a dedicated westbound Rutbrauff Road take advantage of a lull in construction activities to get a good bid!	The eastbound-to-northbound and conversely the southbound-to-westbound movements are extremely critical - please make sure turn-bay storages are sufficient. How about a free southbound-to-westbound right-turn lane into a dedicated westbound Rutbrauff Road acceleration lane to drop at a sufficient distance to the west?
Williams, Donald	5242 N. Royal Palms Dr. Tucson, AZ 85705	690-0690		Yes	I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard. I live off of Curtis Road.			Until this project is started or completed, is there any reason going south on La Cholla Boulevard across the bridge, that drivers could merge right into a lane that is striped as a no-drive lane to make a right on Curtis Road? It might help the flow of traffic until completion of the project.
Anonymous					I regularly drive through this section of La Cholla Boulevard.	Looks nice.	Construction time - five years? This is a major way to the hospital.	
Anonymous					Commercial Development			Get this project bid!! This is a period one RTA commitment! Forget the Right-Of-Way purchase! Get the construction cost more in line with the

Open House No. 1 Comment Summary

Name	Address	Phone	E-mail	Add to Mailing List	1. What is your primary interest in La Cholla?	2. What do you like about this project?	3. Please list any questions or concerns about this project.	4. Please list any other comments you have concerning this project.
Miller, Chad	4545 N. La Cholla Blvd. Tucson, AZ 85705	696-8557	millerch@foxwin gwells.k12.az.us	Yes	I regularly drive through this section of La Cholla Boulevard. I work at the school south of this section of La Cholla Boulevard.	To move traffic through the bridge and Curtis Road intersection while traveling southbound-northbound. Also, the sidewalks will be an added safety feature for students walking home from school.	Increase in the width of the intersection at La Cholla Boulevard and Ruthrauff Road. This will directly impact the safety of the students commuting to and from school.	I would like to request that the intersection at Ruthrauff Road have long enough "walk" times so students can safely travel through the intersection.
Bass, Bonny	1502 W. Kilburn Tucson, AZ 85705	293-6841	bbass@pd- law.com	Yes	I own a rental property in the area adjacent to this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	I like the fact that the project is getting done. I think the section of road is long overdue for repairs and upgrading, and will ultimately be an asset to people who live in the area and commute through it.	See attached comments.	See attached comments.



La Cholla Boulevard: River Road to Ruthrauff Road



Comment Form
March 6, 2008

1. What is your primary interest in La Cholla?

- I live in rented property adjacent to this section of La Cholla.
- I own the property in which I live in the area adjacent to this section of La Cholla.
- I own rental property in the area adjacent to this section of La Cholla.
- I work in a business on this section of La Cholla.
- I regularly drive through this section of La Cholla.
- I regularly walk or bike through this section of La Cholla.
- Other, please explain. _____

2. What do you like about this project?

See Attached

3. Please list any questions or concerns about this project.

See Attached

4. Please list any other comments you have concerning this project.

See Attached

IMPORTANT! Please print the following information:

Name: Dimmy Bass Telephone: 293-6841
 Address: 1503 W Kilburn
 City: Tucson State: Az Zip: 85705
 e-mail: bbass@pd-law.com

Mail to: Pima County Community Relations Office, 201 N. Stone 4th floor, Tucson, Arizona 85701 or Fax to 740-6439

For more information visit <http://www.roadprojects.pima.gov/laChollaRiver>

2. What did you like about this project?

I like the fact that the project is getting done. I think this section of road is long overdue for repairs and upgrading and will ultimately be an asset to people who live in the area and who commute through it.

3. Please list any questions or concerns about this project:

- a. I am disappointed by the design of the proposed project. It adversely affects people's homes, property and lives due to the increase of congestion and noise the improved roadway will bring to the neighborhood.
- b. I would like to know why bus pullouts will not be designed into the project. One selling point of the RTA prior to the election last year was that bus pullouts would be instituted on major arteries to aid in the efficient flow of traffic.
- c. The fact that Pima County sees no need for sound abatement bothers me. I do not believe that this project would be presented to neighborhoods where the "perceived" income of the residents is higher. I have always felt and continue to feel that Pima County does not care about the "lower income" citizens who reside in these neighborhoods and therefore does not care to rectify the noise problems.
- d. I continue to be distressed by the fact that Pima County believes it is okay to narrow the travel lanes along the stretch of roadway where the 10 or 11 homes are to "squeeze" the roadway in.
- e. I would like to know why no one, other than the RTA and Pima County, had a say in who the artist on this project would be. In attending other road widening projects, the CAC Committee members will be given the opportunity to choose the artist. We are stuck with some lady, even if she is nationally recognized, from Seattle. I find it hard to believe that there are no qualified artists in the entire State of Arizona. I am assuming we will be stuck with a "Sonora" or other similarly bad art projects in and around Tucson.
- f. As the project is proposed, there is no room for landscaping.
- g. I do not like the one way access roads proposed for the east and west sides of the street. It is unlike any other roadway projects in the Tucson area, except for some that were done perhaps in the 1980's. Pricilla Cornelio's solution is for residents to use the alley behind their homes to access their properties. This is not a viable solution and the arrogance of the suggestion is a slap in the face.
- h. I continue to be concerned how the City of Tucson will maintain its well site on La Cholla Boulevard after the access road is installed. The maintenance vehicles

will virtually close the access road to any through traffic since there will be no where for them to park.

- i. The suggestion by an Engineer at the Open House that although this access road is proposed to be one way, it won't matter whether or not people drive two ways on it was insulting. His further suggestion that people will park along this access road even if they are told not to was also insulting. If there is no parking there, then the PCSO could issue parking citations.
- j. I am confused why the road width and the sidewalk width will not be constructed according to guidelines set out in the *Pima County Roadway Design Manual*.
- k. If there is such a big problem with the budget then who is responsible for the shortfall? If the County knew they were going to do this project, and understood that the project needed to comply with the *Pima County Roadway Design Manual*, then why weren't the appropriate amount of funds set aside? Who fell down on the job?

**Pima County Department of Transportation
La Cholla Boulevard: Ruthrauff Road to River Road
Open House Summary**

Date, Location and Time

- Thursday, Sept. 11, 2008
- Ellie Towne Flowing Wells Community Center
- 6 to 8 p.m.

Public Notification

- Postcard announcing meeting mailed:
 - Week of Aug. 11, 2008
 - Mailed to approximately 900 residents and businesses in a one-half-mile radius of the project area
- Newspaper notification:
 - Arizona Daily Star – Aug. 27, 2008
 - Daily Territorial – Aug. 27, 2008
- Web site:
 - Meeting date and time was posted on project Web site
- Business outreach
 - Sept. 2 and 3, 2008; attempted to or made contact with 62 businesses along the corridor

Team Attendance

- **Pima County:** Priscilla Cornelio, Rick Ellis, Ali Fermawi, Ana Olivares, Dean Papajohn, Annabelle Quihuis
- **HDR Engineering:** Larry Barela, Mike Barton, Catherine Bolm, Ted Buell, Martha Davis, Christine Jacobs-Donoghue, Bethy McGehee, Arturo Ledesma
- **McGann & Associates:** Darlene Showalter
- **City of Tucson Water Department:** Patricia Eisenberger
- **Regional Transportation Authority (RTA); MainStreet Program:** Britton Dornquast, Steve Taylor
- **SunTran:** Bea Paulus
- **Gordley Design Group:** Barb Alley, Arizeder Urreiztieta

Public Attendance

- 50

Comments

- Six comments received at the open house
- Thirty one comments received during the two-week period following the open house

Materials

- Comment forms
- Fact sheets
- Sign-in sheets

Agenda

- Introductory remarks: Sharon Bronson, Pima County Board of Supervisor
- Question-and-answer session

- Team introductions
- Review displays with one-on-one interaction

Displays

- Bridge design
- City of Tucson Water
- Environmental
- Landscape design
- Roadway Design (two display tables)
- RTA MainStreet
- SunTran

Room Set-up

- Sign-in table
- Refreshment table
- Eight display tables set up around the room
- Seating for approximately 45 people with podium

Signs

- A-frame signs to direct traffic into parking lot and facility

La Cholla Boulevard: River Road to Ruthrauff Road Public Opinion Survey Sept. 11, 2008								
Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project.	3. Please list any comments you have about design elements of the proposed roadway improvements, such as sidewalks, bike lanes, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.
Comments received at the Open House								
Bastierfeld, Gary and Jackie	5745 N. La Cholla Blvd. Tucson, AZ 85741			I regularly drive through this section of La Cholla Boulevard.	The reason we like this project is because it will help with traffic congestion in the area along La Cholla Boulevard.	Please make sure the bike lanes are wide enough. Also, at Ruthrauff Road and La Cholla Boulevard, is it possible to have right-turn arrows? Too many people make the right turn on red to beat out the people making a left turn off of Ruthrauff Road onto La Cholla Boulevard.		
Erickson, Bill	4967 N. La Cholla Blvd. Tucson, AZ 85705	509-6969		I own rental property in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	I like the three-lane each way design. Hopefully it will prevail.	Taggers spray painting on the sound barrier wall.		
Jarvis, James	4935 N. Jay Ave. P.O. Box 50703 Tucson, AZ 85705	888-2497		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	I like the three-lane each way design. Hopefully it will prevail.	Please include wide sidewalks and bike lanes.	Trees are great, but please don't put them too close to intersections. They are hard to see around when you are turning onto the busy, fast roadway. Bushes are great.	I am concerned about the "cut-through" traffic on Jay Avenue right now, as well as what can be planned to protect our neighborhood during construction.
Krueger, Wendy	2211 W. Calle Narciso Tucson, AZ 85705	349-3844		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.		La Cholla Boulevard and Calle Narciso raised median makes it very difficult to turn north.		
Metz, Wayne	4901 W. La Cholla Blvd. Tucson, AZ 85705	887-0553	ps341@msn.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	More lanes.	The County plans to take away parking in front of my home, subjects me to increased noise as no wall will be built in front of my home. This also subjects me to increased safety risk due to increased lanes and will lower my property value.	The current plans will bring the road within 12 to 15 feet of my home. This will subject me to increased noise and air pollution. The County plans do not really provide for landscaping unless they opt for alternate plans.	Please approve Alternative D.
Steckman, Laura	5484 N. Bramble Brook Ln. Tucson, AZ 85704		laurafree@comcast.net	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	Well organized presentations for the community.	Can drivers heading south on La Cholla Boulevard make U-turns? If not, please allow for them.		Please allow U-turns at double left-turn lanes. I cannot exit out of my house on the northeast corner of River Road to go east onto River Road because there is too much traffic.
Comments received after the Open House								
Bender, Dick Shamrock Dairy	1900 W. Ruthrauff Rd. Tucson, AZ 85705	887-0300		I regularly drive through this section of La Cholla Boulevard.	It will help to relieve congestion.			
Green, Mary	1949 W. Paul Pl. Tucson, AZ 85705	887-2932		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	It would no longer be necessary to merge to one lane when driving south on La Cholla Boulevard - it is dangerous. Also, it would considerably improve the whole area.	I hope the median landscaping will be well taken care of - landscaping along both sides of Ruthrauff Road needs trimming.		I hope it would require a general clean-up of properties around the area.

La Cholla Boulevard: River Road to Ruthrauff Road
Public Opinion Survey
Sept. 11, 2008

Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project	3. Please list any comments you have about design elements of the proposed roadway improvements, such as vehicle lanes, bike lanes, sidewalks, medians, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.
Horn, Marvin	4961 N. La Cholla Blvd. Tucson, AZ 85705	887-1056		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	Everything about this project will be a problem because of limited space. The traffic and its elements will be too close to our home. We are disappointed about the proposed complicated roadways and lack of access to emergency vehicles and pedestrians.	We feel that the design elements are not improvements and will increase noise, pollution, decrease street access and our overall property value will go down. We will no longer have a view.	We were not aware that this area was to become a business area. The quality of our neighborhood has vastly decreased. We will no longer have access for visitors.	
Hoxsie, Dolores	4661 N. Brightside Dr. Tucson, AZ 85705	603-9509		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	It will move traffic faster and safer.	Noise control?		
Langford, Dave and Patty	2049 W. Brittain Dr. Tucson, AZ 85705	203-6319	pattylangford@yahoo.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. We live in Edgebrook 1.	Making the road six lanes is great, along with better lanes for bicycles, etc. - great!	We would like something (and maybe this is in the plan) put up to hide the unsightly trailers and homes that are on the other side of Curtis Road. Also, the Quick Mart Family Foods needs to be cleaned up.	Sorry we couldn't make the meeting, my husband is out of town and I work until 7:30 or 8 p.m. We would be pleased to support this project!	
Moreno, Rosalino	4971 N. Mathews Ave. Tucson, AZ 85705	408-1160		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	All proposed improvements.		A traffic signal is needed at the Calle Narciso and La Cholla Boulevard intersection. With all the improvements, the problem will be worse.	
Muhs, Peggy	5513 N. Silver Stream Way Tucson, AZ 85704	408-5235	pjmuh@aol.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	Everything!!! Much needed project!	This should be a continuation of La Cholla Boulevard as far as design.	I think the art work should be minimal as the area is prone to taggers, and maintenance and clean-up is a real concern to be figured into the project.	
Loflin, Isabell	4921 N. River Vista Tucson, AZ 85705			I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	Improved bicycle mobility, safety and better bridge crossing for bicycles. I am glad the area will be brightened up with public art and landscaping.	In the process of redoing the bridge, the bike and hiking path along the Rillito River should be repaved.		
Peterman, Dan and Eleanor	1940 W. Paul Pl. Tucson, AZ 85705	396-8099	nanapeterman7777@comcast.net	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	There will be no more traffic congestion.	As long as there will be turning lanes, I will be happy.	Noise is a factor.	When will this work begin?

La Cholla Boulevard: River Road to Ruthrauff Road Public Opinion Survey Sept. 11, 2008									
Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project.	3. Please list any comments you have about design elements of the proposed roadway improvements, such as vehicle lanes, bike lanes, sidewalks, medians, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.	
Logan, Belinda	2232 W. Calle Fortunado Tucson, AZ 85705			I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	I like the idea of the extra lane for traffic flow.	Why six lanes? I believe that is too many lanes. Will this affect the road between Wetmore Road and Ruthrauff Road with the schools?	Noise would be a concern.		
Rafje, Jeff	1938 W. Alder Grove Dr. Tucson, AZ 85704	408-8269	jmatje@hotmail.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I regularly walk or bike through this section of La Cholla Boulevard.	I ride SunTran from this area to work each day. The bus improvements are a welcome addition. The landscape median over the bridge is also a nice design element.	It would be nice to have improvements to the River Walk as part of construction.	The increased traffic around River Road and La Cholla Boulevard poses a problem for the growing number of residences and new home construction. Noise Mitigation is necessary. Also, graffiti is a big problem here. Anti-graffiti materials and a dedication to remove it quickly need to be factored in.	As someone who lives in the area, traffic has grown exponentially due to the work on I-10. I am concerned this area will become so busy it is not nice to live here anymore. Traffic mitigation needs to be looked at. Enhanced SunTran routes might help with this.	
Smith, William	5559 N. Silver Stream Rd. Tucson, AZ 85704	293-1153		I live in the area adjacent to this section of La Cholla Boulevard.	Finish the major roadway improvements for River Road and La Cholla Boulevard.	Don't forget roadside visual barriers where needed.	Make sure there is good visual roadside barriers along ugly post development areas like La Cholla Boulevard - not like La Cholla Boulevard on the north side of River Road on the west side - ugly missing sections.	Consider roundabouts at major intersections. See attachment of Roundabout in Carmel, Indiana.	
Shash, Betty Ann	2151 W. Calle Narciso Tucson, AZ 85705	887-3273	cozette@aol.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	A left-turn lane at La Cholla Boulevard and Calle Narciso. It is almost impossible to make a left turn onto La Cholla Boulevard.	Please give us an attractive wall - not the ugly multicolored wall between Orange Grove Road and Ipa Road is very pretty and would add to the neighborhood.	This work needs to be done and it will make getting around much easier.	I would like to be informed on the progress. I was unable to attend the meeting due to illness.	
Thompson, Lori	4783 N. Woodside Dr. Tucson, AZ 85705		thomplee@gmail.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	Upgrades! Newness!	I would prefer a more aesthetic and pleasing view of this area with provisions for safety components. I would prefer to see less of the unattractive homes and businesses along the La Cholla Boulevard and Curtis Road area.	Perhaps have medians with landscaping. Maybe a barrier wall hiding the homes and businesses along La Cholla Boulevard (south of River Road to Ruthrauff Road).		
Wiewel, Martha	1847 W. Waterleaf Dr. Tucson, AZ 85704	887-2957	marthawiewel@msn.com	I own property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	I would like to bike from La Cholla Boulevard to Ruthrauff Road and I would like to bike on Ruthrauff Road to Sweetwater Drive.	Bike lanes - sidewalk and other improvements as you desire.	Please do not plant trees or bushes. People throw trash under them and they are not trimmed. Plant ocotillo, saguaros or golden barrels.	This project sounds good to me. I want to thank you for the work you are accomplishing for the County. I would like to see the debris picked up more often, especially along River Road. The trees need to be trimmed.	
Zumpano, Leonard	4716 N. Woodside Dr. Tucson, AZ 85705	690-7349		I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	I feel this would help reduce traffic congestion.	This would help with traffic flow. Access and to be able to pull off the road with commuter traffic buses would not slow up the traffic flow, especially during rush hour.	Natural landscape trees, bushes and cactus would bring improvements.	This should improve traffic flow and hopefully reduce accidents due to impatience in long lines at the light.	

La Cholla Boulevard: River Road to Ruthrauff Road
Public Opinion Survey
Sept. 11, 2008

Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project.	3. Please list any comments you have about design elements of the proposed roadway improvements, such as vehicle lanes, bike lanes, sidewalk, median, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.
Anonymous				I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard. I use Ruthrauff Road and La Cholla Boulevard at least once a day.	After widening Ruthrauff Road and all the improvements there, La Cholla Boulevard is woefully lacking in space for cars driving down it. I think this will improve La Cholla Boulevard.	There is quite a bit of dust around the side of the roads. I would hope that they would bar that area to cut down on the amount of dust that comes from the side of the road. Hopefully the area around the wash will be improved for bikers. I hope they put appropriate speed limit signs - that is a real speed zone and I have seen several accidents in that area in the past few months. I worry for those who will be working in the area. The islands make it impossible to get into my property from the north. We need a center lane.	Seasonally they get a lot of water in the wash. I hope diligence is taken to make sure that this area is safe, especially for businesses that are near the wash. I like those multi-colored walls that are further down on La Cholla Boulevard. I hope color is used on this project.	I would just like to be kept in the loop in terms of progress and suggestions for keeping us safe for those who are working in the area, and realize that this will take some time. Keep law enforcement in the loop to keep everyone safe. But, I approve this project.
Nita Ruth's, LLC (Nita Freeman)	5140 N. La Cholla Blvd. Tucson, AZ 85705	797-0603		I own or work in a business on this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	Widening. It is about time.		This also affects other property owners and businesses south of the bridge.	
Molis, Frank	2385 W. Wave Hill Ct. Tucson, AZ 85705	888-2000		I own or work in a business on this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.		There needs to be two left turns off of Ruthrauff Road to go north on La Cholla Boulevard. There have been as many as 25 cars backed up trying to make that turn.		
Morse, Dwayne and Heather STL Manufacturing	5150 N. La Cholla Blvd. #2 Tucson, AZ 85705		dwayne@stlmfg.com	I own or work in a business on this section of La Cholla Boulevard. (We have owned our business in this location for 15 years.)	The road construction will beautify the area and hopefully move the traffic more efficiently down La Cholla Boulevard.	We have major concerns about the access in and out of the narrow drive off of La Cholla Boulevard. We have semi tractor trailers with steel loads in and out frequently. The inability to make a left-hand turn in and out of the property will cause time delays and safety concerns. There are four businesses that would be affected by this. Three of which utilize semi tractor trailers. We would like you to consider a southbound turn lane into the property for ease of access and also widening the driveway to facilitate ingress and egress with large/long loads.	We would like to see the improvement of the road and neighborhood, but not compromising our access for business.	We would also like to know about any plans for sewage service for any of the businesses. We (for the most part) have septic systems) would like to know if a pump system will be put in place. We need to be able to maintain full-time access throughout the work week during construction. We have delivery trucks in and out, and also our own service trucks are in and out frequently.
Callhoon, Don	4770 W. Benjamin Rd. Tucson, AZ 85743	572-8455 906-2344		I own a rental property in the area adjacent to this section of La Cholla Boulevard. (4932 N. La Cholla Boulevard)				I am not interested in a high wall in front of property or selling property as I just finished building a duplex. I question property values and relocation expenses if it comes to that.

La Cholla Boulevard: River Road to Ruthrauff Road
Public Opinion Survey
Sept. 11, 2008

Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project.	3. Please list any comments you have about design elements of the proposed roadway improvements, such as vehicle lanes, bike lanes, sidewalks, median, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.
Shurtz, Bill	4944 N. Jay Ave. Tucson, AZ 85705			I own the property in which I live in the area adjacent to this section of La Cholla Boulevard.	I like the new look for this area.	Lighting.	I am looking forward to the new and improved look in this area.	I am hoping to get speed bumps on Jay Avenue from La Cholla Boulevard to Ruthrauff Road. This should not be a by-pass.
Van Echo, Jay	6750 W. El Camino del Cerro Tucson, AZ 85745	299-8700	jay.vanecho@dmjmharris.com	I regularly drive through this section of La Cholla Boulevard. (Daily commute to/from work)	Intersection improvements to La Cholla Boulevard and Ruthrauff Road, the new bridge and the capacity improvements to River Road and Ruthrauff Road.	1) If only four lanes are needed, only build four lanes and save dollars. 2) Do not purchase right of way if not needed. 3) Do not build unnecessary walls if applicable. 4) Be frugal with RTA dollars and build what is required per traffic, drainage and structural standards.	I appreciate the landscape (native vegetation) and public art on RTA projects within budgetary constraints. Control access points close to the intersection.	Be aware of the east- to- north and south- to- west movements at the intersection and time the signal accordingly. Be aware of the right-in/right-out access to Wal-Mart and police accordingly - the public is not obeying signage.
Kunk, Clem and Donna	5120 N. La Cholla Blvd. Tucson, AZ 85705	888-1923	cdgaragedoors@yahoo.com	I own or work in a business on this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	It will bring more traffic by our business and stop the bottlenecking at the bridge.	It is vital that there is a turn lane for southbound traffic into our parking lot that contains six bays. We all have frequent deliveries including semi trucks at our business and the business located at 5050 N. La Cholla Blvd.	These will not affect us.	If there is no turn lane incorporated in to the plan, many delivery trucks will not be able to turn around traveling southbound. This entrance is at the south end of other businesses.
Franzen, Norman and Charlene Accotax P C	4911 N. La Cholla Blvd. Tucson, AZ 85705	744-0600	cfcpa@qwestoffice.com norman@acstone.com	I own or work in a business on this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	We like the improved traffic flow, safety features and aesthetic value to the area.	We do not like the proposed sound barrier. Our property is zoned M1. It was converted to business use many years ago. The value of the property is dependent on clear and convenient access from La Cholla Boulevard. Our ability to use the property for any type of business would be destroyed by the proposed wall. It would be difficult for drivers to find the entrance to the west side of the frontage road. The wall would make it difficult for northbound drivers to see the entrance and how to access it. They would have to watch for the entrance between the end of the wall and the bus stop, at the same time as trying to see through three lanes of oncoming traffic and searching for a place to turn around in order to get to the properties on the west side. The bus stop would obscure the entrance for southbound traffic.	We are not in favor of the proposed sound barriers (see response to question #3). Additionally, the block walls would be hideous without any paint or artwork. The aesthetic value of the project would be lost.	

La Cholla Boulevard: River Road to Ruthrauff Road
Public Opinion Survey
Sept. 11, 2008

Name	Address	Phone	E-mail	1. What is your primary interest in La Cholla Boulevard?	2. Please describe what you like about this project.	3. Please list any comments you have about design elements of the proposed roadway improvements, such as vehicle lanes, bike lanes, sidewalk, median, intersections, utilities, drainage, access, etc.	4. Please list any other comments you have about environmental elements of the proposed roadway improvements, such as landscape, noise, visual impacts, etc.	5. Please provide any other general comments you have about the project.
Stallings, Doug Backwoods RV	5150 N. La Cholla Blvd. Tucson, AZ 85705	293-4400	doug14723@aol.com	I own or work in a business on this section of La Cholla Boulevard.	It will help east traffic back-ups and reduce car accidents. It will improve the look of the area.	I am concerned that there is no turn lane going south to turn left, as there are many businesses that have large trucks and other vehicles in and out all day. How will anyone going south be able to get to our business? Will I lose my parking?	See attached.	
Lafferty, Oakley	5150 N. La Cholla Blvd., #2 Tucson, AZ 85705	408-7647	oakleylafferty@comcast.net	I own or work in a business on this section of La Cholla Boulevard.	Road improvements are sorely needed on this very busy section. Extra lanes and the bridge are a plus. The left-turn lane with arrow northbound onto Curtis Road is clearly needed.	The proposed bull nose at the entrance to 5150 N. La Cholla Boulevard is a restriction to our access. We have many vehicles, large trucks included, both from our business and our suppliers who will have difficulty with this plan.	Landscape and other visual impacts, while important, should clearly be secondary to functionality.	We strongly request a southbound turn lane into our business from the southbound lane. A widened entrance would also be a great help with the traffic from the bar at the back of the complex. (See attached.)
Iammarino, Robert Diamond Ventures, Inc.	2200 E. River Road, Ste. 115 Tucson, AZ 85718	577-0200	biannarino@diammondvent.com	I own the property in which I live in the area adjacent to this section of La Cholla Boulevard. I own rental property in the area adjacent to this section of La Cholla Boulevard.	Implementation of RTA Period 1 roadway improvements. It is expected the roadway will be completed by 2011 and will help traffic improvement.	1) The project needs to meet the voter mandated budget of 14 million dollars. 2) Based on number 1, the project should be designed for four lanes instead of six lanes. 3) The underground storm drainage should be value engineered to take care of the two year storm event. 4) The bus stop on the northwest corner, adjacent to the Circle K, should accommodate APTL employees, inclusive of their handicapped employees who have asked for special bus accommodations. Please contact Sumran. 5) It appears the access locations to serve the southeast corner of River Road and La Cholla Boulevard are adequate.	Visual impact should be conducive to the marketability of our property. Hardscaping and landscaping should be consistent with the architectural theme already established for the southeast corner.	Adhere to the RTA budget and schedule for Period 1 completion! This includes capping the number of Citizen Advisory Committee (CAC) meetings to assure the Period 1 schedule can be met.
Hendricks, Terry	2135 W. Calle Fortunado Tucson, AZ 85705	888-4789 243-1832		I regularly drive through this section of La Cholla Boulevard. My home is 300 feet from La Cholla Boulevard.	The extra travel lanes and medians look at future traffic counts.			Bus shelters are needed.
Leo, Ken	5150 N. La Cholla Blvd. #2 Tucson, AZ 85705	292-1779		I own or work in a business on this section of La Cholla Boulevard.	It improves the neighborhood and traffic flow.	We need access through median for large semi truck deliveries to 5150 #2 N. La Cholla Boulevard.		
Montgomery Auto	5150 N. La Cholla Blvd. #A Tucson, AZ 85705	407-9456		I own or work in a business on this section of La Cholla Boulevard. I regularly drive through this section of La Cholla Boulevard.	The effort being made to keep businesses and residents along La Cholla Boulevard aware of what is being proposed and considering their input.	I am concerned about the lack of an entrance into the business center because of a raised median, no parking outside the fenced area or room to stop to open gate. I am fully aware you can not accommodate everyone, but these are our concerns.	I think walls and any landscape would be a huge improvement.	

Table 1: Summary of Public Participation Plan

Public Participation Plan	Activities completed	Activities remaining
Public meetings	First public meeting held March 6, 2008 from 6-8:00 p.m. at the Ellie Towne Flowing Wells Community Center. Second public meeting held September 11, 2008 from 5:30-7:30 p.m. at the Ellie Towne Flowing Wells Community Center.	Anticipated schedule for remaining public meeting 7/20/09
Public hearing		Will only be held if needed.
Public Opinion questionnaires	Comment form handed out at public meetings held March 6, 2008 and September 11, 2008. See appendix for summary.	Comment forms will be distributed at each of the public meetings. A questionnaire was mailed out to the project area after the draft EAMR was presented at the second open house.
CAC	Six CAC meetings have been held with the County: August 7, 2007; October 2, 2007; October 9, 2007; December 6, 2007; July 24, 2008 and August 12, 2008. The CAC has met without the County on a number of occasions. The CAC has submitted two letters to the County explaining their recommendation for Right-of-way acquisition, wider lanes, increased median buffers for landscape and sound mitigation walls, and public art near the residences.	Meetings will be held to discuss the construction plans and public art.

Table 1: Summary of Public Participation Plan (continued)

<p>Contact with affected parties</p>	<p>Contact information on affected parties is updated on a continuing basis; RTA/Mainstreet is communicating with affected businesses; conference call, Jan Gordley/Fred Bass; Pima County has responded to various phone call and e-mail inquiries.</p>	<p>Ongoing as needed</p>
<p>Media relations</p>	<p>2/12/08 – Report from KVOA TV CH 4 - “Plans to Widen Tucson Road Upsetting Neighbors”. This article quoted a resident who will directly be affected by the widening of La Cholla Boulevard from two lanes to six lanes. The residents want to be bought out by the County; however, as stated in the article, acquiring property would cost the County an additional \$4 to \$5 million. The voters approved funding for this project in 2006.</p> <p>2/22/08 – Report from KOLD TV CH 13 – “State budget Woes Could Keep Trickling Down.” Residents in the area have expressed the need for improvements on this stretch of road; however, with the state having more than a \$1 billion budget shortfall, this project may have to wait. Chuck Huckelberry, Pima County Administator, stated that the local governments are usually the ones to suffer when there is a shortfall at the state level.</p>	<p>Ongoing</p>
<p>Informational materials</p>	<p>A fact sheet was prepared for the public meetings on March 6, 2008 and September 11, 2008.</p> <p>A project Web site is available at: http://www.roadprojects.pima.gov/LaChollaRiver/</p> <p>See appendix for a copy of the fact sheet.</p>	<p>The fact sheet will be updated for each of the future public meetings. The Web site will be updated as needed.</p>

Table 1: Summary of Public Participation Plan (continued)

This chart summarizes key issues raised by CAC and individuals and the County responses.

Table 2: Summary of project concerns and responses

	Issue	County response
1.	Crosswalks	
	Travel lanes	
	Sidewalks	
	Bike lanes	
	Bus stops	
	HAWK light	
	Public art	
	Landscape	
	Well site	
	Access and U-turns	
	Parking	
	Frontage road	
	Alley	
	Accidents	
	Noise	
	Property value	
	Drainage	
	Etc.	

(Information for this table can come from the CAC and DOT correspondence, as well as from the questionnaires.)

- Include in appendix:**
 Questionnaire summary
 CAC letters
 DOT letters responding to the CAC
 Fact sheet



PIMA COUNTY
DEPARTMENT OF TRANSPORTATION
201 NORTH STONE AVENUE, THIRD FLOOR
TUCSON, ARIZONA 85701-1207



PRISCILLA S. CORNELIO, P.E.
DIRECTOR

(520) 740-6410
FAX (520) 838-7537

December 7, 2007

Dear Community Advisory Committee for La Cholla:

Pima County appreciates your service on the Community Advisory Committee for the roadway improvement project on **La Cholla Boulevard: Ruthrauff Road to River Road**. You provide the local perspective that is helpful in identifying needs and wants for this project. Additionally, you have demonstrated your commitment to the project by meeting independently three times since the Oct. CAC meetings.

On Oct. 2 the County presented the recommended roadway alignment for La Cholla Blvd. Due to CAC concerns a follow-up meeting was held Oct. 9. At that meeting the County walked the committee through a number of design alternatives that were developed, but deemed less desirable for not meeting certain criteria. Multiple options were not presented as a menu for the CAC to choose from, but as background information so the CAC could see that the design team had performed due diligence in developing the design amidst competing project needs. During these meetings the CAC expressed a variety of opinions regarding design and alignment. You were asked to put your concerns in writing so that the County could communicate them to the appropriate people within the County and provide a formal response. We informed various people at the County that a written response was forthcoming from the CAC. When a written response was not received it was unclear whether concerns were not as great as first appeared, or if the committee was not in agreement, or if there was something else going on. It was expected that a letter from the CAC would be completed in a couple of weeks. Since two months have passed without receiving written comments, a CAC meeting was held last night to try and facilitate the process. It is apparent that the CAC still has concerns. I strongly encourage the CAC to submit their concerns in writing to Pima County Department of Transportation (DOT). Even if you have not finalized all of your arguments it is important for the County to receive your written feedback at this stage of design. In the meantime, I have informed the director of DOT of your concerns (please see attached memo). County staff continues to collect data to determine the best way to meet as many of the CAC's concerns as possible.

Once again, thank you for serving on this committee. If you have any questions, please let me know.

Regards,

A handwritten signature in blue ink that reads "DEAN".

Dean Papajohn, P.E.
Project Manager

Encl.

January 15, 2008

Regional Transportation Authority
Pima County Transportation Department

Re: La Cholla Boulevard; Ruthrauff Road to River Road
Community Advisory Committee

The Community Advisory Committee (CAC) for the La Cholla Boulevard Improvement Project (4LCITR) would like to thank Pima County and the Regional Transportation Authority for the opportunity to serve and advise on this important project. It is hoped that with the ideas and cooperation of all involved, the citizens of Pima County will benefit from a safe and more efficient corridor in which to travel.

The Regional Transportation Authority (RTA) has several criteria to be considered in the design of La Cholla Boulevard improvement. These consist of: Safety, Budget, Function, Right of Way, Aesthetics and Schedule. The PCDOT studied several alignment alternatives and recommends alignment alternative E to the CAC. The CAC believes other alignment alternatives are preferable to alternative E.

The CAC would like to discuss each alternative beginning with their recommended Alternative, Alternative B.

I. ALTERNATIVE B:

Alternative B requires the purchase of the property on the west side of La Cholla Boulevard. This alternative is the recommended option of the CAC. In support of their recommendation, the CAC believes the benefits of Alternative B include, but are not limited to:

1. Alternative B would allow the travel lanes, bike paths and sidewalks to remain consistent with the widening project of La Cholla Boulevard north, River Road to Omar Road. The width of the travel lanes would be consistent with safe and efficient flow of traffic, bicycles and pedestrians as proven by the existing design of other road improvement projects including the La Cholla widening project, River to Omar Roads; Wetmore Road widening project between Fairview and Romero Roads, the Wetmore/Ruthrauff widening project between Romero and La Cholla Boulevard.
2. Alternative B allows for improved safety precautions to be designed into the intersection of La Cholla Boulevard and Ruthrauff Roads for children walking to

nearby elementary and junior high schools, including marked pedestrian crossings and additional room between the existing roadway and the pedestrian sidewalks.

3. Alternative B allows for public transportation bus pull-outs, permitting more efficient vehicular travel.
4. Alternative B allows for additional crosswalks to public transportation bus stops for both existing north and southbound bus stops. Right now, pedestrians are running across traffic to catch their bus. A Hawk Light should be installed for safe crossing of pedestrians at the bus stops.
5. Alternative B eliminates the right of way issues for homes on both sides of La Cholla Boulevard. The CAC recommends moving the proposed roadway to the west after the acquisition of property. This would allow additional room for a two way street on the east side of La Cholla Boulevard for homeowners to access their property.
6. The CAC suggests in addition to Alternative B, a 10 foot masonry wall sound barrier be installed on the west side of La Cholla Boulevard for noise abatement.
7. The CAC would suggest in addition to Alternative B, a 10 foot masonry wall sound barrier be installed on the east side of La Cholla Boulevard for noise abatement.
8. Alternative B would allow for a two way side street to the east of the 10 foot masonry wall sound barrier on the east side of La Cholla Boulevard. This would allow a safe, convenient right of way for residents on the east side of La Cholla from all directions. This design would be consistent with the design of the River Road widening project from La Canada to Oracle Road on the south side of the road.
9. Alternative B allows more room for improved aesthetics such as landscaping and artwork so as to comply with Federal law which mandates that part of the budget be used for artwork and landscaping.
10. Alternative B allows more room for wider, safer sidewalks and pedestrian pathways for children walking to the nearby junior high and elementary schools, bus stops and businesses in the area.

11. This alternative would allow the County to own the property surrounding the existing well located on the west side of La Cholla Boulevard, allowing safe efficient access to the well for maintenance and upkeep.
12. By reducing the likelihood of potential lawsuits for inverse condemnation, this alternative would allow the efficient scheduling of the project.
13. This alternative substantially reduces the liability potential to Pima County for roadway design defects.

The CAC recommends Alternative B as it requires less acquisition of properties, would affect fewer households and businesses at a greatly reduced cost over Alternative A.

II. ALTERNATIVE A:

Alternative A requires the purchase of the properties on the east side of La Cholla. This alternative has basically the same benefits as listed in Section I above. However, this alternative is considerably more expensive budget wise, and includes the acquisition of a greater number of residential and business properties, displacing significantly more people than Alternative B.

III. ALTERNATIVE E:

This CAC finds alternative E to be unacceptable for a number of reasons. Although this alternative is the least expensive, the CAC contends that it does not meet any of the other criteria necessary for approval of the project: safety, function, right of way, aesthetics and schedule. Allow us to explain.

1. By using the existing right of way, the DOT proposes to significantly reduce the width of the travel lanes, bike paths and sidewalks, thereby constricting the safe and efficient flow of travel for trucks, public transportation, school buses, vehicles, bicycles and pedestrians.
2. Of immediate concern for safety, reducing the width of travel lanes decreases the margin of error for traffic to avoid accidents and could compound its devastation by possibly ending in someone's front yard or worse, their house.

3. The bike path will also be narrowed suffering the fate of once again reducing the margin of error.
4. By not engineering out safety hazards, Pima County leaves itself open for potential lawsuits for negligent design of the roadway, bike path and pedestrian walkways.
5. The right of way to the houses situated on either side of La Cholla Boulevard is unsafe and unacceptable. Homeowners on either side La Cholla will be forced to make unsafe u-turns on a busy street in order to enter the "one way" side streets to access their property.
6. No U-turns are permitted at River Road & La Cholla. The CAC believes that conditions will exist requiring same at Curtis & La Cholla and Ruthrauff & La Cholla. This will require homeowners to take extensive alternative routes in order to access the one-way right of way to their homes.
7. This proposal will leave homeowners on either side of La Cholla Boulevard with issues regarding parking and right of way for any guests or family who come to visit them at their homes. The proposed 11 foot one way road does not leave room for any curbside parking.
8. Further, the proposed 11 foot one way road along the homes on either side of La Cholla Boulevard does not allow room enough for larger trucks and vehicles to turn into the resident driveways.
9. The suggestion to use the utility easements for access to homes is not allowed. Utility easements are not to be used for ingress and egress.
10. The noise level to the surrounding neighborhood, and especially the houses on the street itself, will be unacceptable. The noise abatement efforts suggested will be insufficient to properly address the livability within the households and their adjacent yards.
11. Aesthetics: with alternative E, there are none. It could be viewed that since this is considered a lower income neighborhood, that the same provisions provided to the neighborhoods up north of River Road, perceived to be of a higher income, were not considered, i.e., sound barriers, etc. Further, it does not meet the criteria for Federal

funding which mandates a portion of the funding be used for artwork and landscaping.

In addition to its other shortcomings, Alternative E would impose adverse effects upon the houses situated on either side of La Cholla Boulevard and the surrounding neighborhoods. The inhabitability created by the road noise, air quality, access and safety issues, would greatly increase potential for lawsuit and could delay the schedule by years, substantially increasing the budget for this project due to increased legal fees and inflation of construction costs.

In short, it seems that all the criteria, with the exception of budget, were not considered by the RTA and PCDOT. The CAC does not approve this alternative.

The CAC recommends that if appropriate funding cannot be made available to institute Alternative B for this project, that the project be tabled until such time that the necessary funds can be obtained. The CAC recommends that the project be done correctly, with all aspects of the design criteria being considered. The CAC believes this project should be given the same consideration as other improvement projects in the neighborhoods on the north side of La Cholla.

Once again, we would like to thank the Regional Transportation Authority and Pima County for the opportunity to assist in this project.

Sincerely,

Paul W. Bass

Annette Grimm

Andy [Signature]

Wayne A. Metz

Norma E. Metz

Ellie [Signature]



PIMA COUNTY
DEPARTMENT OF TRANSPORTATION
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January 24, 2008

La Cholla Boulevard, River Road to Ruthrauff Road
Community Advisory Committee
Attention: Bonny L. Bass
145 South Sixth Avenue
Tucson, AZ 85701-2007

Subject: Your Letter Dated January 15, 2008, Regarding La Cholla Boulevard: Ruthrauff Road to River Road

Dear Community Advisory Committee Members:

We received your letter dated January 15, 2008, expressing your views on the roadway alignment design for La Cholla Boulevard. This project is one of the County's key Capital Improvement Projects in the first quarter of the Regional Transportation Authority (RTA) plan and we appreciate the Community Advisory Committee's (CAC) concern to "get it done right." We have communicated your concerns to County Administration and to Supervisor Sharon Bronson, Pima County Board of Supervisors. We would like to take this opportunity to clarify several important issues that may impact on how the CAC views the proposed alignment. Below is a clarification of the issues that relate to safety features of the roadway, aesthetics, utilities, access, and general right-of-way issues:

Roadway Features

Crosswalks: Marked pedestrian crossings are proposed for the Ruthrauff Road intersection.

Travel Lanes: The proposed width of travel lanes meets the national standards provided by the American Association of State Highway and Transportation Officials (AASHTO) and is not considered unsafe or substandard.

Sidewalk: The proposed sidewalk width meets Pima County standards.

Bike Lanes: Currently, bicyclists ride on the two-lane pavement or they ride in the dirt right-of-way where drivers turn in and out randomly. A proposed 5' paved shoulder that bicyclists can use will improve safety.

Bus Stops: Bus stops will be provided for SunTran.

HAWK Light: The use of HAWK lights for pedestrian crossings are carefully reviewed by the County Traffic Division. Specific criteria must be met to justify a HAWK light otherwise the HAWK light can turn into a liability rather than an asset. Currently, a HAWK light would not be justified at Jay Avenue. After the La Cholla Boulevard improvements are built the situation can be reviewed again to see if a HAWK light is warranted.

Aesthetics

Public Art: By Pima County policy, one percent of the planning, design and construction costs of arterial roadway projects should be designated for public art. Vicki Scuri of SiteWorks has been selected as the project artist. This selection process was managed by Tucson Pima Arts Council. Information about the artist can be found at www.vickiscuri-siteworks.com.

Landscape: Pima County provides appropriate roadway landscape in medians and parkways. Roadway landscape must take into consideration issues such as site distances and underground and above ground utilities. McGann and Associates has been selected as the landscape architect (www.mcgannland.com/).

Utilities

Well Site: The well site on the west side of La Cholla Boulevard is owned by the City of Tucson. Pima County does not have a need to acquire property adjacent to the City's well.

Access

Access and U-turns: Arterial roadways in Pima County frequently have raised medians which greatly improves safety. Drivers generally are able to adjust their travel patterns and/or utilize u-turns to access certain streets, businesses, and residences. This is necessary to provide safe access management.

Parking: Typically, Pima County does not provide parking on arterial roadways. Since La Cholla Boulevard was designated an arterial roadway prior to 1960 and prior to the development of the properties fronting La Cholla Boulevard, property owners should have been aware that it would be important to provide adequate on-site parking.

Frontage Road: The proposed frontage road is 16' wide, not 11' wide as stated in the letter. Examples of frontage roads in the community include Swan north of Speedway, Wilmot at Julia, and Broadway at Melville. Frontage roads provide managed access to properties fronting busy roadways.

Alley: Pima County will maintain access to all lots fronting La Cholla Boulevard. In your letter, the alley west of La Cholla Boulevard was described as a utility easement only; however, it is also available for ingress and egress and as such provides alternative access to properties.

Properties Adjacent to La Cholla

Accidents: The CAC has raised concerns that traffic on La Cholla Boulevard may create accidents that end up on private property. La Cholla Boulevard was designated as an arterial roadway prior to 1960 and before any residences were built north of Ruthrauff Road. The developer that built and the subsequent people that acquired property fronting La Cholla Boulevard did so with the knowledge that La Cholla Boulevard was designated a wide arterial roadway intended to carry large volumes of traffic. This urbanization is found in many parts of Pima County and the City of Tucson and does not pose unreasonable threats to safety.

Noise: Noise is a factor related to urbanization. Prior to 1960 and prior to development of properties adjacent to the road, La Cholla Boulevard was designated an arterial roadway and the adjacent properties were designated Multi-Use zoning. Noise walls are not effective when driveways are spaced closely and are better suited for residential zoning with large lots rather than for multi-use zoning areas with closely spaced lots.

Property Value: Roadway improvements generally help improve the value of adjacent properties. La Cholla Boulevard improvements will reduce traffic congestion, increase intersection capacity at the Ruthrauff Road intersection, improve capacity of the bridge, provide sidewalks for pedestrians, provide paved shoulders for bicyclists, provide landscape and public art for aesthetics, provide ADA access to bus stops, and provide access management. These capacity, safety, and aesthetic improvements generally help property values.

I hope this explanation helps to clarify the issues the community may have about the La Cholla Boulevard project. Many of these issues revolve around the fact that La Cholla Boulevard was designated an arterial roadway before adjacent properties were developed. This places the burden on property owners to correctly develop and use their properties for an urbanizing environment. The proposed alignment that includes one-way frontage roads for approximately 1000' for the properties north of Ruthrauff Road meets all the needs and many of the wants of Pima County residents. Other alternatives requiring right-of-way acquisition would unnecessarily increase the cost of the project, cause disruption to property owners and increases the schedule for the project. In your letter you have asked the County to build the road "correctly." Based on the information here, I trust you will have confidence that the improved La Cholla Boulevard will be built to National and County standards for a safe and efficient roadway. If you have any questions, please contact our Community Relations representative Carol Brichta at 740-6410, or the Project Manager Dean Papajohn at 740-6471.

Sincerely,



Priscilla S. Cornelio, P.E.
Director

PSC:DP:sap

c: Dean Papajohn, Project Manager, Engineering Division
Carol Brichta, Community Relations

January 31, 2008

HAND-DELIVERED

Priscilla S. Cornelio, P.E.
Director
Pima County Department of Transportation
201 N. Stone Avenue, Fourth Floor
Tucson, Arizona 85701

**Re: La Cholla Boulevard: Ruthrauff Road to River Road
Community Advisory Committee**

Dear Priscilla:

Thank you for your letter dated January 24, 2008. We appreciate that you forwarded our comments to the Board of Supervisors.

Your response addressed several concerns presented in our January 15th letter recommending that the Regional Transportation Authority (RTA) and the Pima County Department of Transportation (PCDOT) use Alternative B for the roadway design. You did not specify in your letter which Alternative you were addressing; however, the Committee assumes you were defending Alternative E – the only Alternative which the RTA and PCDOT propose. All other Alternatives have been suppressed. With this in mind, allow us to discuss the points contained in your January 24th letter.

Roadway Features

1. Crosswalks:

Despite the fact that marked pedestrian crossings are proposed for the Ruthrauff/La Cholla intersection, the Committee continues to be concerned for the safety of school children crossing that intersection. There are two schools, Centennial Elementary School and Flowing Wells Junior High School south of the Ruthrauff/La Cholla intersection. This school crossing traffic increases the use of the crosswalk dramatically and the Committee believes the safety of these children and adults crossing that intersection is paramount. Extra precautions should be taken.

2. Travel Lanes:

Although the 11 foot travel lanes may meet the national standards provided by the American Association of State Highway and Transportation Officials, it does not meet Pima County standards according to the *Pima County Roadway Design Manual* for this size of urban roadway. See chapter 2, p. 2-32, figure 2-10 and table 2-1, p. 2-6. Further, the Committee maintains this roadway project should be consistent with the La Cholla Road widening project, River to Omar Road.

3. Sidewalks:

The Committee agrees that the sidewalks should meet PCDOT standards which is six feet.

4. Bike Lanes:

Although bicyclists do ride on the two-lane paved road or on the dirt shoulder of La Cholla Boulevard, the Committee believes that since bike lanes are part of the improvement plan, they should be built consistent with the ones installed north of River Road in the La Cholla Road to Omar Road widening project. Further, according to the *Pima County Roadway Design Manual*:

“On curbed roadways, six feet are to be added to the typical width of outside travel lanes to accommodate bicycles.” *Pima County Roadway Design Manual*, chapter 2, p. 2-15.

5. Bus Stops:

As presented to the Committee, Alternative E does not show bus stops long the corridor from Ruthrauff to River Road. Simply stating that bus stops will be provided is not sufficient for the Committee. The Committee believes, consistent with advertisements in support of the RTA election, pullout bus stops should be designed into roadway improvement projects. Pullout bus stops allow for the efficient flow of traffic and increased safety for bus passengers.

6. HAWK Light:

Installation of a HAWK light may require additional studies. The Committee suggests that a marked pedestrian crossing should be provided at Jay Avenue.

Aesthetics

7. Public Art:

With Alternative E, there will essentially be no place for public art on the south end of the improvement project. This includes no room for art installed at bus stops or on sound abatement walls. Will all of the public art be installed at the north end of the project?

8. Landscape:

As pointed out in point 7 above, with Alternative E, there is no room for landscaping.

Essentially, the aesthetics contained in Alternative E are nonexistent and bodes unfavorably towards the "perceived" lower income neighborhood.

Utilities

9. Well site:

The Committee understands that the well site located on the west side of La Cholla Boulevard is owned and operated by the City of Tucson. However, when periodic maintenance occurs, large trucks carrying pipes and equipment park at the well site. These vehicles would be forced to park on the one way frontage road to perform maintenance on the well, thereby blocking the roadway and denying access to homes and businesses on the frontage road.

Access

10. Access and U-turns:

The Committee agrees that raised medians greatly improve safety. However, it appears that you did not get the point the Committee was making regarding U-turns. If you review the diagram of Alternative E, you will see that there is a break in the median at Jay Avenue for turns. However, the entrance to the one way frontage road for residents on the west side of La Cholla Boulevard is north of Jay Avenue. Therefore, residents coming from the south would have to make a U-turn somewhere else. Currently U-turns are prohibited at the River Road/La Cholla intersection. The Committee is assuming that no U-turns will be permitted at Curtis and La Cholla, since southbound traffic will be coming off a bridge.

Priscilla S. Cornelio, P.E.
Director
Pima County Department of Transportation
January 31, 2008
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Alternative E severely limits the access to properties on the west side of La Cholla Boulevard by forcing these residents to either find alternative access or by negotiating potentially dangerous U-turns. In doing so, Pima County is creating a unique subgroup for this neighborhood.

11. Parking:

It is true that several of the residents whose homes face La Cholla Boulevard have been living there since the homes were built, perhaps as early as 1960. However, these residents should not have been expected at that time to look into their crystal ball to see what the roadway design plans would be 40 years down the road. In 1960 and most assuredly prior to those homes being built along La Cholla Boulevard, Pima County did have engineers and planners who knew or should have known of the roadway design plans for the future. If indeed Pima County intended in 1960 to build an arterial roadway on La Cholla Boulevard, Pima County should have taken that into consideration prior to allowing these homes to be built.

12. Frontage Road:

Once again, the Committee fails to understand why frontage roads built many years ago in Tucson such as the ones mentioned in your letter have anything to do with the frontage road proposed in Alternative E. The frontage roads at Swan north of Speedway, Wilmot at Julia and Broadway at Melville all have entrances after turning off the main street onto a side street. Further, these frontage roads are wider than 16 feet and allow two-way traffic.

In contrast, the entrance to the proposed frontage road on the west side of La Cholla Boulevard is not off a side street such as Calle Narciso, it is off La Cholla Boulevard. There is no proposed turn lane or "slow down" lane for traffic entering this frontage road. Therefore, the Committee assumes that 45 mile per hour traffic must quickly slow in order to negotiate the right turn entrance onto the frontage road. This could potentially put Pima County at risk for lawsuits arising out of rear end type accidents for a faulty road design. The Committee believes this is a faulty road design.

13. Alley

Up to this time the alley has not been used as regular ingress or egress by the residents and most likely could not be considered a road. Coupled with the fact that the alley is dirt, residents who utilize it for said reason would be in violation of Pima County

Priscilla S. Cornelio, P.E.
Director
Pima County Department of Transportation
January 31, 2008
Page 5

Code 17.16.090, A-E concerning air quality control. In order for the alley to be used for ingress and egress, it must be paved and/or dust abatement must occur. In addition, all drainage issues must be resolved. The Committee believes Pima County would be responsible for this mitigation.

Properties Adjacent to La Cholla

14. Accidents:

Once again, Pima County cannot seriously believe that residents who purchased their homes in 1960 would have the foresight to know that La Cholla Boulevard was designated as an arterial roadway. Further, the same argument applies to Pima County in allowing such residences to be built despite the fact that they did know that La Cholla Boulevard was designated as an arterial roadway. It simply stands to reason that increasing the traffic flow, decreasing the lane width and not reducing the speed limit will cause increased accidents. The statement made to the Committee that a six inch curb will eliminate vehicles from coming onto private property is not sufficient. The increased danger and negligent design will open Pima County to lawsuits.

15. Noise:

Although the Committee has asked repeatedly for the noise studies and the traffic flow studies, we have not been provided with any information other than they are in draft form and not available. The Committee believes that the noise levels of the roadway currently are above the levels allowed according to the *Pima County Roadway Design Manual*, chapter 1, Appendix 1-A-9, §7.1(1)(c).

The Committee understands that noise walls are not effective when driveways are spaced closely which is why the Committee recommends the taking of those residential lots along the west side of La Cholla Boulevard. This would allow for noise retention walls to be built to protect the other homes in the neighborhood which will be also effected by the noise level.

16. Property Value:

The CAC Committee does not concern itself with property values. The CAC Committee concerns itself with building the road correctly and consistent with the La Cholla to Omar Road widening project.

Priscilla S. Cornelio, P.E.
Director
Pima County Department of Transportation
January 31, 2008
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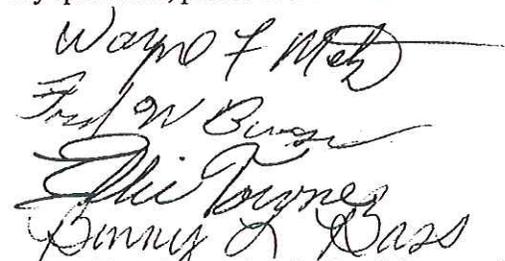
A strong argument can be built against Pima County when it maintains that:

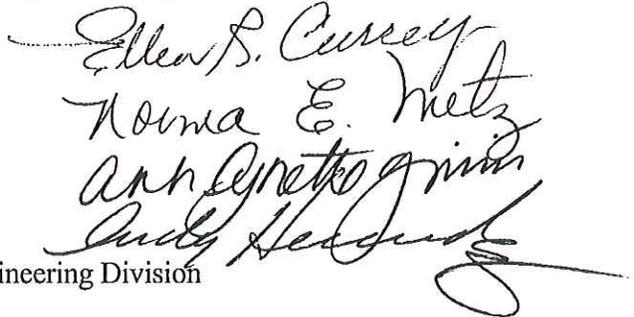
“La Cholla Boulevard was designated an arterial roadway **before** adjacent properties were developed. This places the burden on property owners to correctly develop and use their properties for an urbanizing environment.” [emphasis added].

The Committee, which is not solely made up of those particular residents, is offended by this statement. Once again, the Committee believes the onus is on Pima County who did have the information and knowledge that this was an arterial roadway. Perhaps Pima County should not have allowed these homes and developments to be built. The fact remains that they were built, people live and work there and the proposed roadway design suggested in Alternative E does not consider how it would adversely effect the homes and businesses in this neighborhood.

It is clear to the Committee, especially after receipt of your letter, that Pima County’s only criteria in proposing Alternative E is the cost of the project. Alternative E would impose adverse effects upon the homes situated on either side of La Cholla Boulevard and the surrounding neighborhood. The inhabitability created by the road noise, air quality, access and safety issues would increase the potential for lawsuits thereby delaying, possibly for years, the project and increasing substantially the budget for the project due to legal fees and inflation of construction costs. Further, the proposed road in Alternative E does not substantially meet Pima County standards as set out in the *Pima County Roadway Design Manual* and is not consistent with other roadway improvement projects in the surrounding area.

The Committee thanks you again for your time in reviewing our concerns. If you have any questions, please feel free to contact Frederick Bass at 407-3767.





c: Dean Papajohn, Project Manager, Engineering Division
Carol Brichta, Community Relations



PIMA COUNTY
DEPARTMENT OF TRANSPORTATION
201 NORTH STONE AVENUE, FOURTH FLOOR
TUCSON, ARIZONA 85701-1207



PRISCILLA S. CORNELIO, P. E.
DIRECTOR

(520) 740-6410
FAX (520) 740-6437

February 11, 2008

La Cholla Boulevard, River Road to Ruthrauff Road
Community Advisory Committee
Attention: Frederick Bass
145 South Sixth Avenue
Tucson, AZ 85701-2007

Subject: Your Letter Dated January 31, 2008, Regarding La Cholla Boulevard: Ruthrauff Road to River Road

Dear Community Advisory Committee Members:

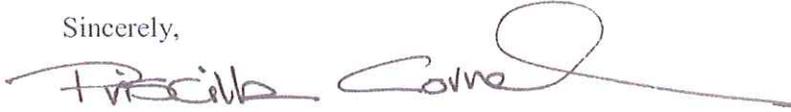
We received your letter dated January 31, 2008, expressing your views on the roadway alignment design for La Cholla Boulevard. It appears to me that we share many of the same goals for the project, such as improved mobility for motorized vehicles, facilities for pedestrians and bicyclists, efficient and safe intersections, access to bus transportation, a wider bridge, collection of storm runoff, and the integration of landscape and public art. The additional concerns your committee has raised with regards to noise, accessibility, parking, etc., are the concerns that our design team has been investigating even before the first Community Advisory Committee (CAC) meeting. Each time the CAC has provided input our design team has dug deeper and deeper to identify and evaluate possible solutions. Unfortunately, at this point, it appears that the County is proposing solutions that differ from the property acquisition proposal the CAC has provided.

As a public project there are certain procedures that must be followed in the design of La Cholla Boulevard. The next steps in the process include holding an open house to allow the community to learn more about the project and share their feedback. Two technical documents must be completed. The first is called the Design Concept Report or DCR. The second is the Environmental Assessment and Mitigation Report or EAMR. Drafts of each of these reports will be discussed at future CAC meetings. Each of these reports will have sections on Public Involvement which will clearly convey the CAC's concerns and the CAC's request for an increased budget for property acquisition. It is the intent of Pima County Department of Transportation to work with the CAC through all of these steps and continue to address issues and concerns as we are able. Ultimately, the EAMR is presented to the Board of Supervisors with a letter from the CAC supporting or not supporting the Pima County Department of Transportation's recommendations.

La Cholla Boulevard, River Road to Ruthrauff Road Community Advisory Committee
Your Letter Dated January 31, 2008, Regarding La Cholla Boulevard: Ruthrauff Rd to River Rd
February 11, 2008
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As you can see, there is still much work to be done on the La Cholla project and Pima County Department of Transportation is committed to continuing to work with the CAC. We appreciate the commitment the CAC has already given to the project and look forward to continuing to work with you in the months ahead. In the meantime, if you have any questions or concerns, please contact the project manager Dean Papajohn at (740-6471).

Sincerely,

A handwritten signature in black ink, appearing to read "Priscilla Cornelio". The signature is fluid and cursive, with a large loop at the end of the last name.

Priscilla S. Cornelio, P.E.
Director

PSC:DP:sap

c: Dean Papajohn, Project Manager, Engineering Division
Carol Brichta, Community Relations



Appendix C

Stage I (15%) Construction Plans

(Bound Separately)



Appendix D

Right-of-Way Requirements Plans

(Bound Separately)