



MEMORANDUM

Date: November 6, 2012

To: The Honorable Chairman and Members
Pima County Board of Supervisors

From: C.H. Huckelberry
County Administrator

A handwritten signature in black ink, appearing to be "CH", is written over the printed name "C.H. Huckelberry".

Re: **River Park Enhancement and Expansion Initiative**

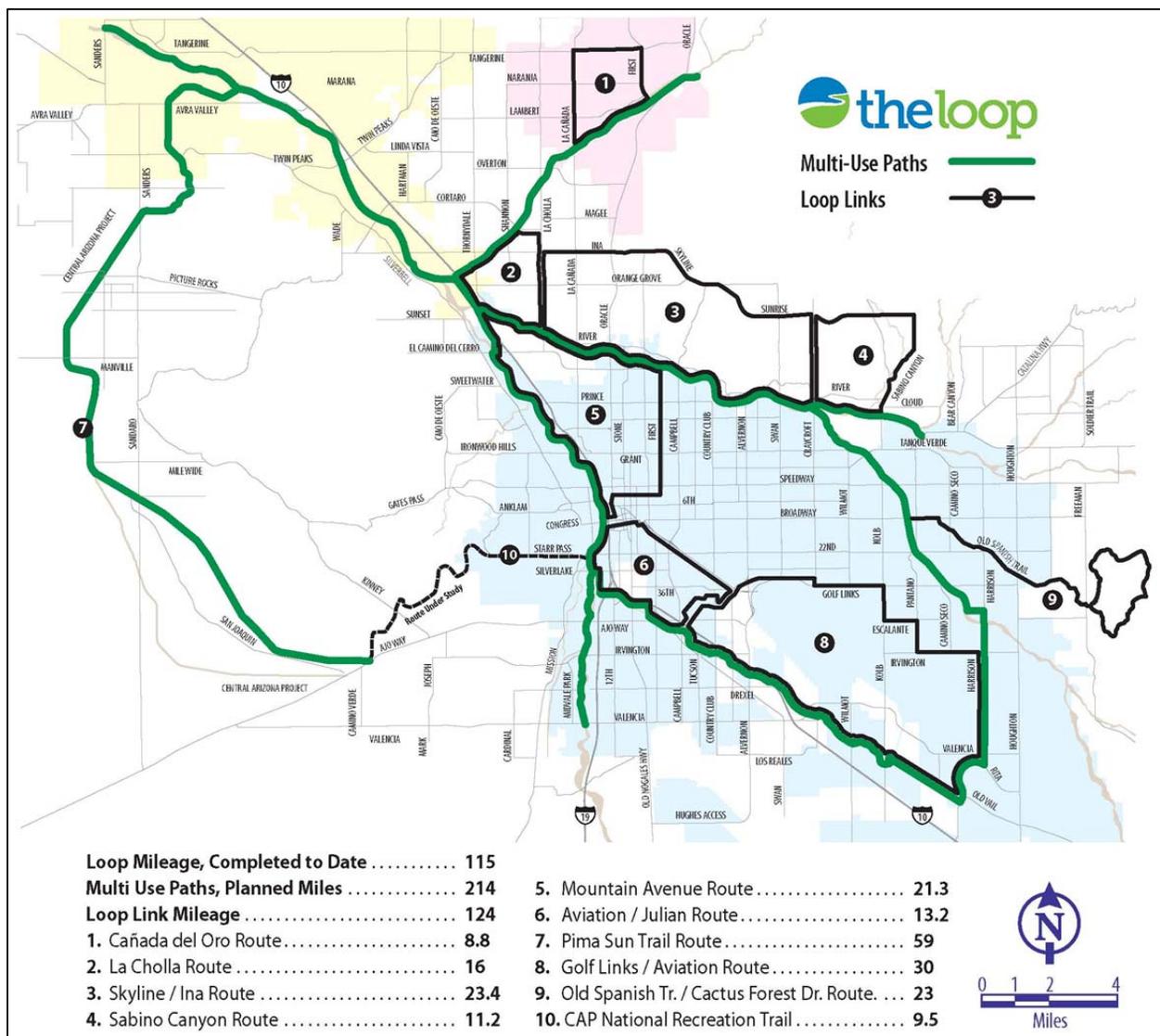
TABLE OF CONTENTS

I.	BACKGROUND	1
II.	INITIATIVE PROCESS	3
III.	RIVER PARK STANDARDS	5
	A. Paths	6
	B. Landscaping	6
	C. Irrigation with Reclaimed Water	7
	D. Neighborhood Connections	7
	E. Nodal Parks	8
	F. Underpasses	9
	G. Bridges	11
IV.	COSTS TO BRING TO PREMIUM STANDARD	12
V.	MAINTENANCE	13
VI.	MARKETING PLAN	14
	A. Bike Tourism in Pima County	14
	B. Branding The Loop	14
VII.	CENTRAL ARIZONA PROJECT NATIONAL RECREATION TRAIL	15
VIII.	SUMMARY	16
IX.	RECOMMENDATIONS	17

I. BACKGROUND

The Loop will soon be a reality with the addition of 26 miles of new path in the past three years. Another 12 miles of paths are currently under construction. As The Loop comes together and the final link is connected, the true value of this community asset can be realized. The Loop System, with the addition of the various tributary river parks or Loop Extensions; the Canada del Oro, Tanque Verde and northern and southern reaches of the Santa Cruz, has grown to 214 miles in length; 115 miles are complete to date. This

includes both paved and unpaved paths. The Loop is one of the longest multiuse pathway systems in the Country and a key economic driver for our region. The pathways not only link the various jurisdictions, neighborhoods and people, they also enable opportunities to highlight and enhance those areas we consider special or important to our community. These areas include cultural and historic resources, parks and sports facilities, even shopping and entertainment venues. The Loop traverses the entire region and connects to many of the area’s community treasures.



The Loop Multiuse Paths and Links Map

The Loop provides the region with the alternate modes transportation infrastructure similar to the interstate highway system. The Loop Links provide an additional 124 miles of alternate modes transportation opportunities by linking to on-street bike lanes. The City of Tucson proposes the development of bike boulevards as the arterial bikeway system. When connected to The Loop, these boulevards provide a low-stress alternative to the less-seasoned bike commuter and encourage more ridership. Bike lanes provide the tertiary system and are most frequently used by the experienced riders. Pima County continues to advance The Loop Links with the addition of 8 routes strategically located throughout the region. As The Loop expands, additional Loop Links will be created.

The purpose of the River Park Enhancement and Expansion Initiative is to confirm the river park development standard for The Loop, create a database of existing conditions, identify opportunities for enhancement and/or expansion to meet the current standard, and develop a portfolio of projects to bring The Loop up to standard.

The Loop was built over the past 30-plus years, resulting in a variety of park standards. Today, The Loop in the future will be built with 16-foot paved paths, a minimum of 8-foot decomposed granite paths, landscaping with irrigation, and other amenities as budget and space allow. To maintain a consistent quality of experience, some of the earliest segments of The Loop need enhancement.

The initiative contains the following enhancement categories: Landscaping, Irrigation with Reclaimed Water, Pathways, Neighborhood Connections/Nodal Park Development, Bridges, and Park-oriented Development.

II. INITIATIVE PROCESS

A team of technical experts was assembled from various County departments for this effort. Assignments were divided into three phases: inventory, analysis, and recommendations. The two primary deliverables for this effort include a matrix of existing conditions by reach and a specific GIS layer of the existing conditions. Project sheets with maps are created from the GIS database. The PWA SharePoint site was established to store all associated data collected and will continue as a repository of information related to The Loop.

The Initiative Team developed thousands of pages of data and summarized them in 64 individual project sheets tied to a legend map of the entire Loop system (Attachment A). These project sheets provide an overview of existing conditions in each reach. A matrix was created by reach, and it itemizes all items inventoried for this exercise: condition, size, costs to bring to new standard and other key information. This large document is located on our PWA SharePoint website (<http://sharepoint.pima.gov/sites/pwa>) and access can be granted upon request.

Key to developing recommendations for enhancement was the creation of design guidelines or minimum standards for each infrastructure element or enhancement category. The high quality areas of The Loop were used as the model for many of the standards, such as landscaping, paths and park development.



Premium Prototype Area

The Loop's greatest potential as an economic driver comes from the way the community perceives and uses this asset. As part of the analysis effort, the Team evaluated dozens of similar bikeways and river parks to determine key elements or common themes that drove successful development. Although many of the sites had common components to The Loop and most have reaped some economic benefit, two specific communities appear to have seen significant economic advantage from their infrastructure investment. These communities are the Atlanta Beltline in Georgia and the Bayshore Bikeway in San Diego. Some key commonalities include:

The Honorable Chairman and Members, Pima County Board of Supervisors
Re: **River Park Enhancement and Expansion Initiative**
November 6, 2012
Page 5

- Unifying vision
- Public/private partnership
- Sustainable Asset (includes economic, environmental, social equity elements)
- Focus on green space
- Community support
- Business sponsorship

The following are links to the Atlanta and San Diego documents:

http://www.sandag.org/uploads/projectid/projectid_63_5152.pdf
[http://beltline.org/progress/planning/master-planning/.](http://beltline.org/progress/planning/master-planning/)

A primary deliverable from this effort is the identification of economic development opportunities along The Loop. The report entitled, "Pima County's New Main Street: The Loop," is Attachment B. Highlights of the report include the following recommendations:

- Consideration of a Loop Overlay Zone that will standardize land uses and adjacent property development across jurisdictions.
- Consideration of a mobile vendor policy on both private and public lands adjacent to The Loop.
- Formation of a business coalition to support The Loop and adjacent businesses.
- Development of various Loop maps, including electronic versions or applications, for commuters to employment centers and one for tourists to sports and tourism venues.
- Target hotels and motels along The Loop for tourism use, bike rentals and information dissemination.

The Loop provides the region with an unparalleled opportunity to create a cohesive sense of place. There are numerous points of interest throughout The Loop. These points of interest are enhanced further by connectivity with transit, retail and residential neighborhoods. The Initiative Team identified areas of opportunity for further delineation and/or improvements. This information is provided on maps and in narrative format. Areas owned by Pima County or in the unincorporated area were the focus of the assessment.

III. RIVER PARK STANDARDS

Prior to initiating this analysis, a rating scale or standard to measure the existing conditions was established. A grading scale was then created to differentiate quality. In some cases,

grading was binary. Pass/fail or yes/no was used to establish the existing condition, such as whether a reach was irrigated with reclaimed water. This methodology was applied to the assessment of the entire river park system. For purposes of design priority, the asphalt path is the primary improvement, followed by the decomposed granite path, and then landscaping. The following information provides a brief summary of the standards for each focus area:

A. Paths

Early reaches of the river park system were built with 8-foot width asphalt paths. The divided urban path (DUP) became the river park standard in 1981 with the acceptance of the Rillito River Park Master Plan. The DUP specifies 12 feet of asphalt for bikes and users preferring a smooth surface and 6 to 8 feet of decomposed granite for horses and those who prefer a soft path. As The Loop becomes more popular and utilization increases, user groups such as walkers and bicyclists are forced to coexist with less room to move. To alleviate the continuing space crunch, the asphalt path standard was increased to 16 feet. For purposes of design priority, the asphalt path is primary, followed by the decomposed granite path, and then landscaping.

B. Landscaping

The Loop traverses a dozen different biozones or ecosystems along its course. From creosote flats along the Julian Wash and portions of the Harrison Greenway to the mesquite bosques and cottonwood/willow habitats along the Santa Cruz and Rillito Rivers, the landscape palette is as diverse as the adjacent surroundings. The Loop provides a variety of experiences for its users: from urban space to open space, cultural sites to sports parks, and historic places to entertainment venues. The landscape standard needed flexibility to allow for this diversity. The main element of the landscape standard is the tree canopy. Anyone who spends any time in the desert knows the value of shade. Long stretches of asphalt pavement without shade appeal to a very small subsection of Loop users: the veteran or commuter bicyclists. Even then, bicyclists appreciate the occasional rest area with a tree canopy or shade structure. Therefore, trees dominate the proposed plant palette. The rating scale used to determine landscape quality is Attachment C. Areas with significant mature trees, such as those along the Rillito River, were used as the sample standard or basis for the premium landscape designation. The density of vegetation along the Rillito and portions of the Santa Cruz is the result of requirements by the U.S. Army Corps of Engineers (USACE) or other funding agencies as mitigation. In these restoration areas, the tree canopy and associated vegetation take priority over any new infrastructure, such as widening existing or adding new paths or new irrigation that would result in removal of mature tree species.

C. Irrigation with Reclaimed Water

Reclaimed water lines run through much of the metropolitan region. There are still opportunities for Pima County to connect developed and undeveloped sections of the Santa Cruz River Park, Pantano Wash River Park and Julian Wash River Park to reclaimed water. Tucson Water has reclaimed water lines near these parks, in many cases less than one mile away, that still have capacity for further expansion (see Attachment D).

During peak usage, potable water from Tucson Water can cost an estimated \$1,200 per acre-foot. Under Pima County's wheeling agreement with the City of Tucson, the cost is \$60 per acre-foot using output from Pima County's Randolph Water Reclamation Facility. Additional reclaimed water can be provided at the environmental rate of approximately \$300 per acre-foot. A golf course uses an average 700 acre-feet of reclaimed water per year per 18 holes; river parks use an average 12-16 acre-feet per mile per year for both banks. Tucson Water requires Pima County to pay for expansion of its reclaimed water system; however, there are opportunities to share these infrastructure costs with other users, such as nearby schools and businesses.

As The Loop continues to expand and further development occurs at the El Corazón on the Santa Cruz River Park, reclaimed water needs may be supplied directly by the new Water Campus.

Pima County has a reclaimed water allocation of approximately 3,600 acre-feet per year from the metropolitan wastewater reclamation facilities. In 2011, Pima County used 1,271 acre-feet of its allocation for direct reuse and 418 acre-feet for aquifer replenishment at the High Plains Constructed Recharge project, leaving 1,912 acre-feet of the County's reclaimed water allocation. The County chose to use the remaining reclaimed water allocation on the County/City Southeast Houghton Area Recharge Project.

D. Neighborhood Connections

Accessibility to The Loop is vital to utilization. Neighborhoods, commercial/retail centers, and businesses require close, convenient access to maximize The Loop as an alternate transportation system. For purposes of this Initiative, access points were targeted at one-quarter mile increments. Access points can be either a trailhead or a neighborhood connection. A trailhead is a parking lot for trail users with an improved access to The Loop. A neighborhood connection does not have a parking lot, but it is a trail that accesses The Loop from a subdivision or a public road. The total number of existing trailheads is 22; proposed new trailheads are 3. Existing access points are 111 and proposed new access points are 21. The inventory of existing connections includes everything from a private backyard gate into The Loop to a large multi-car parking area with numerous amenities.

The improvements available at both trailheads and neighborhood connections vary significantly. Trailhead improvements range from a basic design, which includes a parking lot and a paved entrance to The Loop, to those with significant enhancements such as restrooms, drinking fountains, seating, ramadas, picnic tables, signage, art and landscaping.

Neighborhood connection designs range from a basic access point with a dirt path to additional enhancements, including signage, lighting, hardscape and landscaping, a trash can, bicycle rack, seat wall, and a drinking fountain.



Neighborhood Connection

E. Nodal Parks

There are 33 existing parks along The Loop or within a short distance of The Loop. These parks vary from sports fields to BMX parks to memorial or contemplative parks. Most are larger than a few acres in size. A nodal park is a smaller area that can provide a spot to rest or be a scenic viewpoint for the river park user. A few features typical to the nodal park include enhanced landscaping, a bench and artwork. These nodal parks make a nice

entrance to The Loop and a wonderful asset for the adjacent neighborhood. The photo below is one of the typical nodal parks along The Loop.

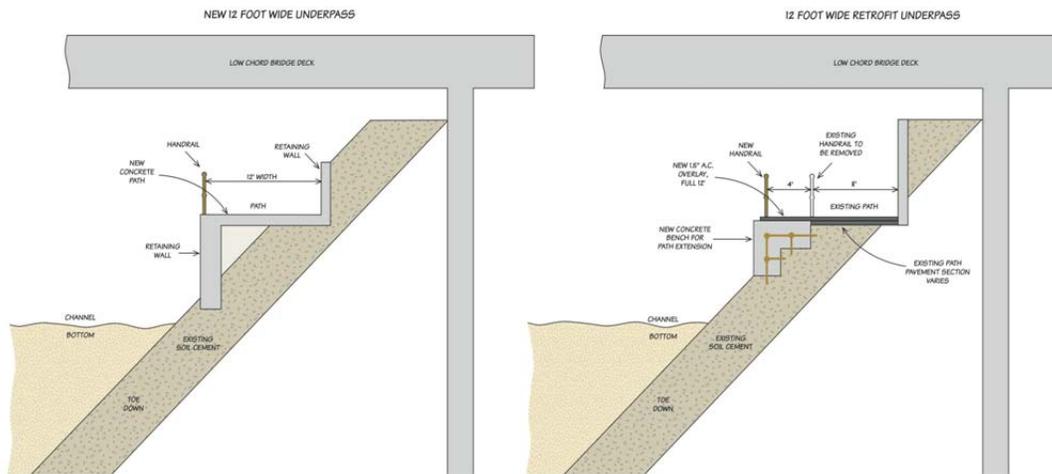


Nodal Park

F. Underpasses

Central to The Loop's appeal is that users are separated from cars. The Loop crosses 22 roadways throughout the region. Because users occupy both banks of the river parks, in most cases, two underpasses are needed at each crossing. The majority of these underpasses is in place or will be completed in the next two years. However, many of the existing underpasses are undersized to meet the current Loop volumes. Of the 49 existing underpasses, 30 (or 61 percent) do not meet current travel requirements. The Initiative Team proposed a modification that expands the travel area while minimizing impact to the floodway. This retrofit can be done at a fraction of the cost of installing a new, wider underpass. A cross-section of the proposed retrofit is shown below. The average cost of the retrofit is \$200,000 per underpass.

UNDERPASS SCENARIOS- NEW PATH AND RETROFIT



Underpass retrofit under construction.

Of the 13 remaining at-grade crossings, two will be constructed by the Arizona Department of Transportation with the construction of its adjacent interchanges (Sunset Road and Ajo Way). Three have minimal vehicular traffic and do not warrant an underpass or will be unnecessary with the completion of the final Loop alignment (Orange Grove and Interstate 10 Frontage, Wilmot Road at the Julian Wash, and Dodge Boulevard at the Rillito River). Two run parallel to a road with no opportunity for an underpass (Harrison Greenway at Irvington and Valencia). The remaining six at-grade crossings have been included in the enhancement cost estimates, but they are not programmed in the current Five-Year Capital Improvement Program:

1. Ina Road at the Santa Cruz River (to be built by the Town of Marana)
2. Camino del Cerro at the Santa Cruz River
3. Valencia Road at the Santa Cruz River (if The Loop is extended to San Xavier Mission)
4. Rita Road at Union Pacific Railroad along the Julian Wash
5. Ajo Way at Julian Wash
6. Pantano Wash at Broadway Boulevard

The Rita Road and Ajo Way at-grade crossings require overpasses to cross the railroad tracks. These alternatives are not included in the cost estimates for these segments.

G. Bridges

The same team assessed bridge conditions along The Loop system. There are 59 bridges total; 15 (or 25 percent) do not meet current requirements. Bridge width is the primary reason. Bridges are costly to fabricate and require significant lead time. The Initiative Team proposed two separate bridges for opposing travel directions. This allows all of the existing bridges to be retained, most in their existing locations, while the second bridge is installed nearby. The cost of the additional or relocated bridges is included in the cost estimates for each reach.



Existing two-bridge installation at Mountain Avenue and the Rillito River.

IV. COSTS TO BRING TO PREMIUM STANDARD

Much of The Loop is complete or under construction. Development of The Loop occurred over decades, resulting in a wide variety of quality and standards. As part of this exercise, each reach of The Loop was assessed against the proposed standards. Deficiencies were identified and a cost estimate provided to bring each reach of The Loop up to optimal condition. Each of the Initiative Team subcommittees provided recommendations based on conditions existing today. Changes to any of the elements (e.g., path, landscaping, and irrigation) alter the existing condition; thereby affecting the original recommendation. For example, if a well-vegetated reach can accommodate a 16-foot paved path and is recommended but requires the removal of mature landscaping, the landscape recommendation for the area may change from doing nothing to enhancing existing tree canopy. Therefore, the cost estimates for each reach is based on priority (pavement, decomposed granite path, and finally, landscaping). The Initiative Team identified those areas where pavement can be added with minimal impacts to existing improvements (tree removal, bench/ramada relocation, irrigation relocation, etc.) If the impact is minimal, the pavement enhancement is included in the cost estimate. In areas that can accommodate additional pavement width, but at the cost of significant tree removal, additional analysis is

required to determine if the trees are part of a mitigation requirement. If the trees were planted to meet a federal (USACE, Clean Water Act Section 404 or other permit), additional effort may be required to determine the replacement value and whether it can be met within the remaining right of way.

The total cost to improve The Loop to optimal or prime condition is \$84 million. The following information is a breakdown by reach.

Table 1. Loop Improvement Costs.

Loop Reach	Improvement Cost
Santa Cruz River	\$37,704,012
Rillito River	5,918,500
Pantano River	22,119,500
Julian Wash/Harrison Greenway	4,533,000
Canada del Oro Wash	8,730,500
Tanque Verde Wash*	4,712,078
Total	\$83,717,590

*The Initiative Team analyzed both the north and south banks of the Tanque Verde Wash. Total costs for the south bank portion of the 3.5-mile reach are recorded on the project maps. A major component of those costs is right of way. For purposes of this exercise, only the north bank is included in the above cost estimate.

An additional \$27.8 million in funding from a variety of sources remains to be applied to various active or future projects along The Loop, including a major bank control installation and riparian restoration project at the confluence of the Santa Cruz River and Julian Wash. The balance of funding needed to optimize The Loop will be provided by a variety of funding sources in the future.

V. MAINTENANCE

Many segments of The Loop received a lower grade because of issues that can be addressed simply by increasing maintenance. Items such as faulty irrigation, dead trees and shrubs, cracked pavement, and excess sand or gravel overtaking portions of the pavement can all be addressed with additional maintenance resources. Consistency of appearance is also essential. The Loop lies mostly within the City of Tucson; however, Pima County has historically provided maintenance for approximately 50 percent of The Loop. Many of those sections not maintained by Pima County are experiencing an even greater degree of decline. Pima County is currently awaiting approval from the City of Tucson to authorize the County to provide maintenance for all portions of The Loop under City control. Development of a consistent, region-wide maintenance standard for The Loop is essential

to encourage greater use. The average per-mile cost of maintenance is \$60,000 annually. This includes both banks of the river parks. Moreover, as with all major assets, maintenance can extend the life of the asset and reduce long-term capital costs associated with major restoration/improvement on portions that are not adequately maintained.

VI. MARKETING PLAN

A. Bike Tourism in Pima County

Biking is big business in our region. The following information is based on unpublished preliminary findings from the Pima Association of Governments (PAG)-commissioned study by The University of Arizona Eller College of Management: economic impact study on bicycling and bicycle-related industries in the Tucson-Eastern Pima County Region. "Events" includes, but is not limited to, El Tour de Tucson, Tucson Bicycle Classic, 24 Hours in the Old Pueblo Mountain Bike Race, and ABA-BMX Southwest Nationals at the Tucson Convention Center.

Table 2. Bike-related Tourism Revenue.

Attraction	Annual Revenue
Races, Rides, Events and Tours	\$22,662,200
Donations to El Tour Charities	\$1,474,210
Guide Services	\$9,555,000
Hotel	\$5,287,485
Restaurant	\$936,720
Total	\$39,915,615

B. Branding The Loop

The majority of recent studies, including the analysis done by the League of American Bicyclists in reviewing the region for Platinum designation, felt there was additional need for safe, car-free biking alternatives. The Loop exemplifies the best of these alternatives. The Loop has become very popular almost exclusively based on word of mouth. However, there is even greater potential to get the word out both locally and nationally, even internationally through a consistent, well-organized approach.

The Loop should be branded and marketed consistently throughout the year, across several dimensions: education, promotion, and events. By design, these dimensions will overlap to create consistent messaging and promotion of The Loop to Pima County residents and visitors. Branding should include:

- Consistent name and logo usage, established by the Visual Identity Manual, Communications Office.
- Webpage development with interactive or trip planning map features and links to tourism-related activities and sites, Communications Office.
- Public outreach through news releases, social media, Loop webpage, Communications Office.
- Promotional items.

VII. CENTRAL ARIZONA PROJECT NATIONAL RECREATION TRAIL

Although not included in this analysis, Pima County recently secured agreements with federal and state agencies to establish a 42-mile recreational trail on top of the levee or berm that runs along the Central Arizona Project (CAP) canal from the Pinal County line east of Interstate 10 to the Tucson Mountains and ultimately to West Valencia Road. Once connected to the western reach of The Loop, the CAP Trail will be 59 miles long. The CAP Trail will be used by pedestrians, bicyclists and equestrians, and will connect to The Loop at Avra Valley Road and along the southern portion of the Santa Cruz River Park. Trailheads and key connections are still to be determined. In addition to The Loop, the CAP Trail will connect to 12 or more trails in Tortolita Mountain Park, Saguaro National Park's West District, Tucson Mountain Park and Ironwood Forest National Monument.

The CAP Trail in Pima County was designated as a National Recreation Trail by the U.S. Secretary of the Interior in 2003. Since then, the cities of Scottsdale and Phoenix have developed trail systems along the canal, and Pinal County has included a CAP trail in its Trails and Open Space Master Plan so the designation has been extended to the entire 336 miles along the canal from Lake Havasu to Tucson. Pima County Natural Resources, Parks and Recreation Department has a web link to the December 2008 CAP Trail Master Plan at [http://www.pima.gov/nrpr/pdfs/CAP Trail Master Plan Dec2008.pdf](http://www.pima.gov/nrpr/pdfs/CAP%20Trail%20Master%20Plan%20Dec2008.pdf).

The CAP Trail will be Pima County's third Trail of National Significance; one that has been formally recognized under the 1986 National Trails System Act for historic, scenic or recreational importance. The CAP National Recreation Trail joins the Anza National Historic Trail and the Arizona National Scenic Trail as part of Pima County's regional trail system. Once complete, the CAP Trail will provide the region with over 59 miles of paved pathways and extend recreational and commuting opportunities to residents west of the Tucson Mountains.

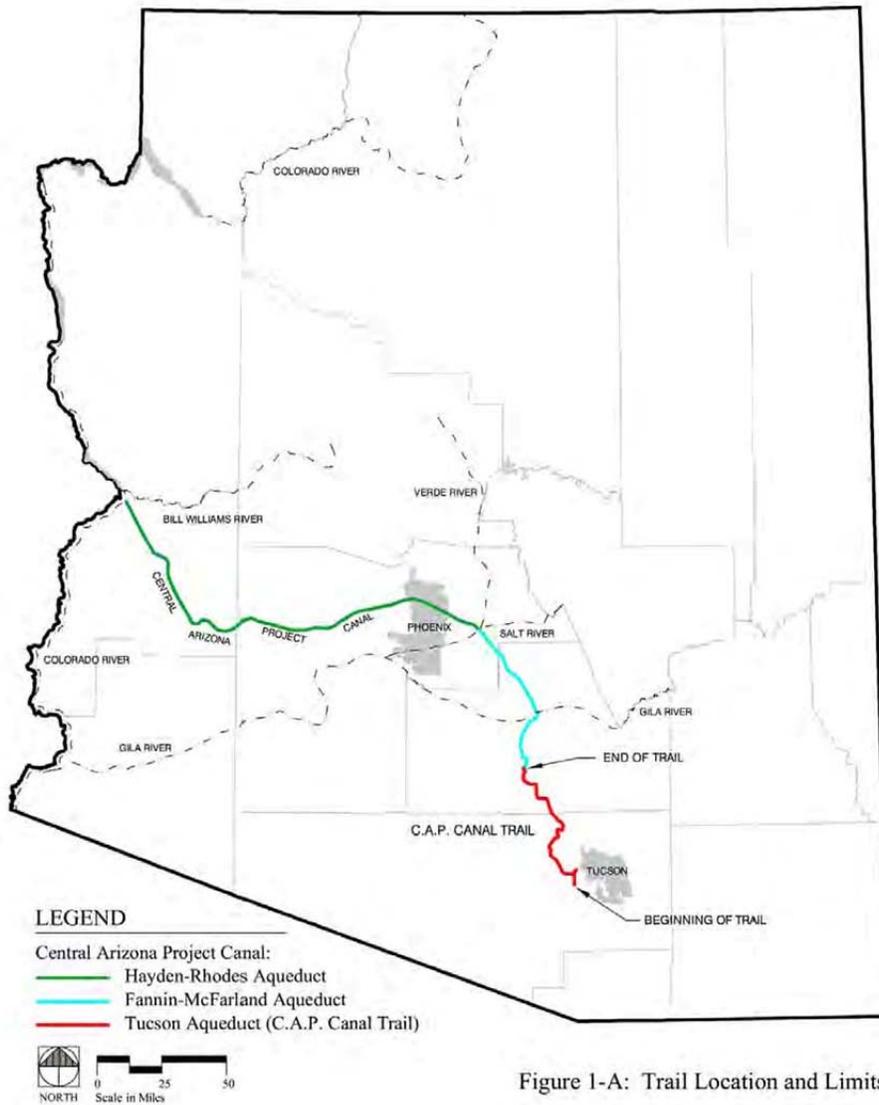


Figure 1-A: Trail Location and Limits

CIP Master Trail Plan Map

VIII. SUMMARY

The Loop is fast becoming the place to be for friends, neighbors, businesses, and visitors. Continued investment in The Loop is paying dividends in both economic development and community health. The work of the Initiative Team will form the basis for continued expansion and enhancement for years to come. Completion and enhancement of The Loop means future generations will continue to enjoy this incredible green space in the Sonoran Desert.

IX. RECOMMENDATIONS

1. Public Education Program

The Communications Office, with assistance from various departmental public information officers and staff (Transportation; Natural Resources, Parks and Recreation; and the Regional Flood Control District), should develop a public information plan that includes an input process. The purpose is to educate users and provide the County with information about the public's priorities regarding development of The Loop. Some options include:

- a. Working with special interest groups (e.g., homeowners associations, biking groups, Pima Trails Association, etc.) to develop support and gather community buy-in.
- b. Economic Development and Tourism. Develop areas of "regional significance" in marketing The Loop (example: sports venues, cultural venues, birding sites, etc.). Coordinate communications of these specialty areas with the Metropolitan Tucson Convention and Visitor's Bureau (MTCVB).
- c. Participate in all bike-related activities or major tourism events to establish a presence in the community.
- d. Develop a plan for bringing in adjacent merchants and commercial interests. Include cooperative communication strategies and opportunities for sponsorship.
- e. Web Page Maintenance. Provide regular updates on all activities related to The Loop and biking, hiking, and other trail-related activities in the region.

The Communications Office will submit the proposed plan with all the above elements, including timeframes and estimated costs, within the next 30 days.

2. Prototype Enhancement

One of the best ways to determine community support for bringing various portions of The Loop to optimal condition is to show them what the improved asset looks like. I recommend enhancing five or six prototypes of our Optimal Loop System, at a total cost of \$1.1 million, for the public to experience and provide input. These prototype areas are geographically located throughout the community to allow the broadest cross-section of users and potential users to comment. Only those portions of The Loop requiring minimal investment to bring them up to premium or optimal condition were selected. The map identifying the prototype sections is Attachment E. Existing condition photos are included on the map. The improvements cover path widening wherever possible and landscape and irrigation upgrades. New underpasses or bridges are not included at this time.

3. Specific Loop Maintenance Plan

Much of the differences between existing river park condition and optimal condition can be eliminated with increased maintenance; therefore, I recommend the Natural Resources, Parks and Recreation Department be directed to develop a Loop maintenance plan specific to each river park. The maintenance plan would provide the cost of maintaining the river park at the improved standard. The plan will be used to develop a Request for Proposals for providing the maintenance with outside landscape contractors. The lowest cost provider, outside consultant or internal labor, will be submitted to the Board for approval with the Fiscal Year 2013/14 Recommended Budget.

4. Economic Development Plan for The Loop

Sustainable economic development is a key element of a healthy community. To maintain the ongoing level of investment and support for these types of initiatives, the community expects a reasonable return on investment. The Loop's contribution to the community in terms of business retention/attraction, health improvement, and tourism has not been fully measured and quantified. I recommend Pima County join with The University of Arizona Eller College of Management, the MTCVB, and Tucson Regional Economic Opportunities, Inc. to prepare a comprehensive cost/benefit analysis, including opportunities to fund continuing investment in The Loop system from revenues specifically linked to this asset.

5. The Loop Business Attraction

The Loop provides a unique and unparalleled opportunity to share the Sonoran Desert and our region's rich cultural heritage with the world. Development adjacent to The Loop is key factor to creating a vibrant, active space that continues to attract users. Each jurisdiction has an opportunity to build upon the framework that is The Loop. This includes establishing key connections and routes that make accessing The Loop easier for users. Additionally, the region should consider a simplified and consistent approach to attract new businesses that wish to locate along The Loop or that cater to Loop users. This could be done through a special overlay zone or other development standards. I recommend the County work with the various jurisdictions to develop consistent standards for Loop-related development that fosters additional private investment adjacent to The Loop.

6. Integration

The Loop provides the region with alternate modes transportation infrastructure similar to the interstate highway system. The Loop controls access, separates user groups (cars from pedestrians and bicyclists), traverses the entire metropolitan area, and links with many of the region's largest employers. Like the interstate highway system, a secondary system such as the bike boulevards must be developed to allow broader access and regional coverage for users. From that, bike lanes provide the tertiary support. The Loop is the hub

of this transportation network, with bike boulevards forming the spokes or arterial system. I recommend Pima County fully support the development of the bike boulevard system when:

- a. The route links directly to The Loop.
- b. PAG provides the region with a list prioritizing the development of bike boulevards based on criteria specific to this transportation mode. The highest priority routes should be completed to allow for continuity and full use of this transportation system before beginning new routes.
- c. The Pima County Department of Transportation will work with PAG to develop additional data on the use of HAWK signals and phase timing to ensure the investment in these types of signals is warranted on bike boulevards.

Staff intends to proceed as outlined in this memorandum and will return to the Board from time to time with funding and contracting decisions. Staff will also provide all of the enclosed information to the Bicycle Advisory Committee, as well as the Parks and Recreation Commission, Planning and Zoning Commission and Flood Control District Advisory Committee for their discussion and input regarding The Loop and the system of non-motorized, car-free travel ways.

I have also asked staff to arrange a meeting with the Board of Health to discuss the individual and community wellness initiatives The Loop offers.

County staff is also making all of this information available to the various bicycle and cycling enthusiasts by placing it on our website and inviting them to review the detailed plans and diagrams associated with The Loop in this report in an interactive Geographic Information System (GIS) environment.

CHH/mjk

Attachments

- c: John Bernal, Deputy County Administrator for Public Works
Nanette Slusser, Assistant County Administrator for Public Works Policy
Suzanne Shields, Director, Regional Flood Control District
Priscilla Cornelio, Transportation Director
Rafael Payan, Director, Natural Resources, Parks and Recreation
Linda Mayo, Director, Office of Sustainability and Conservation
Christina Biggs, Manager, Real Property Services

ATTACHMENT A

West Bank

Santa Cruz River Park: Drexel to Irvington - Map S2

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	fair
Asphalt Cond. DG Path:	12' 8'
Connectivity:	1E TH
Bridge:	N/A
Underpass:	N/A

Comments:
Irrigation on potable maintained by COT but reclaimed available less than 1 mile away. A 12' paved path and an 8' soft path exist on most of the segment.

Enhancement Opportunities:
Fairly new landscape and irrigation installed under City of Tucson direction. Asphalt pathway to be widened and rehabilitated. Provide trail signage. Provide new neighborhood connections for Midvale Park. May require additional ROW.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$200,000.0
Trailhead/RR	\$0.0
Node	\$30,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$141,000.0
Total	\$371,000.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1 Mile



Existing Conditions:

Landscaping:	E
Irrigation:	poor
Asphalt Cond. DG Path:	12' 8'
Connectivity:	3E NC
Bridge:	N/A
Underpass:	N/A

Comments:
The river park has a 12' paved path and a 8' soft path. Major erosion along the edge of the DG path with exposed irrigation system failing in general. City of Tucson maintained section

Enhancement Opportunities: repair irrigation system and replace and enhance damaged landscaping. Asphalt path way to be widened and rehabilitated with new DG path alignment developed after erosion mitigation takes place by City of Tucson.

Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$300,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$3,000,000.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$750,000.0
Total	\$4,150,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)

- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

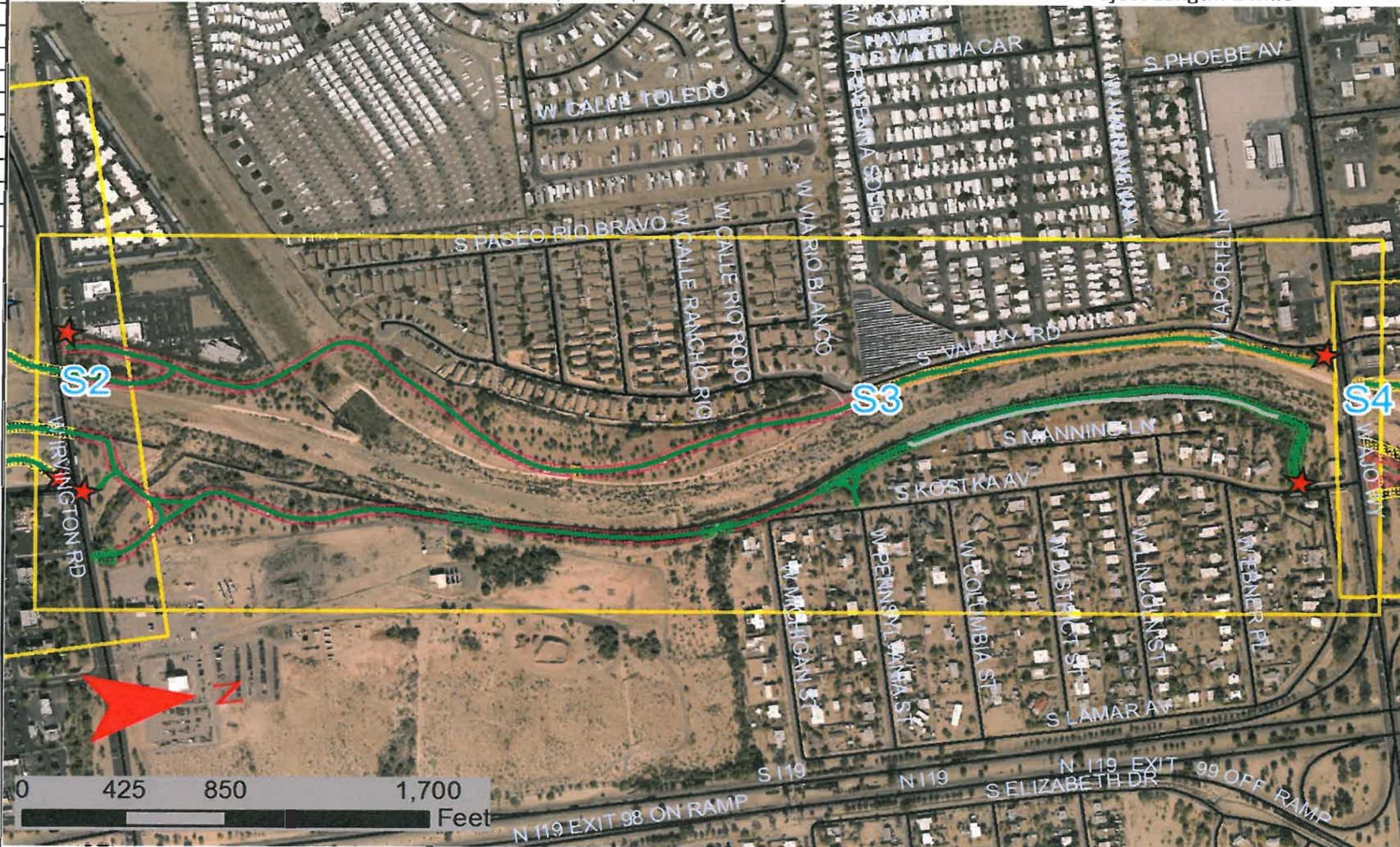
- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Santa Cruz River Park: Irvington to Ajo Way - Map S3

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1 Mile



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank	
Existing Conditions:	
Landscaping:	C & D
Irrigation:	fair
Asphalt Cond. DG Path:	10' not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	1 - pass
<p>Comments: Irrigation on potable, but reclaim is available at Silverlake. This segment has only a 10' paved path.</p> <p>Enhancement Opportunities: Replace irrigation system and enhance landscape. Asphalt path to be widened and rehabilitated. Provide neighborhood connection at Rancho Rio II subdivision. Provide trail signage.</p>	
Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$200,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$75,000.0
Total	\$390,000.0

East Bank	
Existing Conditions:	
Landscaping:	C & B
Irrigation:	fair
Asphalt Cond. DG Path:	10' 8' partial
Connectivity:	3E NC
Bridge:	2- pass
Underpass:	1 - failing
<p>Comments: Irrigation on potable, but reclaimed available at Silverlake. Segment has a 10' paved path and a partial 8' soft path on the northern half.</p> <p>Enhancement Opportunities: replace irrigation system and enhance landscape. Widen asphalt path and provide DG path where applicable. Irvington underpass to be widened to 12 feet. Provide trail signage.</p>	
Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$200,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$215,000.0
Total	\$715,000.0

West Bank

Santa Cruz River Park: Ajo Way to 29th Street - Map S4

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	not installed
Asphalt Cond. DG Path:	not installed
Connectivity:	not installed
Bridge:	N/A
Underpass:	1 - new

Comments:
Construction start expected 4/13 as part of Paseo de las Iglesias project.

Enhancement Opportunities:
New irrigation system, landscape, pathways, and trailhead to be installed via Paseo de las Iglesias project. Provide new underpass at Ajo Way.

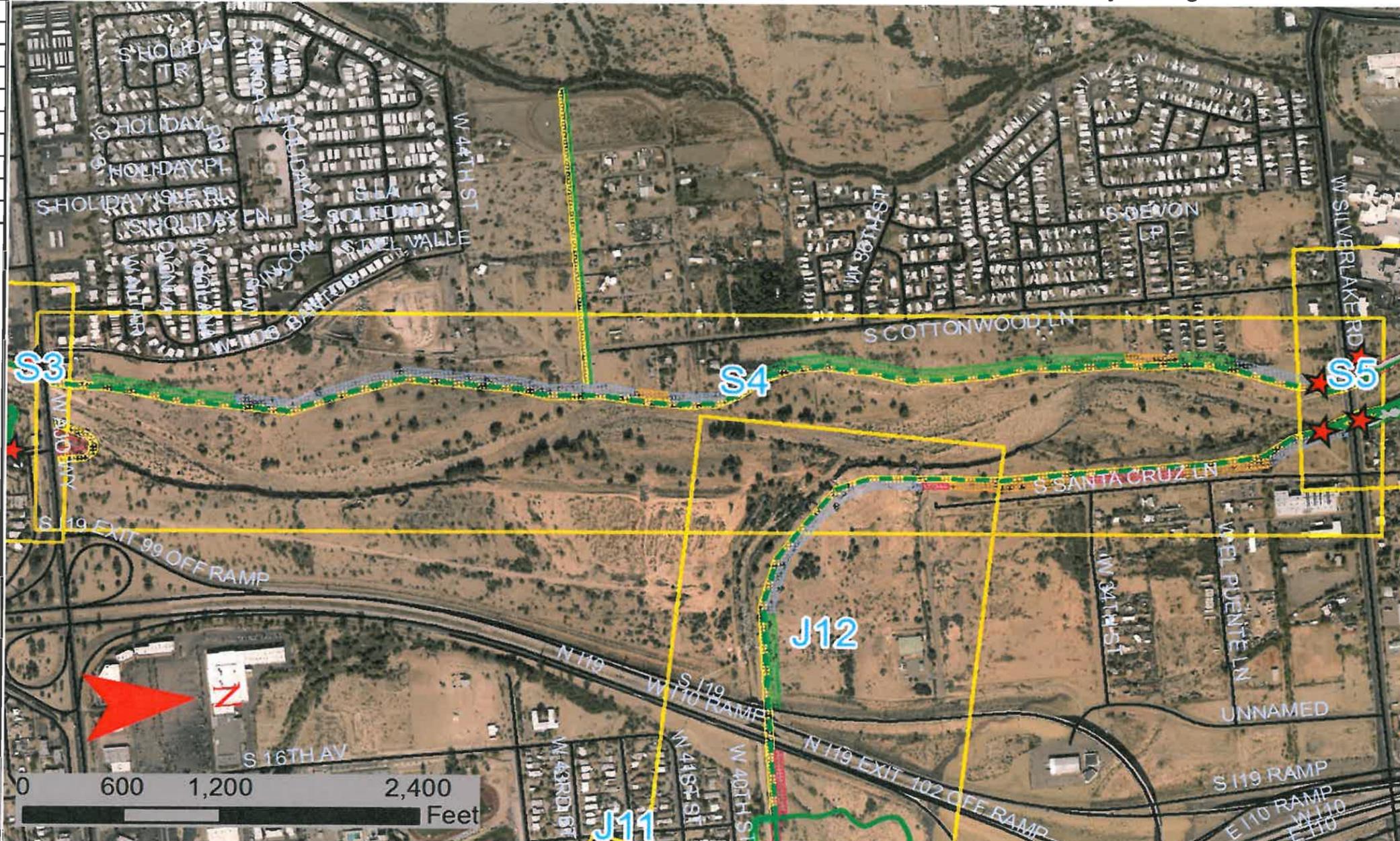
Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$125,000.0
Total	\$425,000.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.5 Miles



Existing Conditions:

Landscaping:	E
Irrigation:	not installed
Asphalt Cond. DG Path:	not installed
Connectivity:	not installed
Bridge:	1 - new
Underpass:	1 - new

Comments:
Construction start expected 4/13 as part of Paseo de las Iglesias project.

Enhancement Opportunities:
New irrigation system and landscape to be installed via Paseo de las Iglesias project. Provide bridge over Julian Wash and pathway connections to Ajo Way. Provide new underpass at Ajo Way.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$300,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$262,500.0
Underpass	\$300,000.0
Other	\$260,000.0
Total	\$1,122,500.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

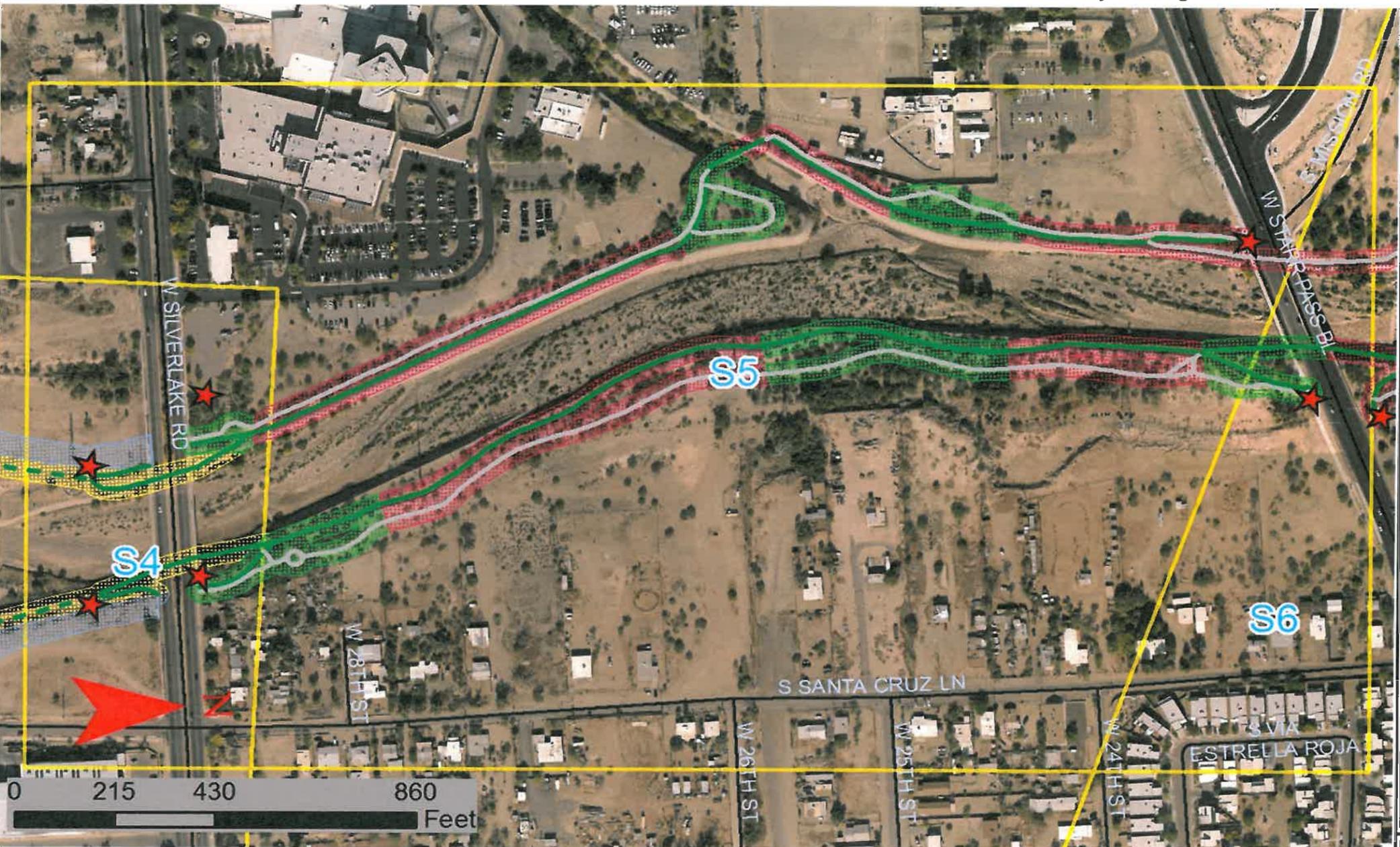
Santa Cruz River Park: 29th Street to 22nd Street - Map S5

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 0.5 Miles

West Bank	
Existing Conditions:	
Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path	12' 8'
Connectivity:	2E NC
Bridge:	1 - fail
Underpass:	1 - pass
Comments: Irrigation on potable, but reclaim available at Silverlake Rd. A 12' paved path and an 8' soft path make up this segment.	
Enhancement Opportunities: replace irrigation system and enhance landscape. Widen and rehabilitate pathways. Replace bridge over West Branch confluence. Provide trail signage.	



East Bank	
Existing Conditions:	
Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path	12' 8'
Connectivity:	2E NC
Bridge:	N/A
Underpass:	1 - pass
Comments: Irrigation on potable, but reclaim available at Silverlake. Segment has a 12' paved, and 8' soft path that is separated south and next to the paved path north. Old landfill in some areas of this section that is settling.	
Enhancement Opportunities: replace irrigation system and enhance landscape. Widen and rehabilitate pathways. Provide trail signage. Drainage improvements and address settling issue associated with landfill.	

Estimated Cost:	
Landscape	\$25,000.0
Irrigation	\$25,000.0
Asphalt (& DG)	\$100,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$210,000.0
Underpass	\$0.0
Other	\$153,000.0
Total	\$513,000.0

Map Legend:

Street	Existing Trailhead
Map Sheet Reference	Proposed Trailhead
Open Pathway (Asphalt)	Existing Neighborhood Connection
Open Pathway (Decomposed Granite)	Proposed Neighborhood Connection
Conceptual Pathway (Asphalt)	Access Pathway
Planned Pathway (Asphalt)	

Abbreviations:

	A - Premium, 100% to 81% Tree Canopy
	B - High, 51% to 80% Tree Canopy
	C - Medium, 21% to 50% Tree Canopy
	D - Low, 0% to 20% Tree Canopy
	E - Undeveloped, in Planning or Construction

Estimated Cost:	
Landscape	\$25,000.0
Irrigation	\$25,000.0
Asphalt (& DG)	\$100,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$50,000.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$60,000.0
Total	\$260,000.0

Santa Cruz River Park: 22nd Street to Congress - Map S6

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.25 Miles

West Bank

Existing Conditions:	
Landscaping:	C, small B & D
Irrigation:	poor
Asphalt Cond. DG Path:	12' not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	2 - pass

Comments: This segment includes sites for proposed Mission San Augustín Garden and Tucson Origins Heritage Park. Potable - Mission Lane to 22nd. COT reclaim meter at Alameda with controller at Garden of Gethsamanes. Reclaim line runs from St. Mary's - Mission Lane. No DG path installed.

Enhancement Opportunities: replace irrigation and enhance landscape. Widen and rehabilitate asphalt pathway and install DG path where applicable. Repair connection to Mission Road and provide connection to Rio Nuevo projects. Provide trail signage.

Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$75,000.0
Asphalt (& DG)	\$250,000.0
Trailhead/RR	\$0.0
Node	\$30,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$113,000.0
Total	\$518,000.0

East Bank

Existing Conditions:	
Landscaping:	C
Irrigation:	poor
Asphalt Cond. DG Path:	12' not installed
Connectivity:	2E NC 1E TH
Bridge:	1 - pass 1 - fail
Underpass:	2 - pass

Comments: Potable - Mission Lane to 22nd COT reclaim fed from Cushing St. with meter on south edge of new bridge to Mission Lane. No DG path installed

Enhancement Opportunities: replace irrigation system and enhance landscape. widen and rehabilitate asphalt path and install new DG path where applicable. Repair parking lot of 22nd Street trailhead. Replace wood decking of Old Julian Wash Bridge. Provide trail signage.

Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$75,000.0
Asphalt (& DG)	\$250,000.0
Trailhead/ RR	\$25,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$50,750.0
Underpass	\$0.0
Other	\$130,000.0
Total	\$580,750.0



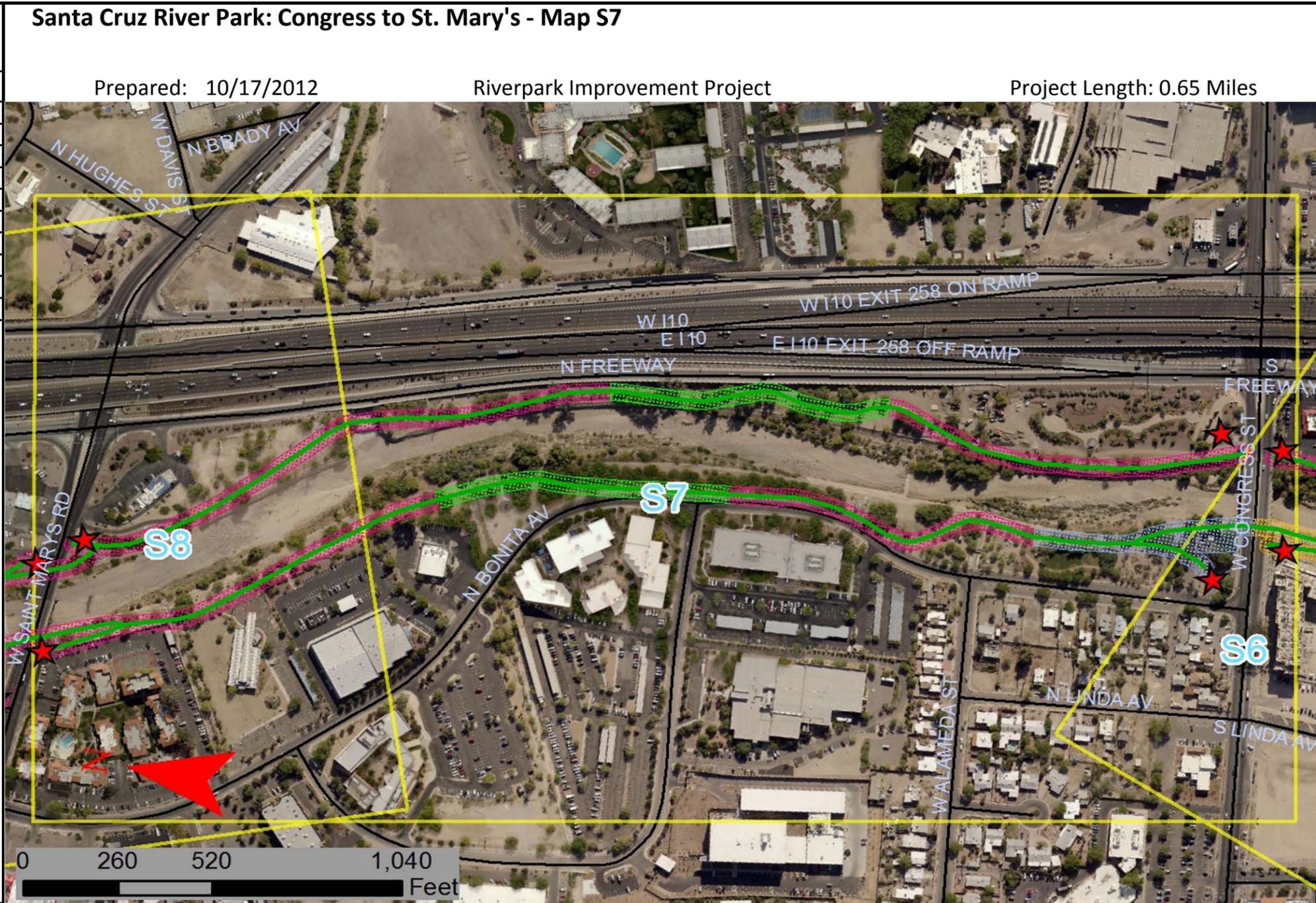
Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank	
Existing Conditions:	
Landscaping:	Small A; C & B
Irrigation:	poor
Asphalt Cond.	12'
DG Path	not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	failing
<p>Comments: At Congress, well established park with turf, amenities, mature canopy. Segment features a 12' paved pathway only. COT reclaim meter at Alameda with controller at Garden. Reclaim line runs from St. Mary's - Mission Lane.</p>	
<p>Enhancement Opportunities: replace irrigation system and enhance landscape. Widen and rehabilitate asphalt path and install DG path where applicable. Widen existing Congress Street underpass. Provide trail signage.</p>	



Estimated Cost:	
Landscape	\$30,000.0
Irrigation	\$35,000.0
Asphalt (& DG)	\$130,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$130,000.0
	\$525,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank	
Existing Conditions:	
Landscaping:	C & B
Irrigation:	poor
Asphalt Cond.	12'
DG Path	not installed
Connectivity:	2E NC
Bridge:	1 - fail
Underpass:	1 - new
<p>Comments: Segment features a 12' paved pathway only. COT reclaim fed from Cushing St. with meter on south edge of new bridge</p>	
<p>Enhancement Opportunities: replace irrigation system and enhance landscape. widen and rehabilitate asphalt pathway and install DG path where applicable. Replace bridge at Arroyo Chico Wash. Provide new underpass at Congress Street. Provide trail signage.</p>	

Estimated Cost:	
Landscape	\$30,000.0
Irrigation	\$35,000.0
Asphalt (& DG)	\$130,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$175,000.0
Underpass	\$300,000.0
Other	\$230,000.0
	\$900,000.0

Santa Cruz River Park: St. Mary's to Speedway - Map S8

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 0.5 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank

Existing Conditions:

Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path:	12' not installed
Connectivity:	5E NC 1E TH
Bridge:	N/A
Underpass:	1 - fail

Comments: Existing park with parking, turf and amenities. This side features only the 12' paved path. COT reclaim meter at Alameda with controller at Garden. Reclaim line runs from St. Mary's - Mission Lane.

Enhancement Opportunities: replace irrigation system and enhance landscape. widen and rehabilitate asphalt path and install DG path where applicable. Repave existing parking lot and rehabilitate existing neighborhood connections. Widen existing St. Mary's underpass and resolve utility conflicts.

Estimated Cost:

Landscape	\$25,000.0
Irrigation	\$25,000.0
Asphalt (& DG)	\$100,000.0
Trailhead/RR	\$20,000.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$126,000.0
Total	\$511,000.0

East Bank

Existing Conditions:

Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path:	12' not installed
Connectivity:	3E NC
Bridge:	N/A
Underpass:	1 - fail

Comments: This side features only the 12' paved path. COT reclaim fed from Cushing St. with meter on south edge of new bridge

Enhancement Opportunities: replace irrigation system and enhance landscape. Widen and rehabilitate asphalt pathway and install DG path where applicable. Widen St. Mary's underpass. repair existing artwork and parking lot north of commercial property. Provide trail signage.

Estimated Cost:

Landscape	\$25,000.0
Irrigation	\$25,000.0
Asphalt (& DG)	\$100,000.0
Trailhead/ RR	\$50,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$131,000.0
Total	\$531,000.0

West Bank

Santa Cruz River Park: St. Speedway to Grant - Map S9

East Bank

Existing Conditions:

Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path	12' 8' sporadically
Connectivity:	4E NC 1E TH
Bridge:	1 - pass
Underpass:	1 - fail

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.4 Miles

Comments:
The paved path is generally the only path in this segment, but there are pieces of unpaved and paved 8' pathway. Reclaimed water - 2 Meters supply Grant to Speedway.

Enhancement Opportunities:
replace irrigation system and enhance landscape. widen and rehabilitate asphalt path and install DG path where applicable. repave existing trailhead parking lot. Widen Speedway Boulevard underpass. Provide trail signage.

Estimated Cost:

Landscape	\$60,000.0
Irrigation	\$80,000.0
Asphalt (& DG)	\$280,000.0
Trailhead/RR	\$20,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$214,000.0
Total	\$854,000.0



Existing Conditions:

Landscaping:	B & C
Irrigation:	poor
Asphalt Cond. DG Path	12' not installed
Connectivity:	1E NC
Bridge:	2 - pass
Underpass:	1 - pass

Comments: This segment has only 12' paved path. Reclaimed water - 2 Meters supply Grant to Speedway.

Enhancement Opportunities:
replace irrigation system and enhance landscape. Widen and rehabilitate asphalt path and install DG path where applicable. Provide neighborhood connection to Grant Road Business Park. Provide trail signage.

Estimated Cost:

Landscape	\$60,000.0
Irrigation	\$80,000.0
Asphalt (& DG)	\$280,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$148,000.0
Total	\$583,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank

Existing Conditions:

Landscaping:	E
Irrigation:	fair -none
Asphalt Cond.	12'
DG Path	not installed
Connectivity:	2E NC 1E TH
Bridge:	1 - pass
Underpass:	1 - fail

Comments: This segment features the paved path only with limited landscape. Connection to Juhan Park already in place.

Enhancement Opportunities: install new irrigation system and enhance landscape. Widen asphalt path and install DG path where applicable. Widen existing Grant Road underpass. Provide trail signage.

Estimated Cost:

Landscape	\$100,000.0
Irrigation	\$200,000.0
Asphalt (& DG)	\$600,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$438,000.0
Total	\$1,538,000.0

Santa Cruz River Park: Grant Road to Sweetwater - Map S10

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 3 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	E, small C
Irrigation:	None
Asphalt Cond.	not installed
DG Path	not installed
Connectivity:	not installed
Bridge:	N/A
Underpass:	1 - fail

Comments: pathway project under development to install 16 foot asphalt pathway, bridge structure and connection to Sweetwater Drive.

Enhancement Opportunities: install new trailhead at La Cholla Boulevard with restroom. Widen existing Grant Road underpass.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$250,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$200,000.0
Total	\$650,000.0

West Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	12' not installed
Connectivity:	2E TH
Bridge:	3 - pass
Underpass:	N/A

Comments: Included in El Corazón de los Tres Río del Norte future Recreation District. Only existing amenity is the 12' paved path. Connections to Christopher Columbus Park exist at north and south ends.

Enhancement Opportunities: install new irrigation system and landscape. Widen asphalt path and install DG path where applicable. Install ramada at north Christopher Columbus Park Trailhead.

Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$75,000.0
Asphalt (& DG)	\$250,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$86,000.0
Total	\$461,000.0

Santa Cruz River Park: Sweetwater to Camino del Cerro- Map S11

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.25 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed not installed
Connectivity:	not installed
Bridge:	N/A
Underpass:	N/A

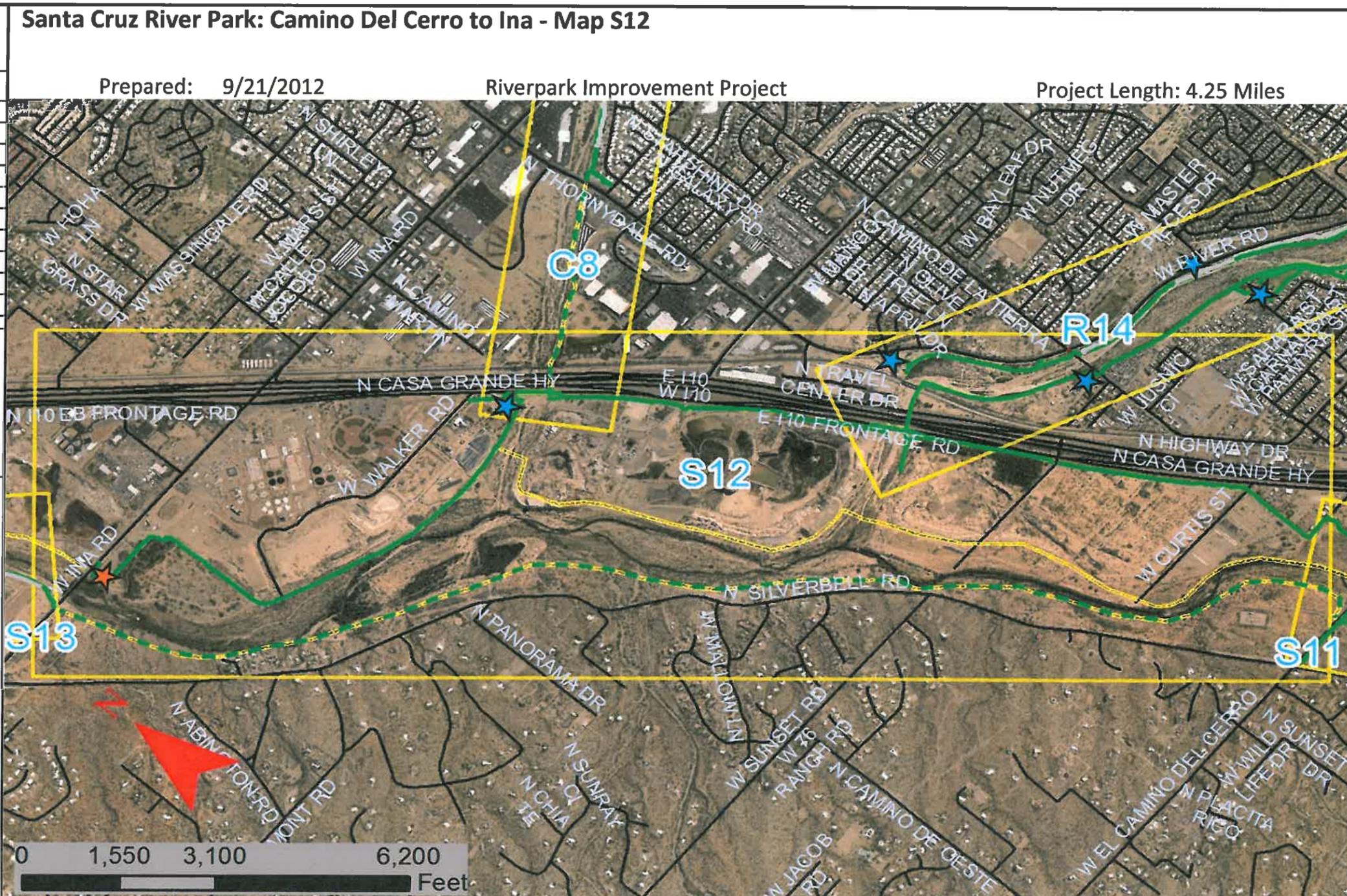
Comments: to be developed upon completion of the Roger Road Treatment Plant upgrades to avoid conflicts.

Enhancement Opportunities: install asphalt pathway along top of levee and connection to Camino del Cerro.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$250,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$90,000.0
Total	\$340,000.0

West Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	None
Bridge:	N/A
Underpass:	1 - new
Comments: Pathway to be developed as part of Silverbell Road widening project	
Enhancement Opportunities: provide new shared use path along Silverbell Road alignment. Install new irrigation system and landscape. install new trailhead near Sunset Road or Orange Grove Road areas. Provide new underpass at Camino del Cerro. Provide trail signage.	



Estimated Cost:	
Landscape	\$125,000.0
Irrigation	\$300,000.0
Asphalt (& DG)	\$850,000.0
Trailhead/RR	\$100,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$525,000.0
Total	\$2,200,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Access Pathway
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	12' not installed
Connectivity:	2E NC 1E TH
Bridge:	2 - new
Underpass:	1 - new
Comments: temporary alignment along frontage road with asphalt path the only amenity. Path north of CDO Wash is in permanent location so only need for 2.5 miles of improvement. Connections to Diamond Street, Sunset Road, and Orange Grove Road already in place.	
Enhancement Opportunities: provide new park alignment along river channel. Install new irrigation system and landscape. Provide new 16 foot asphalt and 8 foot DG paths. Install new neighborhood connection at Ina Road and provide connection into Ina Sports Park. Install new bridges at the Rillito River and CDO Wash. Provide new underpass at Camino del Cerro. Provide trail signage.	

Estimated Cost:	
Landscape	\$100,000.0
Irrigation	\$150,000.0
Asphalt (& DG)	\$750,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$1,750,000.0
Underpass	\$300,000.0
Other	\$985,000.0
Total	\$4,050,000.0

Santa Cruz River Park: Ina to Cortaro - Map S13

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.3 Miles

West Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path:	14' 8'
Connectivity:	2E NC 1E TH
Bridge:	N/A
Underpass:	1 - new

Comments: A 14' paved path and an 8' soft path exist now. Irrigation for Marana Town Park in northern area.

Enhancement Opportunities: Extend irrigation system south to Ina and enhance landscape. Provide new underpass at Ina Road. Install trail signage.

Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$126,000.0
Total	\$526,000.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	not installed
Asphalt Cond. DG Path:	not installed not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	1 - new

Comments: existing soil cement bank protection ends roughly 1 mile north of Ina Road.

Enhancement Opportunities: Acquire ROW from sand and gravel operation. install new irrigation system and landscape at north and south extents. Provide 16 foot asphalt path and 8 foot DG path where applicable. Install new neighborhood connection at commercial center at Cortaro Farms Road. Provide new underpass at Ina Road. fence off trail alignment through unprotected area north of soil cement. Provide general site clean up.

Estimated Cost:	
Landscape	\$50,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$390,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$226,512.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$3,840,000.0
Total	\$4,871,512.0

Santa Cruz River Park: Cortaro to Twin Peaks - Map S14

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 2 Miles

West Bank

Existing Conditions:

Landscaping:	E/D
Irrigation:	good
Asphalt Cond. DG Path:	14' not installed
Connectivity:	3E NC
Bridge:	2 - pass
Underpass:	1 - new

Comments: Irrigation on potable. 14' paved path only.

Enhancement Opportunities:
Repair existing irrigation system as needed and enhance landscape. Rehabilitate 14 foot wide asphalt path north of Wade McLean Park and provide DG path where applicable. Enhance access point at Twin Peaks. Provide trail signage.

Estimated Cost:

Landscape	\$125,000.0
Irrigation	\$75,000.0
Asphalt (& DG)	\$195,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$134,000.0
Total	\$544,000.0

East Bank

Existing Conditions:

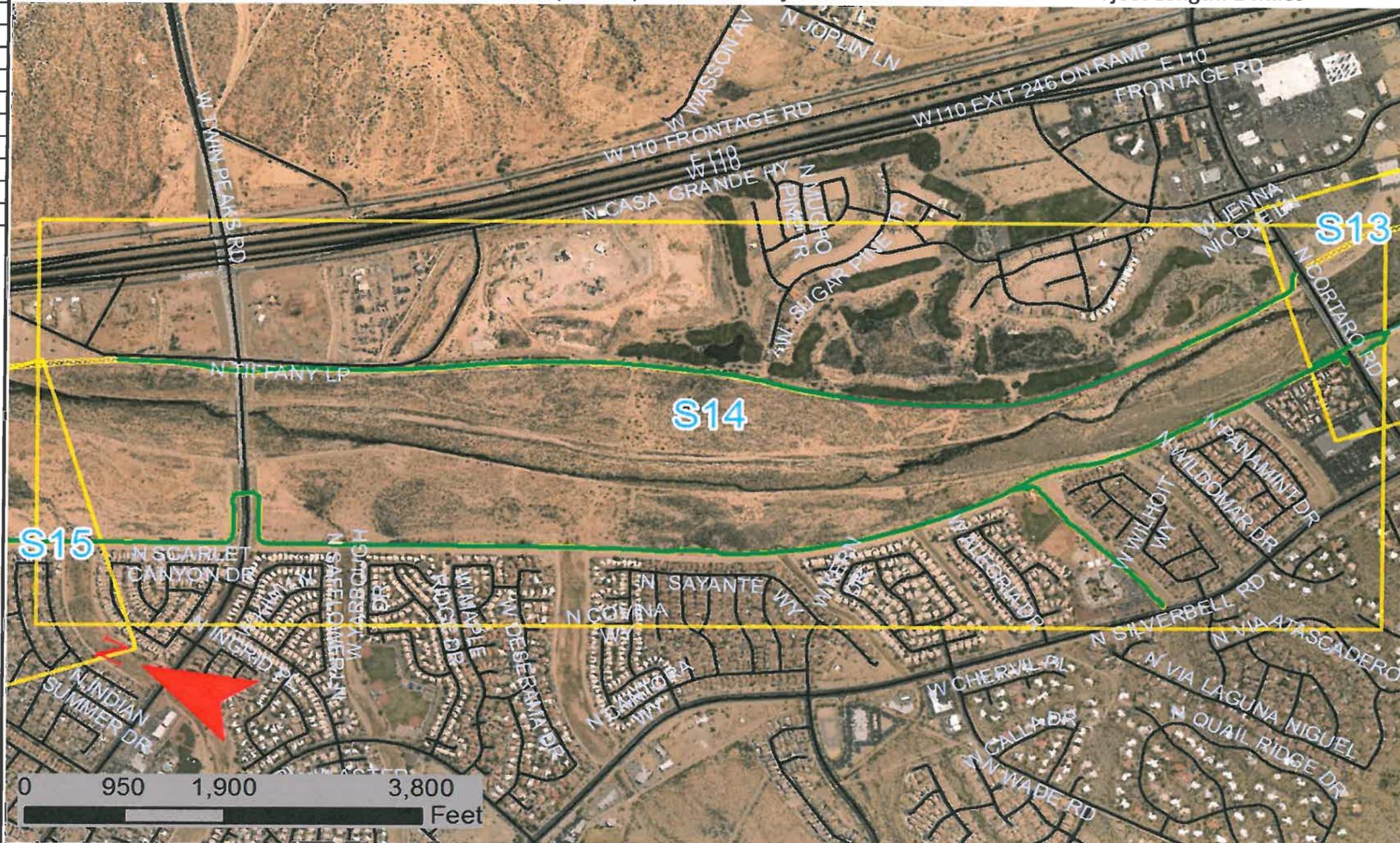
Landscaping:	E/D
Irrigation:	none
Asphalt Cond. DG Path:	14' not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	1 - new

Comments: This segment has a 14' paved pathway only.

Enhancement Opportunities:
install new irrigation system and landscape. Rehabilitate 14 foot wide asphalt path and provide DG path where applicable. Enhance access to Twin Peaks Road. Provide new underpass at Cortaro Farms Road. Provide trail signage.

Estimated Cost:

Landscape	\$75,000.0
Irrigation	\$125,000.0
Asphalt (& DG)	\$300,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$254,000.0
Total	\$1,069,000.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)

- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Santa Cruz River Park: Twin Peaks to Avra Valley Road - Map S15

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 2.7 Miles

West Bank

Existing Conditions:

Landscaping:	E. Small C at north
Irrigation:	good
Asphalt Cond. DG Path:	14' partial. not installed
Connectivity:	1E NC 1E TH
Bridge:	N/A
Underpass:	N/A

Comments:
There's a 14' path only to from Twin Peaks to Coachline

Enhancement Opportunities:
Repair existing irrigation system as needed and enhance landscape. Rehabilitate 14 foot wide asphalt path and install DG path where applicable. Provide trail signage.

Estimated Cost:

Landscape	\$100,000.0
Irrigation	\$40,000.0
Asphalt (& DG)	\$210,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$94,000.0
Total	\$444,000.0

East Bank

Not under consideration

Landscaping:	not installed
Irrigation:	not installed
Asphalt Cond. DG Path:	not installed not installed
Connectivity:	not installed
Bridge:	1 - new
Underpass:	1 - fail

Comments:
not developed

Enhancement Opportunities: install new irrigation system and landscape. Install 14 foot asphalt path and DG path where applicable and new fencing along levee. Develop access to Avra Valley Road. Provide bridge over I-10 drainage channel. Widen existing Avra Valley underpass. Provide trail signage.

Estimated Cost:

Landscape	\$100,000.0
Irrigation	\$170,000.0
Asphalt (& DG)	\$810,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$40,000.0
Drainage	\$0.0
Bridge	\$227,500.0
Underpass	\$200,000.0
Other	\$425,000.0
Total	\$1,987,500.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

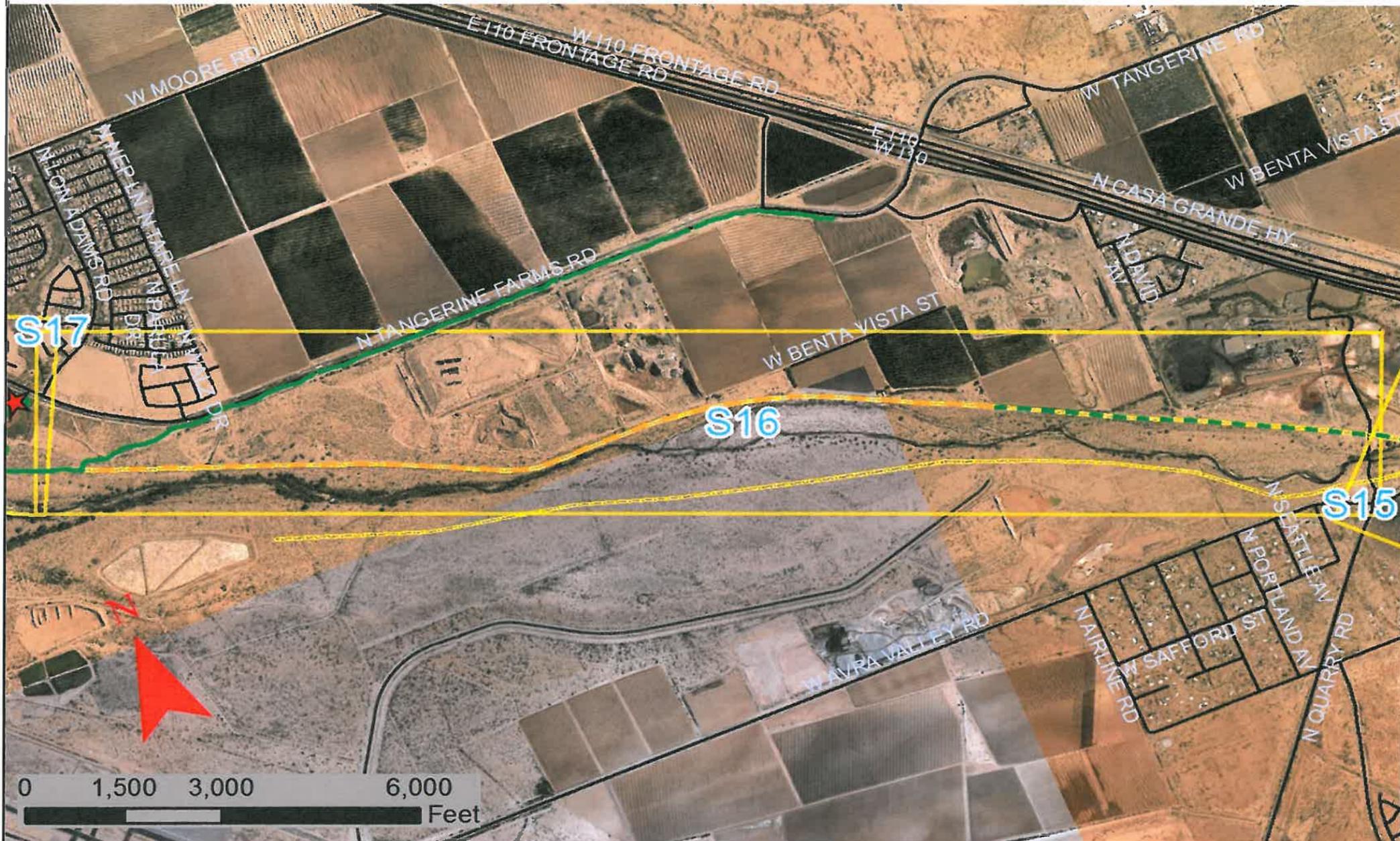
- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Santa Cruz River Park: Avra Valley Road to Tangerine - Map S16

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 4 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond.	not installed
DG Path	not installed
Connectivity:	1E
Bridge:	2 - new
Underpass:	failing

Comments:
14 foot wide asphalt path under development 2 miles south of Tangerine Road by Town of Marana. Includes irrigation and minimal landscape.

Enhancement Opportunities:
Acquire ROW from California Portland Cement. Extend irrigation system south to Avra Valley Road and enhance landscape. Install new 14 foot wide asphalt path. Provide bridge over California Portland Cement conveyor belt and access road. Provide levee fencing and trail signage.

Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$150,000.0
Asphalt (& DG)	\$510,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$68,000.0
Drainage	\$0.0
Bridge	\$525,000.0
Underpass	\$0.0
Other	\$1,700,000.0
Total	\$3,003,000.0

Santa Cruz River Park: Tangerine to Sanders Road - Map S17

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 2.7 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	Good
Asphalt Cond. DG Path	14' not installed
Connectivity:	1E NC 4E TH
Bridge:	N/A
Underpass:	N/A

Comments:
Potable meters at White Ave. & Tangerine Farms Rd. A 14' paved path exists now. New plantings at north end.

Enhancement Opportunities:
repair existing irrigation system and enhance landscape. Rehabilitate 14 foot wide asphalt path. Provide trail signage.

Estimated Cost:

Landscape	\$150,000.0
Irrigation	\$120,000.0
Asphalt (& DG)	\$405,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$162,000.0
Total	\$837,000.0

West Bank

Cañada del Oro Wash: Tangerine Road to Oracle Road - Map C 1

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
Not developed.

Enhancement Opportunities:
Utilize existing natural landscape and intertwine DG path to keep natural feel and access into Big Wash. Install 3 trailheads at Catalina Shadows subdivision. Provide trail signage.

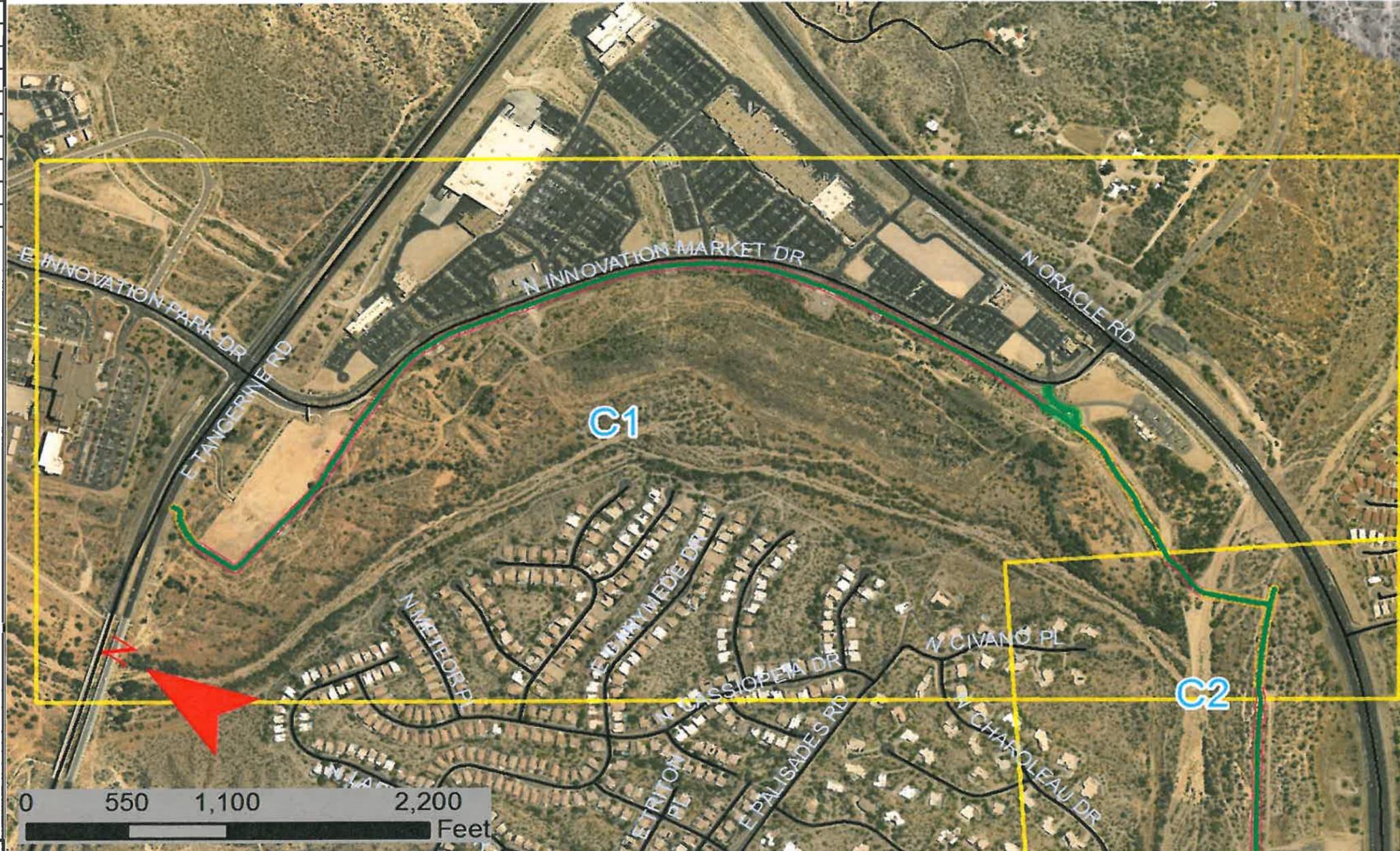
Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$75,000.0
Trailhead/RR	\$0.0
Node	\$45,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$40,000.0
Total	\$160,000.0

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.5 Miles



Map Legend:

— Street	▲ Existing Trailhead
▭ Map Sheet Reference	▲ Proposed Trailhead
— Open Pathway (Asphalt)	▲ Existing Neighborhood Connection
— Open Pathway (Decomposed Granite)	▲ Proposed Neighborhood Connection
- - - Conceptual Pathway (Asphalt)	— Access Pathway
- - - Planned Pathway (Asphalt)	

Abbreviations:

■ A - Premium, 100% to 81% Tree Canopy
■ B - High, 51% to 80% Tree Canopy
■ C - Medium, 21% to 50% Tree Canopy
■ D - Low, 0% to 20% Tree Canopy
■ E - Undeveloped, in Planning or Construction

Existing Conditions:

Landscaping:	C. small D & B
Irrigation:	good
Asphalt Cond. DG Path	12' not installed
Connectivity:	3E NC
Bridge:	1 - pass
Underpass:	N/A

Comments:
There's a 12' path installed along the Oro Valley Marketplace and maintained by management company with limited ROW for enhancement

Enhancement Opportunities:
recently completed project with little additional ROW for expansion. No improvements recommended at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

West Bank

Cañada del Oro Wash: Oracle Road to First Avenue - Map C 2

East Bank

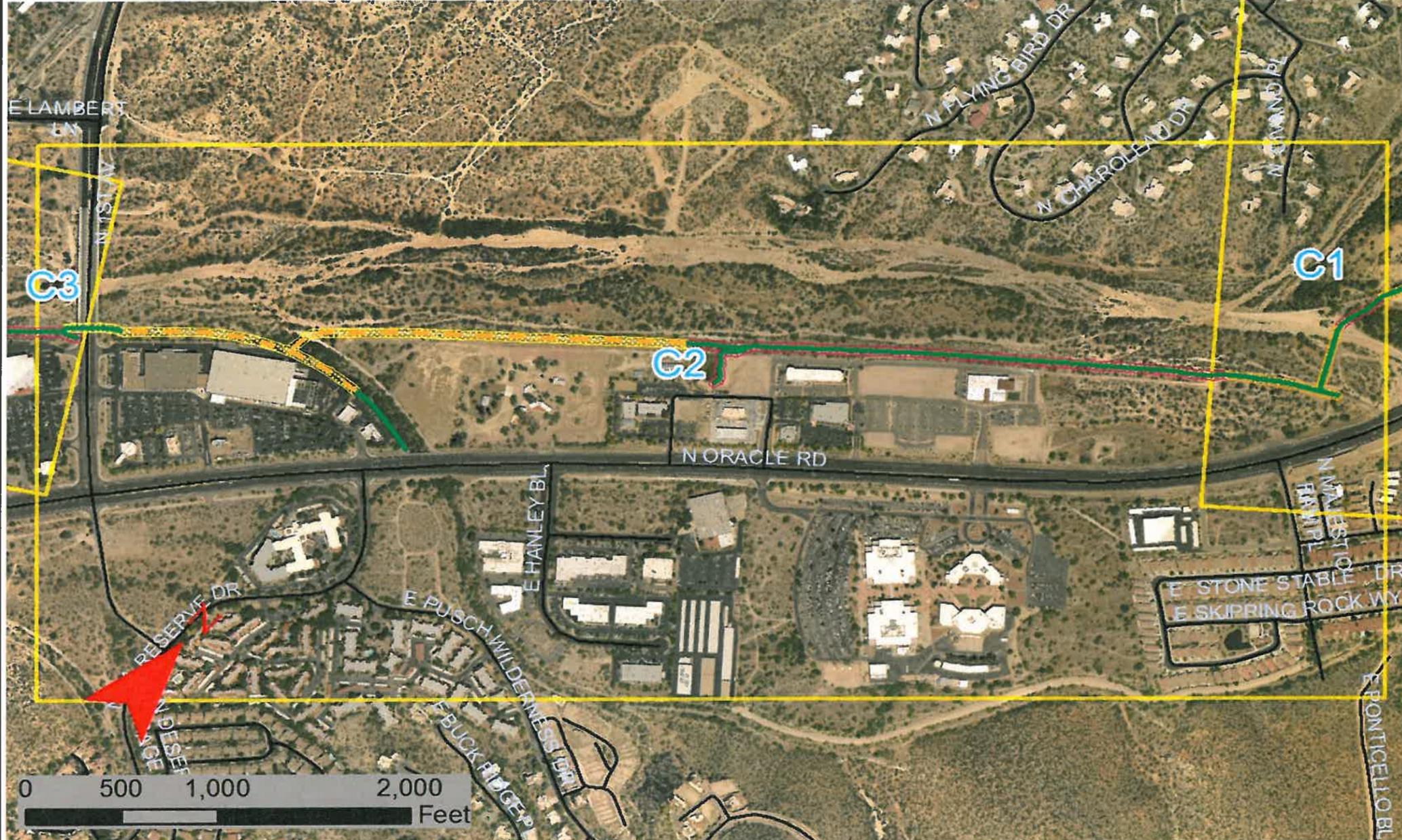
Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	not installed
Bridge:	N/A
Underpass:	N/A

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.25 Miles



Comments: Not developed

Enhancement Opportunities: develop DG path to intertwine with the natural landscape with access into the CDO Wash. Install access point at First Avenue and way finding signage.

Existing Conditions:

Landscaping:	C, D, E, Small B
Irrigation:	None
Asphalt Cond. DG Path	12' not installed
Connectivity:	excellent
Bridge:	4E NC, 1E TH
Underpass:	N/A

Comments: There's 12' paved path on the levee by the commercial portion of Steam Pump Ranch with the connection to First Ave under development. limited development available along the levee due to FEMA restrictions.

Enhancement Opportunities: There are 2 projects in this stretch that will be providing additional amenities. A Transportation Enhancement project to provide a paved path connection to First Ave with a bridge over the Steam Pump Wash and a private development by HSL in the north stretch providing neighborhood connections, irrigation, and landscaping. No additional improvements recommended at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$62,500.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$27,000.0
Total	\$104,500.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Cañada del Oro Wash: First Avenue to Pusch View Lane - Map C 3

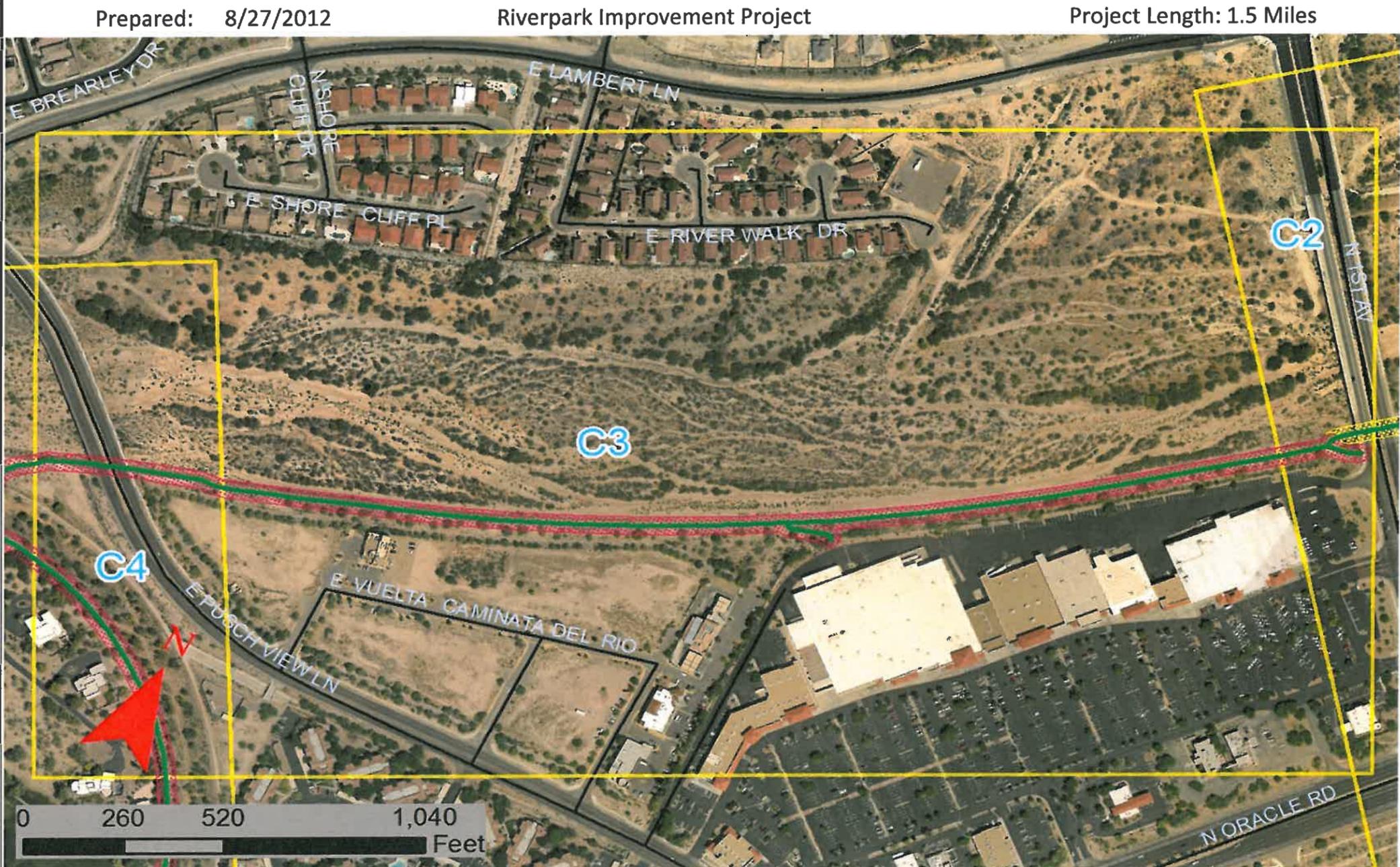
West Bank

Existing Conditions:	
Landscaping:	E
Irrigation:	none
Asphalt Cond. DG Path	10' not installed
Connectivity:	3E NC
Bridge:	N/A
Underpass:	N/A

Comments: existing 10 foot separated multi use path along Lambert Lane. Access MU path at First Avenue street light. No bank protection along CDO wash bank to facilitate riverpark development.

Enhancement Opportunities:
No recommended improvements at this time.

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- ^ Existing Trailhead
- ^ Proposed Trailhead
- ^ Existing Neighborhood Connection
- ^ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:	
Landscaping:	C
Irrigation:	none
Asphalt Cond. DG Path	12' not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	1 - pass

Comments: This segment has a 12' paved path, with a small amount of DG (18" to 6") next to it on the south

Enhancement Opportunities: No recommended improvements at this time.

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

West Bank

Existing Conditions:

Landscaping:	E
Irrigation:	none
Asphalt Cond. DG Path	10' not installed
Connectivity:	2E NC, 1E TH
Bridge:	N/A
Underpass:	1 - pass

Comments: existing 10 foot separated multi use lane along Lambert Lane. No bank protection along the CDO wash to facilitate riverpark development.

Enhancement Opportunities: no recommended improvements at this time.

Estimated Cost:

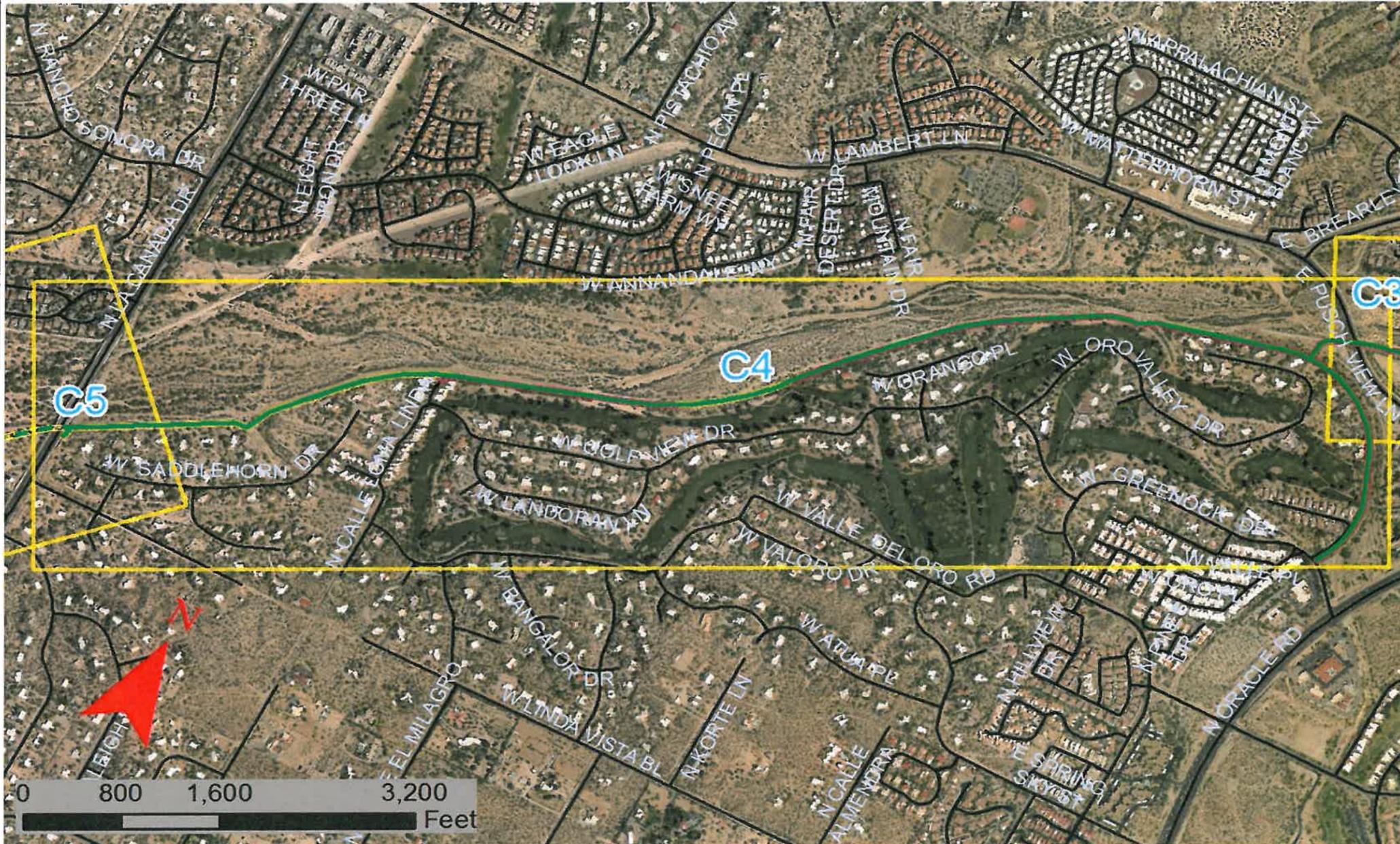
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Cañada del Oro Wash: Pusch View Lane to La Canada - Map C4

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.5 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	C w/ some D
Irrigation:	poor
Asphalt Cond. DG Path	10', 12' shoulders
Connectivity:	4E NC
Bridge:	3 - pass
Underpass:	1 - pass

Comments: Potable Irrigation was used the first 2 years for establishment, but has not been used for the last 3 years. This long segment has a 12' paved path only for most of the way; a 10' paved path covers the roughly 1,300' between the bridges and just south of Pusch View Lane. some good mature canopy.

Enhancement Opportunities: no recommended improvements at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

South Bank

Cañada del Oro Wash: La Canada to La Cholla - Map C5

North Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path:	not installed
Connectivity:	2N NC, 1N TH
Bridge:	N/A
Underpass:	1 - new

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.5 Miles

Existing Conditions:

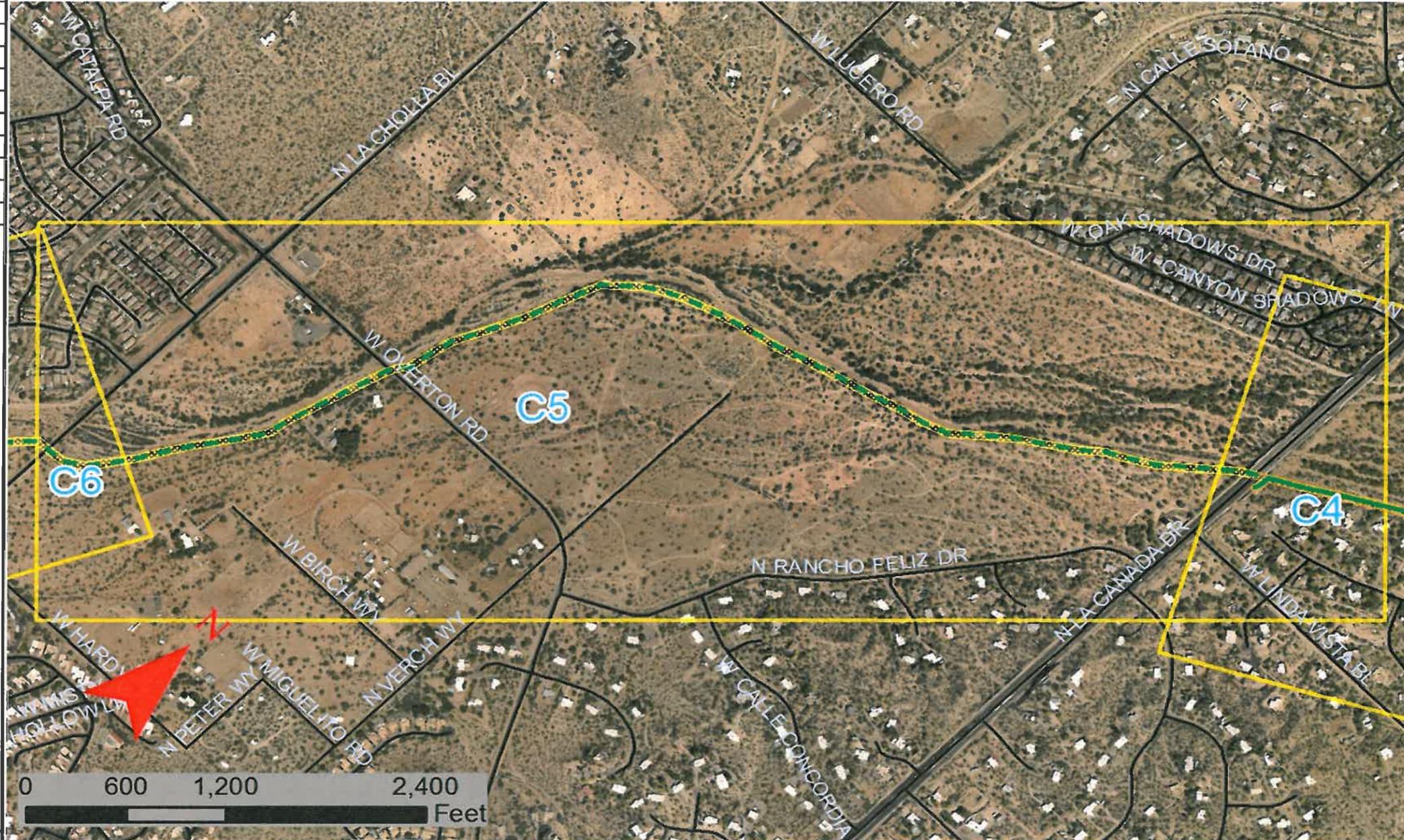
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path:	not installed
Connectivity:	1N NC
Bridge:	N/A
Underpass:	1 - new

Comments:
project under development

Comments:
Not developed

Enhancement Opportunities:
Project under development. Restoration, pathways, trailhead, and La Canada underpass and at grade lighted crossing of Overton improvements all under investigation.

Enhancement Opportunities:
proposed project alignment along Lucero Road with at grade crossing on neighborhood tributary and connection to La Cholla Boulevard. Provide La Canada underpass improvement. No bank protection exists to facilitate riverpark development.



Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$100,000.0
Asphalt (& DG)	\$450,000.0
Trailhead/RR	\$250,000.0
Node	\$30,000.0
Fencing	\$100,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$890,000.0
Total	\$2,170,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)

- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:

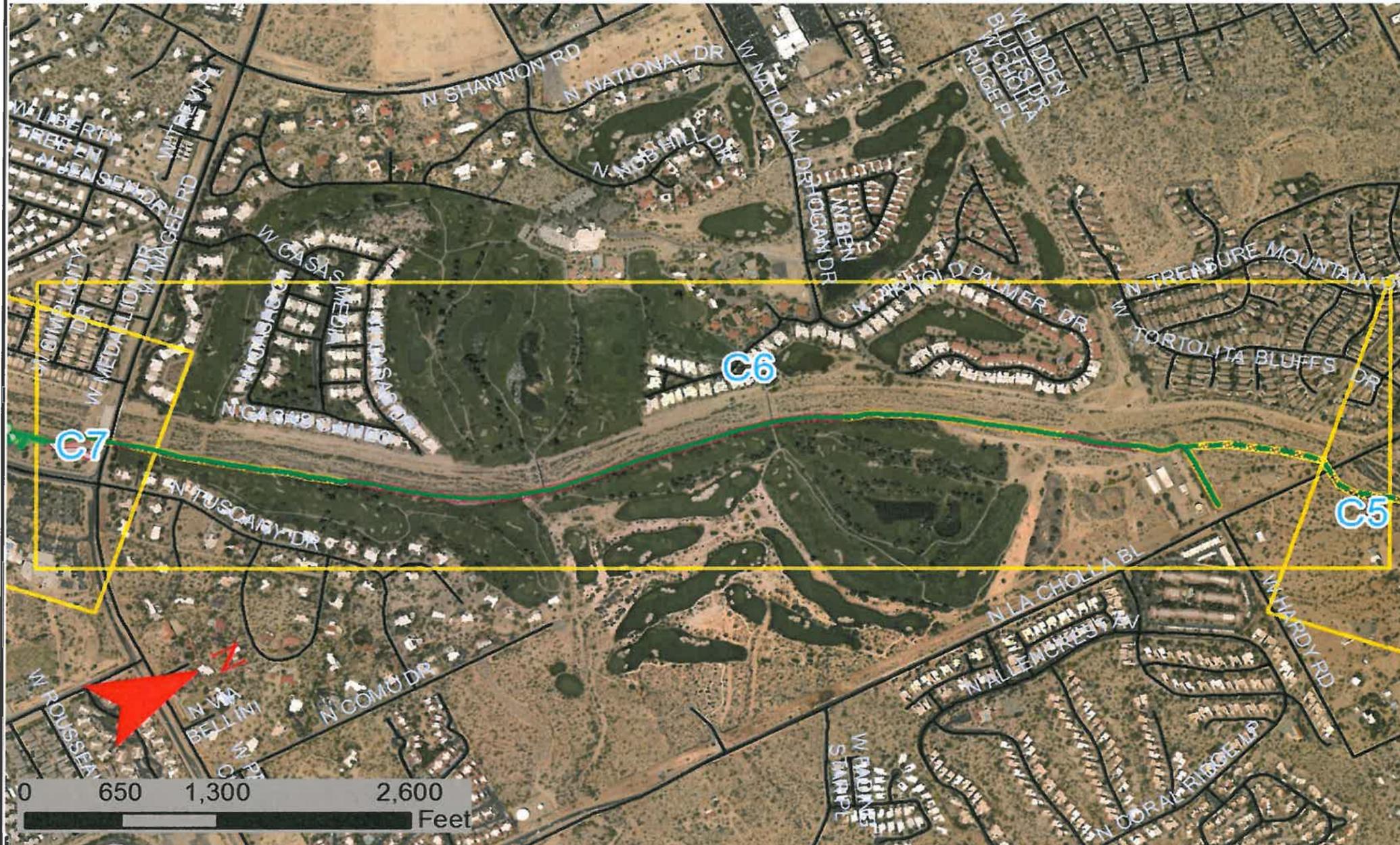
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$230,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$238,000.0
Total	\$768,000.0

Cañada del Oro Wash: La Cholla to Magee - Map C6

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.5 Miles



Map Legend:

- Street
- ▭ Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- ▭ A - Premium, 100% to 81% Tree Canopy
- ▭ B - High, 51% to 80% Tree Canopy
- ▭ C - Medium, 21% to 50% Tree Canopy
- ▭ D - Low, 0% to 20% Tree Canopy
- ▭ E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	C, D
Irrigation:	good
Asphalt Cond. DG Path:	10' (avg.) not installed
Connectivity:	1E NC
Bridge:	N/A
Underpass:	1 - new

Comments: Section only 4 years old. Reclaimed line connection available at Magee, but the line is at capacity during peak usage. Along the golf course, the CDO is in a tight corridor with a 10' path, with variances of up to 15' under a pedestrian bridge to as narrow as 7.5' about 725' north of Magee.

Enhancement Opportunities: recently completed no improvements recommended at this time due to limited ROW.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

West Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments: Not developed

Enhancement Opportunities: install new irrigation and landscape. Install 16 foot asphalt path and DG path where applicable. Install three new neighborhood connections. Requires 2 bridge crossings of local drainage channels and several drainage improvements.

Estimated Cost:

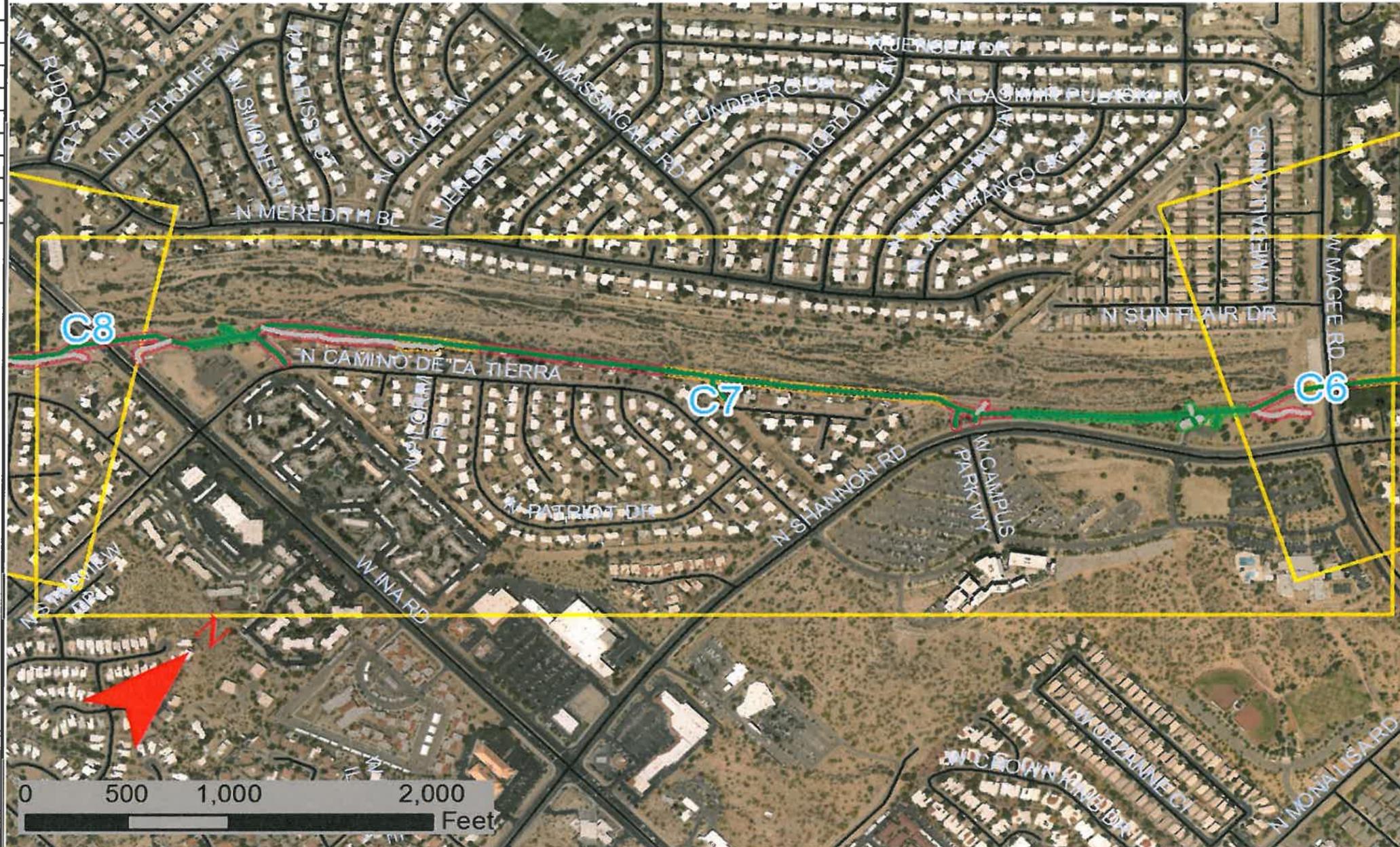
Landscape	\$50,000.0
Irrigation	\$60,000.0
Asphalt (& DG)	\$330,000.0
Trailhead/RR	\$0.0
Node	\$45,000.0
Fencing	\$5,000.0
Drainage	\$40,000.0
Bridge	\$227,500.0
Underpass	\$0.0
Other	\$423,000.0
Total	\$1,180,500.0

Cañada del Oro Wash: Magee to Ina - Map C7

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.1 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	C, D, Small B's
Irrigation:	good
Asphalt Cond. DG Path	10' to 12' 8' partial
Connectivity:	3E NC, 1E TH
Bridge:	1 - pass
Underpass:	1 - pass

Comments: Reclaim line connection available at Magee, but the line is at capacity during peak usage. Irrigation system was installed ready for reclaimed. There's a 12' paved and 8' soft path, but most of the path is a sole 10' paved only with limited available ROW.

Enhancement Opportunities: recently installed and with limited ROW available for enhancement no recommended improvements at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

South Bank

Cañada del Oro Wash: Ina to I-10 - Map C8

North Bank

Existing Conditions:

Landscaping:	good
Irrigation:	Good
Asphalt Cond. DG Path	12' 8'
Connectivity:	1E NC, 1E TH
Bridge:	N/A
Underpass:	1- pass, 2 - new

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.25 Miles

Existing Conditions:

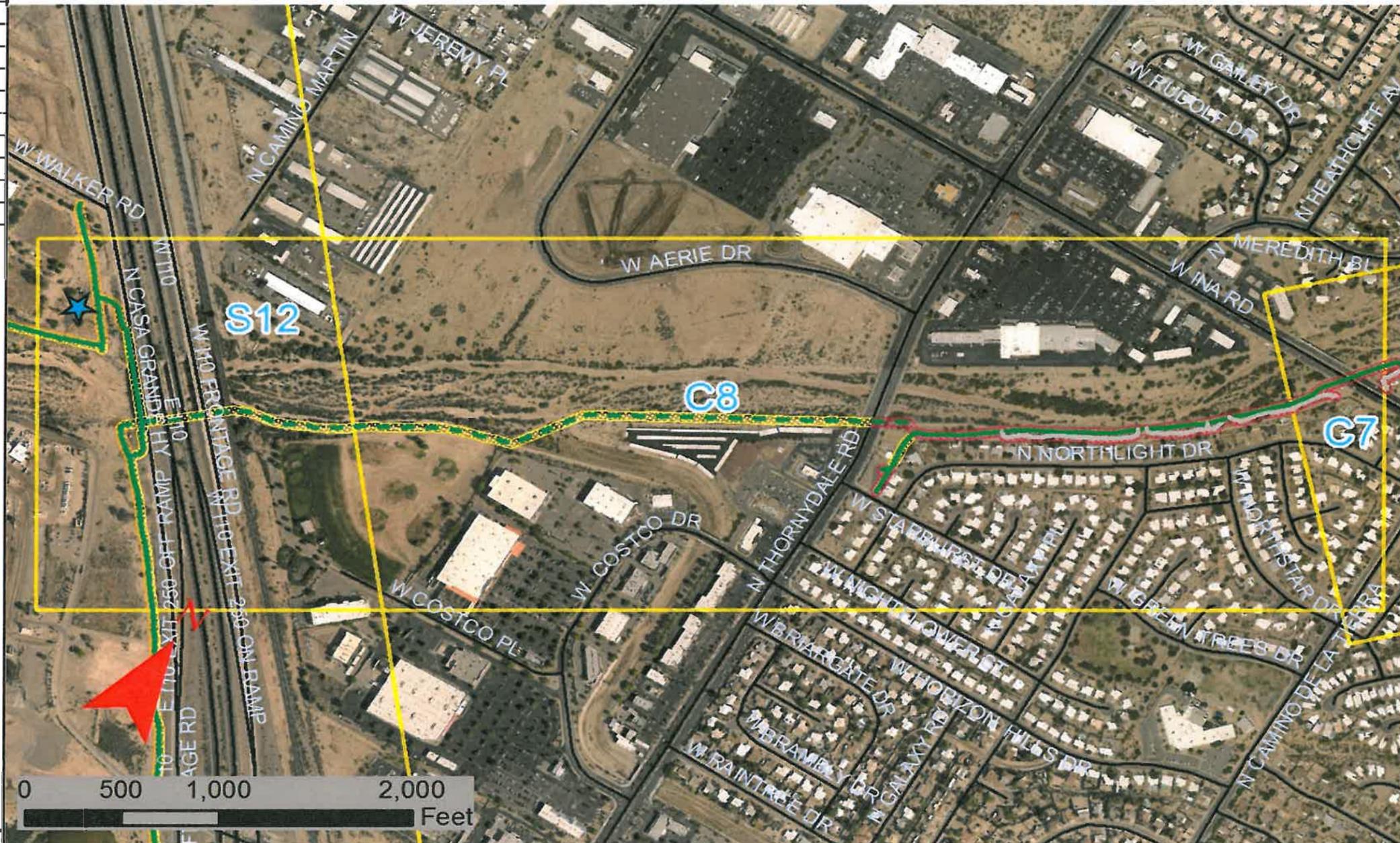
Landscaping:	not installed
Irrigation:	None
Asphalt Cond. DG Path	not installed not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
Reclaimed line available at Magee, but it is at capacity during peak usage times. Improvements end at Thornydale Road but pathway project underway to extend into Santa Cruz River. Limited ROW does not provide for landscape amenities.

Enhancement Opportunities:
Install 16 foot asphalt paths from Thornydale Road to Santa Cruz River. Provide underpass improvements at Thornydale Road and Interstate 10. Pedestrian bridge is required to cross Carmack Wash. Install access to Thornydale Road.

Comments:
Not developed

Enhancement Opportunities:
Install new irrigation and landscape. Install new 16 foot asphalt path and DG path where applicable. Install connections to Ina and Thornydale Roads. Provide underpass improvements at Ina, Thornydale, and Interstate 10.



Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$225,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$297,500.0
Underpass	\$750,000.0
Other	\$540,000.0
Total	\$1,827,500.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)

- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$75,000.0
Asphalt (& DG)	\$375,000.0
Trailhead/ RR	\$0.0
Node	\$45,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$1,200,000.0
Other	\$775,000.0
Total	\$2,520,000.0

South Bank

Existing Conditions:

Landscaping:	C
Irrigation:	fair
Asphalt Cond. DG Path	13' 8' for part
Connectivity:	1E TH, 3E NC
Bridge:	1 - pass
Underpass:	1 - N/A

Comments:
 This area has sparse vegetation. Reclaimed - 1 meter supplies Alvernon to Craycroft including the Swan Wetlands. This section includes a 13' paved path and a 4' d.g. shoulder for first 1/4 mile west of Craycroft, 8' d.g. path almost to Swan. Underpass at Craycroft is under development. No ROW acquisition proposed

Enhancement Opportunities:
 Replace irrigation system and enhance existing vegetation. Recent 12' plus asphalt path installed in 2011, recommend no upgrades at this time. DG path alignment set by existing ROW, no room for enhancement. Sufficient existing connections to neighborhoods and roadways exist. Install trail signage as needed.

Estimated Cost:

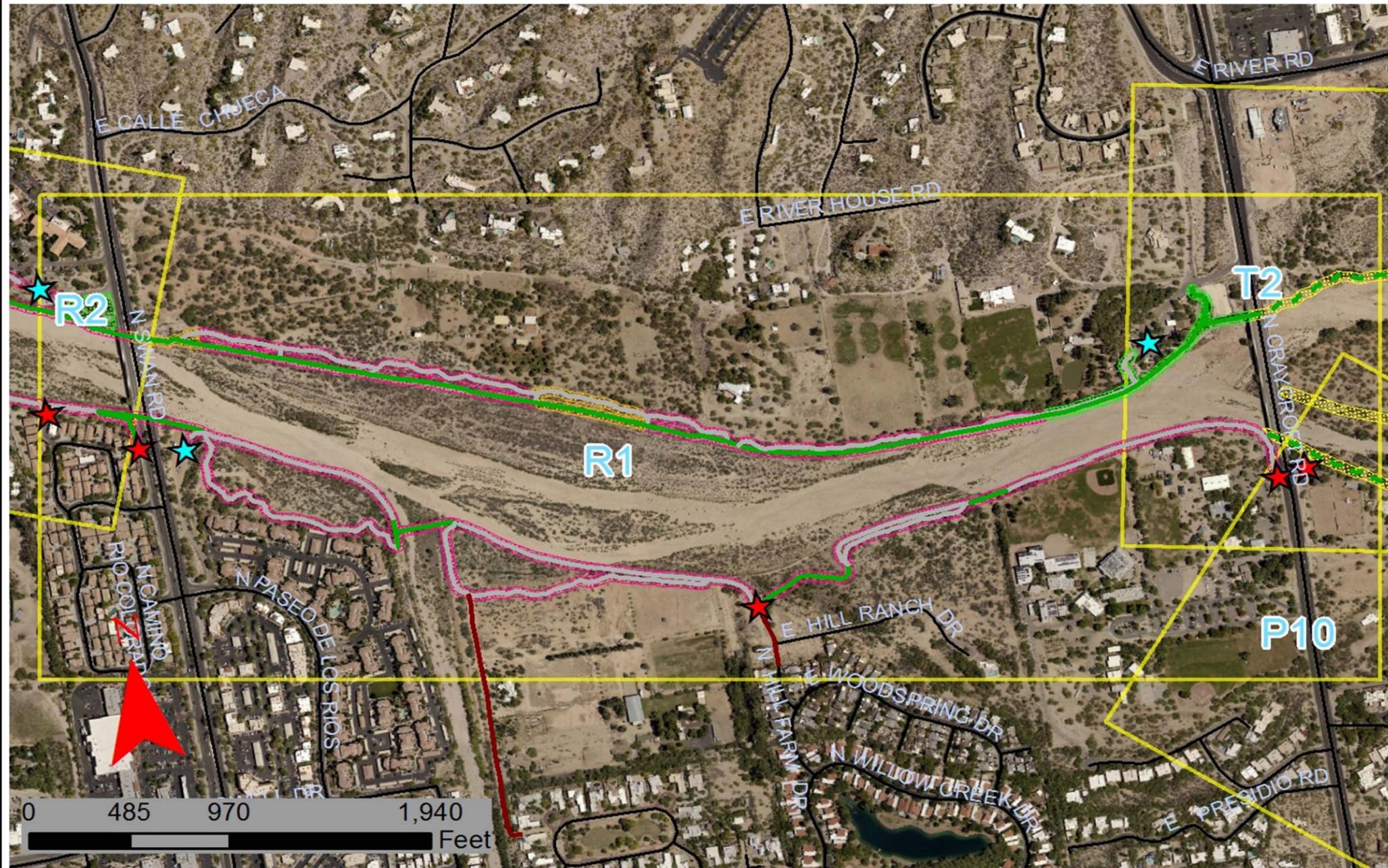
Landscape	\$50,000.0
Irrigation	\$50,000.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$33,000.0
Total	\$133,000.0

Rillito River Park: Craycroft to Swan - Map R1

Prepared: 10/17/2012

Riverpark Improvement Project

Project Length: 1 Mile



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructor

North Bank

Existing Conditions:

Landscaping:	B, C, & D
Irrigation:	fair
Asphalt Cond. DG Path	10' 8'
Connectivity:	2E TH
Bridge:	1 - pass
Underpass:	1 - pass

Comments:
 There is lush veg at Craycroft. To the west, no space for B upgrade due to path at rail. Reclaimed - 2 Meters supply Dodge to Craycroft. Pathway is 10', but can be expanded towards the existing railing. Trailheads exist on either end of the extent; there are no appropriate neighborhood connections due to sparse development adjacent to path.

Enhancement Opportunities:
 Remove and replace existing asphalt path with full 16' section. Replace irrigation and enhance existing landscape. Provide trail signage installation.

Estimated Cost:

Landscape	\$50,000.0
Irrigation	\$50,000.0
Ashpalt (& DG)	\$200,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$63,000.0
Total	\$363,000.0

South Bank

Existing Conditions:

Landscaping:	C, D
Irrigation:	fair
Asphalt Cond. DG Path	13' 8'
Connectivity:	1E TH
Bridge:	1 - Pass
Underpass:	1 - Fail

Comments: Reclaimed - 1 Meter supplies Alvernon to Craycroft including the Swan Wetlands. Spacious in and out of floodplain. Swan underpass requires widening to 12'. Existing bridge is adequate. No new node connections required.

Enhancement Opportunities: Recently installed mainline for irrigation; recommend upgrades as needed and new landscaping enhancements. Pathway recently installed at 12' plus, recommend no widening at this time. Swan underpass widening is required. Trail signage installation required.

Estimated Cost:

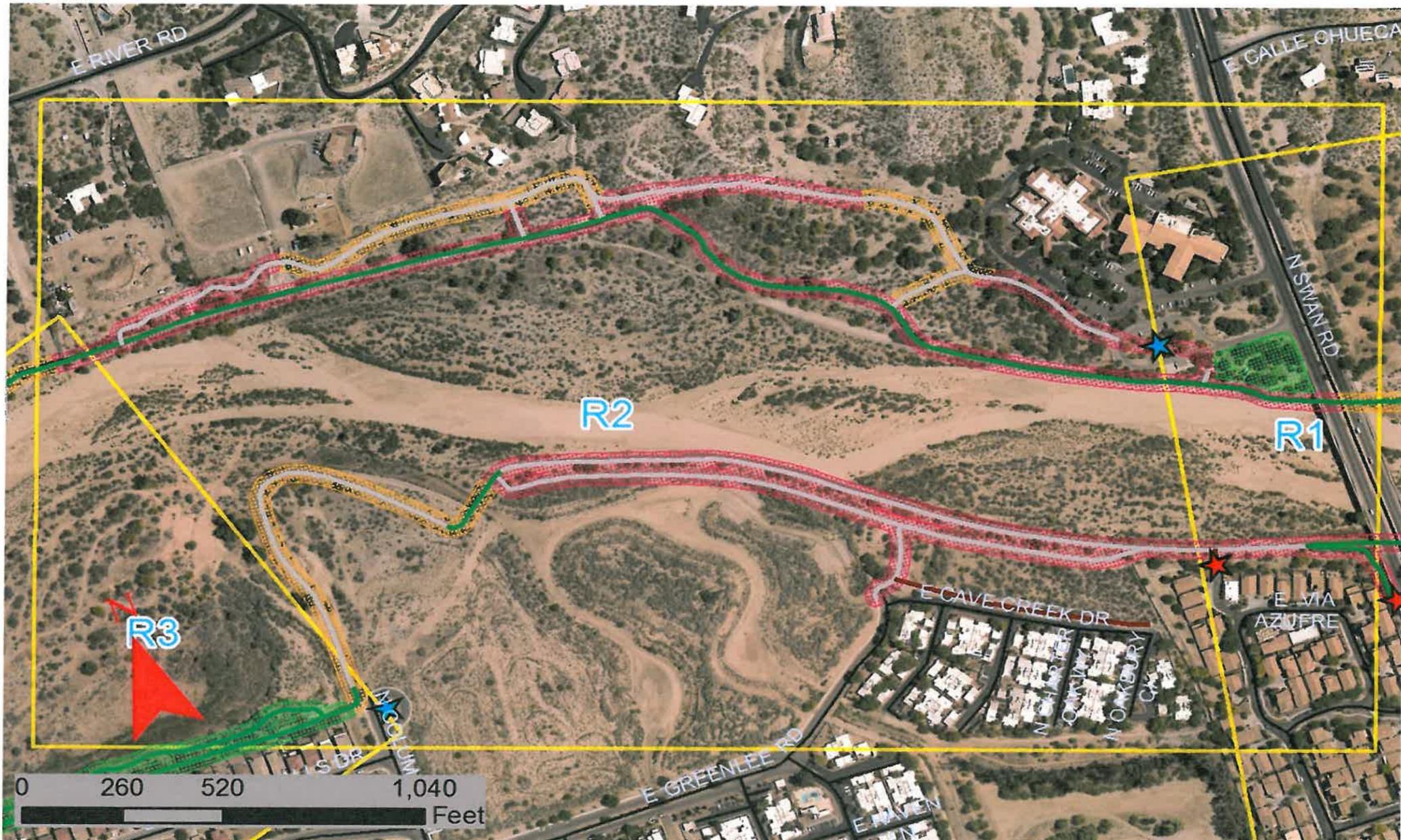
Landscape	\$20,000.0
Irrigation	\$35,000.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$110,000.0
Total	\$365,000.0

Rillito River Park: Swan to Columbus - Map R2

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .7 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructor

North Bank

Existing Conditions:

Landscaping:	C, B, D
Irrigation:	fair
Asphalt Cond. DG Path	10' to 13' 8'
Connectivity:	1E TH
Bridge:	N/A
Underpass:	1 - Fail

Comments: Space for landscape improvements is adequate. Reclaimed - 1 Meter supplies Dodge to Craycroft. Path location allows widening towards rail.

Enhancement Opportunities: Repair and replace irrigation system as needed. Provide enhancement landscape improvements. Rehab and widen asphalt path to 16'. Cleanup and repair as needed existing trailhead. Repair and relocate asphalt path at Fletcha Caida wash as needed. Swan underpass requires widening to 12'. Trail signage installation required.

Estimated Cost:

Landscape	\$10,000.0
Irrigation	\$20,000.0
Ashpalt (& DG)	\$120,000.0
Trailhead/ RR	\$25,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$100,000.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$190,000.0
Total	\$665,000.0

South Bank

Existing Conditions:

Landscaping:	B, C, D
Irrigation:	fair
Asphalt Cond. DG Path	11' to 12' 8' for most
Connectivity:	1E TH, 1E NC
Bridge:	1 - Pass
Underpass:	N/A

Comments: Reclaimed - 1 Meter supplies Alvernon to Craycroft including the Swan Wetlands. The paved path is down to 11 feet wide behind KB Homes subdivision (the old Christopher City), and is 12' wide at the pedestrian bridge just east of the Alvernon Bridge.

Enhancement Opportunities:
New mainline recently installed. Provide enhancement landscaping to the west. Asphalt trails are recently installed at 12', widening should be deferred at this time. Trailhead signage installation required.

Estimated Cost:

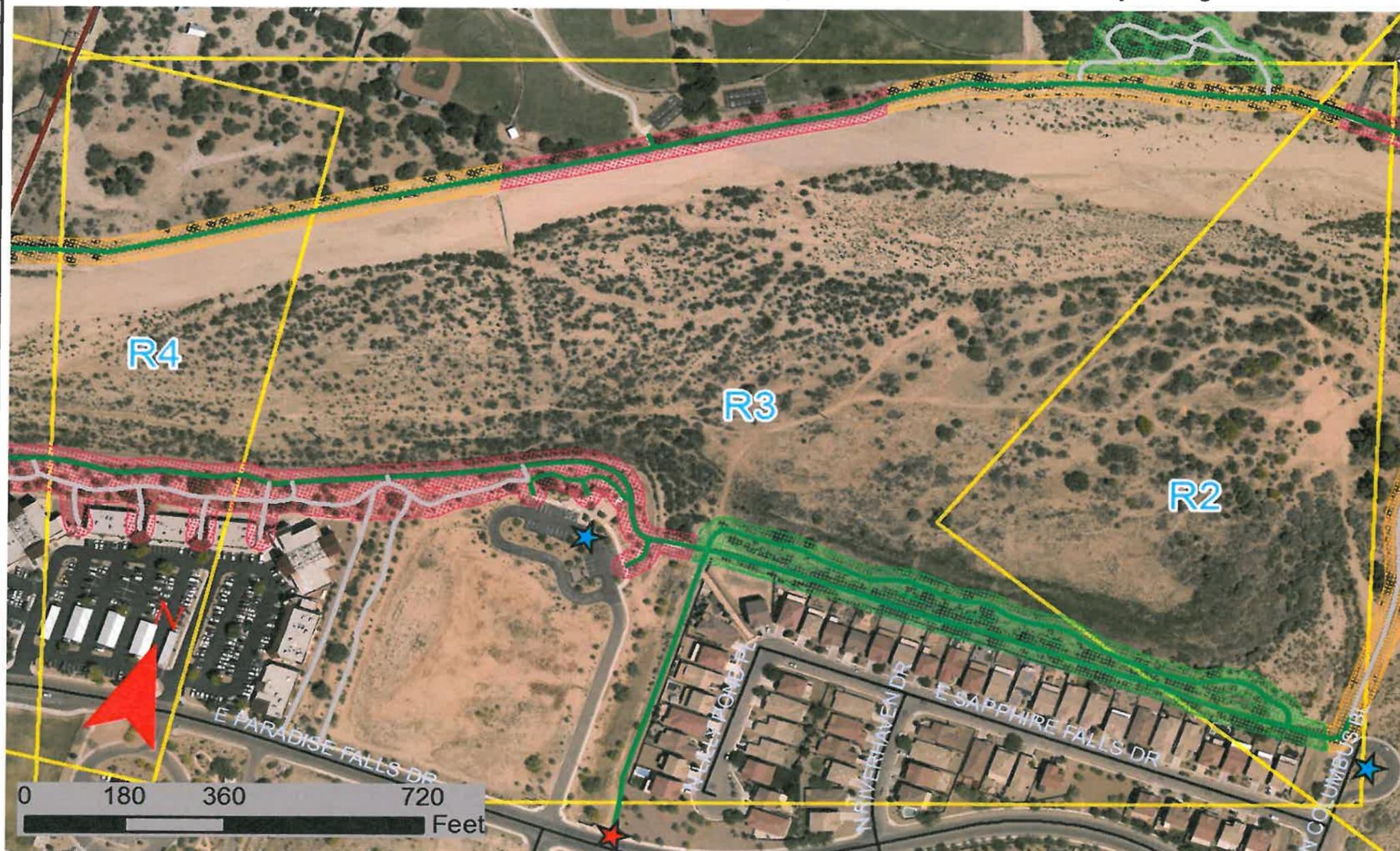
Landscape	\$20,000.0
Irrigation	\$5,000.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$13,000.0
Total	\$38,000.0

Rillito River Park: Columbus to Alvernon - Map R3

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .6 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructor

North Bank

Existing Conditions:

Landscaping:	C, D
Irrigation:	fair
Asphalt Cond. DG Path	10' to 13' 8' for part
Connectivity:	Extg Park
Bridge:	2 - Pass
Underpass:	N/A

Comments: Reclaimed - 2 Meters supply Dodge to Craycroft. For most of the way from Columbus to Alvernon, a paved pathway 10' to 13' wide has been built.

Enhancement Opportunities:
Landscape and irrigation rehab and enhancement provided. Existing path to be rehabilitated and widened to the rail to 16'. Trailhead signage installation required.

Estimated Cost:

Landscape	\$25,000.0
Irrigation	\$5,000.0
Ashpalt (& DG)	\$120,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$50,000.0
Total	\$200,000.0

South Bank

Existing Conditions:

Landscaping:	E/ D
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	6E NC
Bridge:	N/A
Underpass:	1 - Fail

Comments: Not developed. Minimal ROW. Good native trees. No irrigation, section planned for construction.

Enhancement Opportunities: Install new landscape and irrigation. Provide new asphalt path, inadequate ROW for DG path. Provide enhanced local node connection at Dodge. Underpass at Alvernon is 9', provide widening and improve surface. Provide upgrades to street crossing at Dodge Blvd.

Estimated Cost:

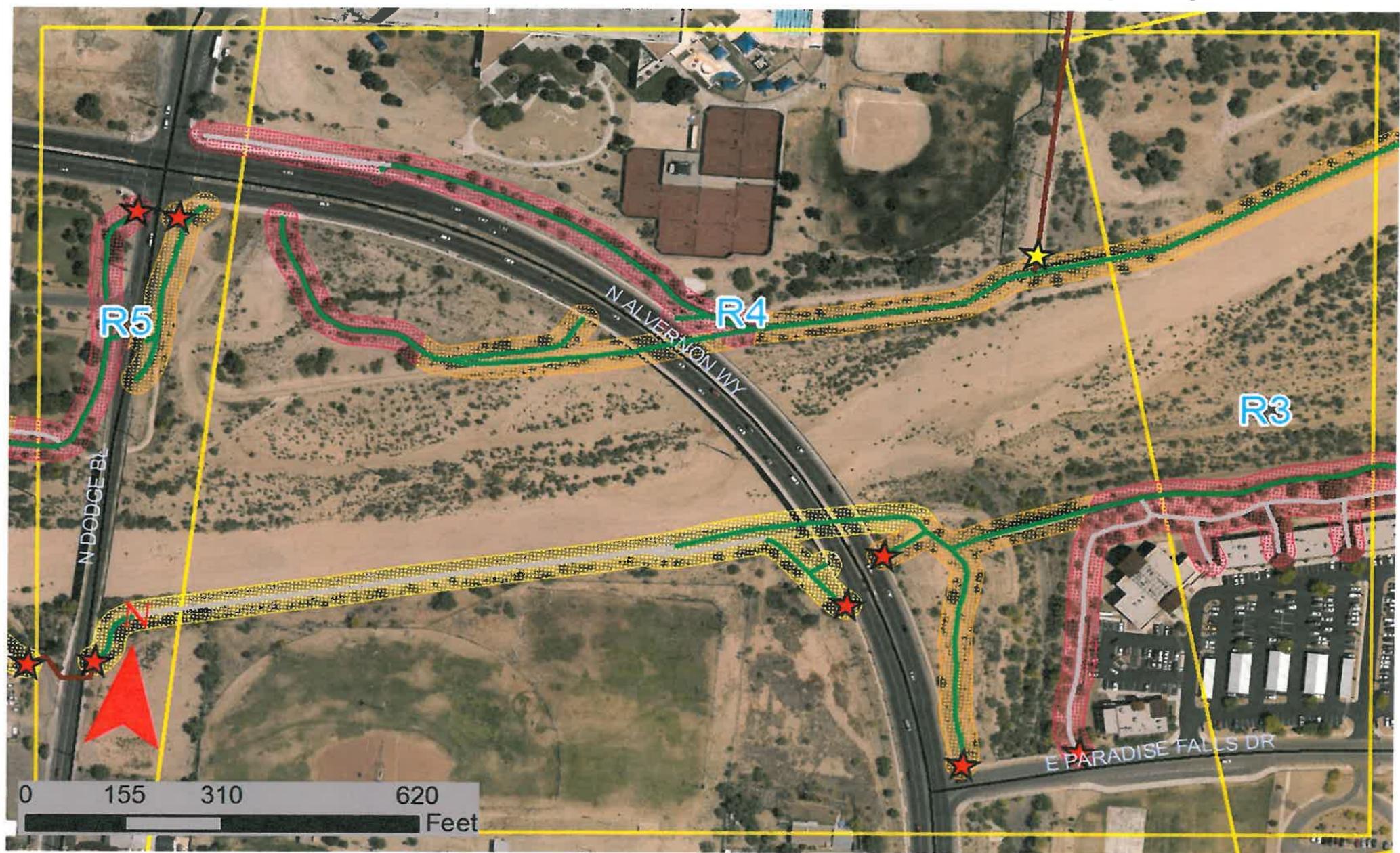
Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Rillito River Park: Alvernon to Dodge - Map R4

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .25 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

North Bank

Existing Conditions:

Landscaping:	C, D
Irrigation:	fair
Asphalt Cond. DG Path	12' not installed
Connectivity:	2E NC, 1N NC
Bridge:	N/A
Underpass:	1 - Fail

Comments: Reclaimed - 2 Meters supply Dodge to Craycroft. The segment has only an 11' paved path leading up from the 8' Alvernon Bridge undercrossing, and no soft path.

Enhancement Opportunities: Enhance landscaping with trees only for this section. Review and widen pathway where possible. Upgrade existing Alvernon underpass to 12'. Restripe and enhance regulatory signage/stripping as well as trail signage.

Estimated Cost:

Landscape	\$20,000.0
Irrigation	\$5,000.0
Ashpalt (& DG)	\$37,500.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$135,000.0
Total	\$397,500.0

South Bank

Rillito River Park: Dodge to Country Club - Map R5

North Bank

Existing Conditions:

Landscaping:	good
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	1N TE, 2E NC
Bridge:	N/A
Underpass:	failing

Comments: Not constructed. Some potential for borrowed landscape. No irrigation. Sufficient space exists to pave 10' to Prince Road. ROW will be required along this extent.

Enhancement Opportunities:
 Provide new asphalt path, irrigation and landscaping from Dodge to Prince only. Path width may be less than standard due to site constraints. Provide 800' of retaining wall stabilization at east end. Enhance existing neighborhood connections. Provide trail fencing as needed. Install new cross channel bridge at Prince Road to provide long term connectivity in lieu of on road connections. Provide new trailhead at bridge; no restroom suggested. Provide trail signage installation.

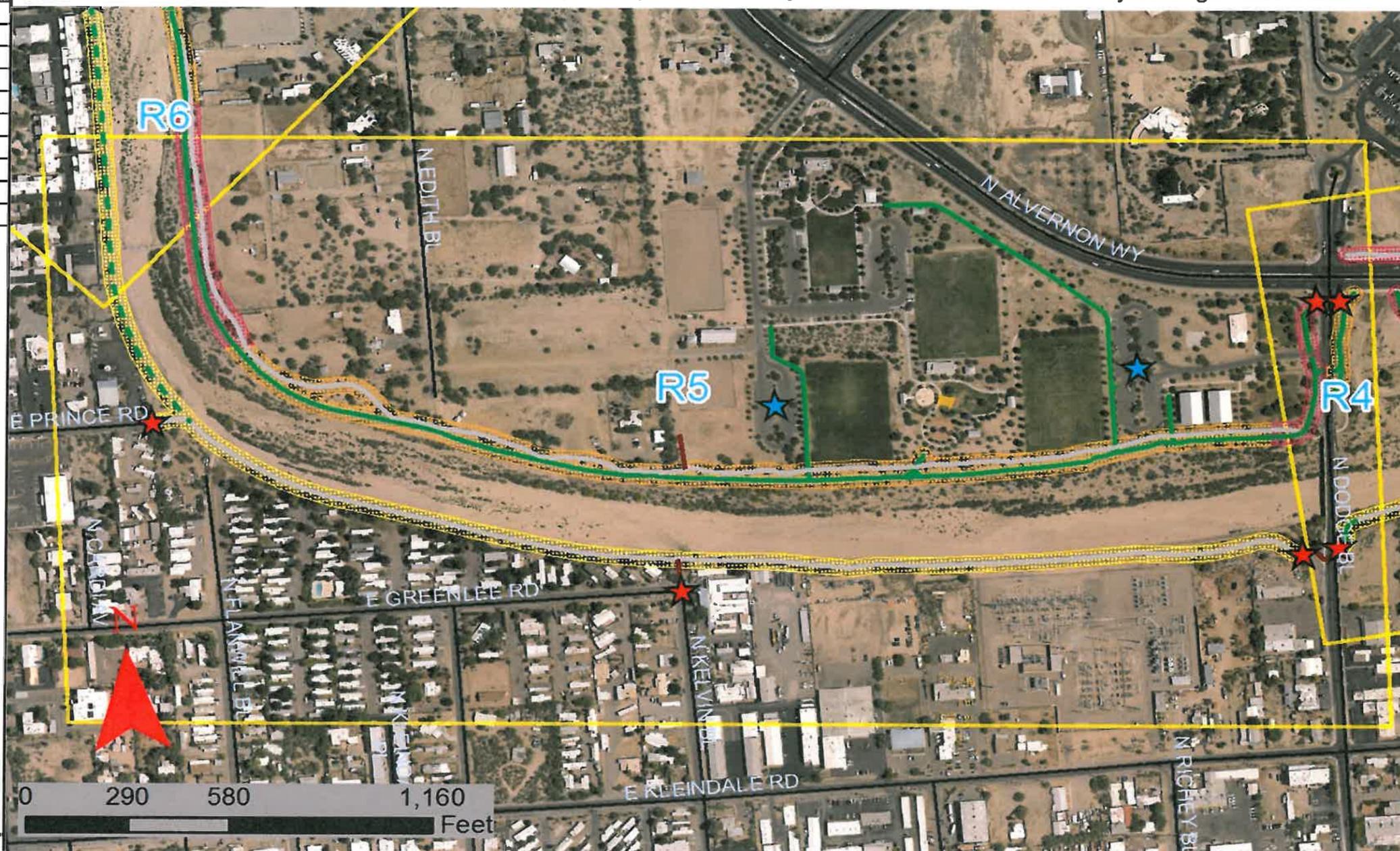
Estimated Cost:

Landscape	\$40,000.0
Irrigation	\$35,000.0
Ashpalt (& DG)	\$225,000.0
Trailhead/RR	\$100,000.0
Node	\$15,000.0
Fencing	\$10,000.0
Drainage	\$0.0
Bridge	\$570,000.0
Underpass	\$0.0
Other	\$920,000.0
Total	\$1,915,000.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 1.35 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructor

Existing Conditions:

Landscaping:	good
Irrigation:	fair
Asphalt Cond. DG Path	12'
Connectivity:	2E TH, 1N NC
Bridge:	N/A
Underpass:	failing

Comments: Need better interface w/ Brandi Fenton. path at rail. Reclaimed - 1 Meter supplies Dodge to Country Club. A 12' paved pathway and an 8' to 10' soft pathway cover the vast majority of this segment. Pathway is against railing, widening will have to extend north.

Enhancement Opportunities:
 Irrigation is about 5 years old, enhance landscaping and minor irrigation adjustments only. Widen recently installed pavement and sealcoat as needed. Provide new local connection at Green Things. Provide trail signage installation.

Estimated Cost:

Landscape	\$55,000.0
Irrigation	\$10,000.0
Ashpalt (& DG)	\$200,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$70,000.0
Total	\$350,000.0

South Bank

Rillito River Park: Country Club to Hacienda del Sol - Map R6

North Bank

Existing Conditions:

Landscaping:	D
Irrigation:	good
Asphalt Cond. DG Path	12' 8'
Connectivity:	2E NC
Bridge:	N/A
Underpass:	N/A

Comments: Reclaimed Irrigation goes 80 yards east of Cactus Blvd., system less than 5 years old. Just west of Country Club bridge, there is a 12' paved path and 8' d.g. path next to the paved path. Paved at rail. Although conceptually a path could be installed between the bridge and Prince Road, it is recommended to provide a cross channel bridge at Prince to use the north bank with fully developed paths.

Enhancement Opportunities: Landscaping can be enhanced west of the racquet club only. Irrigation is fairly new and in good shape. Asphalt pathway west of the racquet club can be widened to 16' with minor rehab of the existing asphalt. Provide new neighborhood connection at Green Things, on the north bank extent as part of this project. Trail signage installation required.

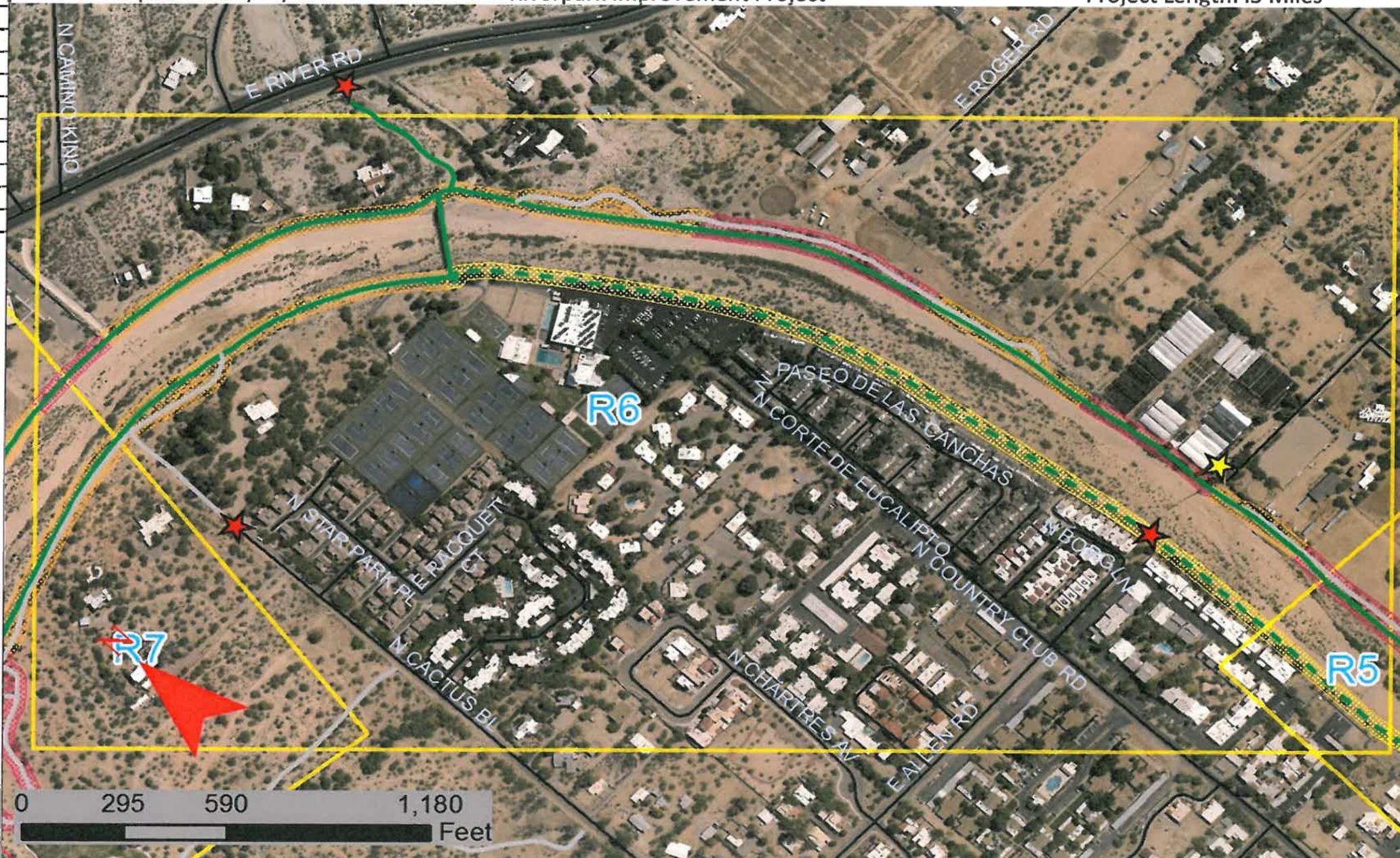
Estimated Cost:

Landscape	\$25,000.0
Irrigation	\$0.0
Ashpalt (& DG)	\$75,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$30,000.0
Total	\$145,000.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .5 Miles



Existing Conditions:

Landscaping:	C/ D
Irrigation:	None
Asphalt Cond. DG Path	12' 8' for part
Connectivity:	1E NC, 1N NC
Bridge:	1 - Pass
Underpass:	N/A

Comments: No veg due to slope restriction north of path. Small segment C. No irrigation. Path at rail. 12' paved path w/ no d.g. path Country Club Bridge to west edge Al-Huda School: ~1,400 feet. 12' paved and 8' dg in future at senior center adjacent to the river park. Existing bridge is 11'6".

Enhancement Opportunities: This section of riverpath has very little available space due to site constraints for enhancement.

A potential new neighborhood connection is identified, but should be developed with a larger project. For purposes of this study; this connection will be included with the southbank work.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructio

Rillito River Park: Hacienda del Sol to Campbell - Map R7

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .75 Miles

South Bank

Existing Conditions:

Landscaping:	good
Irrigation:	fair - good
Asphalt Cond. DG Path	12' 8'
Connectivity:	excellent
Bridge:	1 - pass
Underpass:	N/A

Comments: Reclaimed - 2 Meters supply Stone to Cactus Blvd. Irrigation system is less than 5 years old in back of Hacienda del Sol Apartments. This segment has a 12' paved path and an 8' soft path in place. Existing path can be widened to south.

Enhancement Opportunities: Landscape and irrigation is fairly new. Provide some additional shade canopy landscaping. Asphalt is also recently installed at 12'. Provide widened path with minor rehabilitation to asphalt. Provide trail signage installation.

Estimated Cost:

Landscape	\$19,000.0
Irrigation	\$0.0
Ashpalt (& DG)	\$113,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$35,000.0
Total	\$167,000.0

North Bank

Existing Conditions:

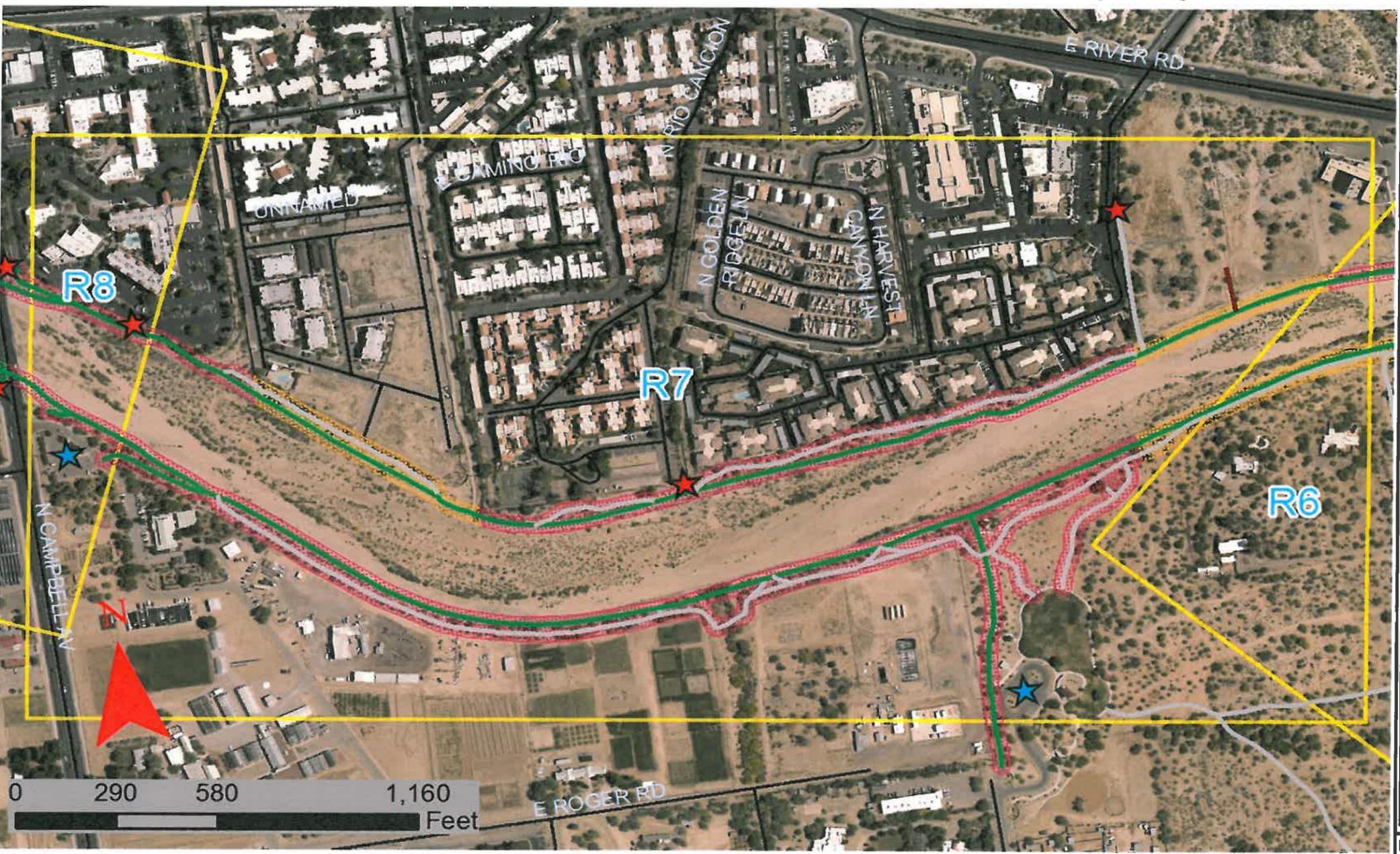
Landscaping:	good
Irrigation:	fair
Asphalt Cond. DG Path	10' to 12' 8' for part
Connectivity:	excellent
Bridge:	2 - pass
Underpass:	N/A

Comments: Reclaimed - 2 Meters supply Stone to Hacienda del Sol. There is a 12' paved path and a 8' d.g. path for most of this section, from Hacienda del Sol to the eastern edge of St. Phillips, a distance of almost 3,000'. At St. Phillips, there is a 10' path centered in the river park by itself until the bridge, a distance of approximately 620'. This area is not wide enough to provide dual path installation.

Enhancement Opportunities: Provide vegetation enhancement and upgrade irrigation as needed. Widen Asphalt path and provide rehabilitation where space allows. Provide trail signage installation, but include extent from Country Club to Campbell.

Estimated Cost:

Landscape	\$26,000.0
Irrigation	\$26,000.0
Ashpalt (& DG)	\$113,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$60,000.0
Total	\$225,000.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)

- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Constructio

South Bank

Rillito River Park: Campbell to Mountain - Map R8

North Bank

Existing Conditions:

Landscaping:	C
Irrigation:	good
Asphalt Cond. DG Path	8' to 9' 3' to 8'
Connectivity:	1E TH, 3E NC
Bridge:	N/A
Underpass:	1 - pass

Comments: At Campbell C is very mature. Reclaimed - 2 Meters supply Stone to Cactus Blvd. Irrigation system recently upgraded in this area. The paved path is almost 9' wide, mostly at rail. 3' to 6' wide soft path adjacent for 1,500 feet west of Campbell. Remaining 1,000 feet to Mountain 8' d.g. path is separate.

Enhancement Opportunities: This is a recently enhanced portion, no upgrades are suggested at this time.

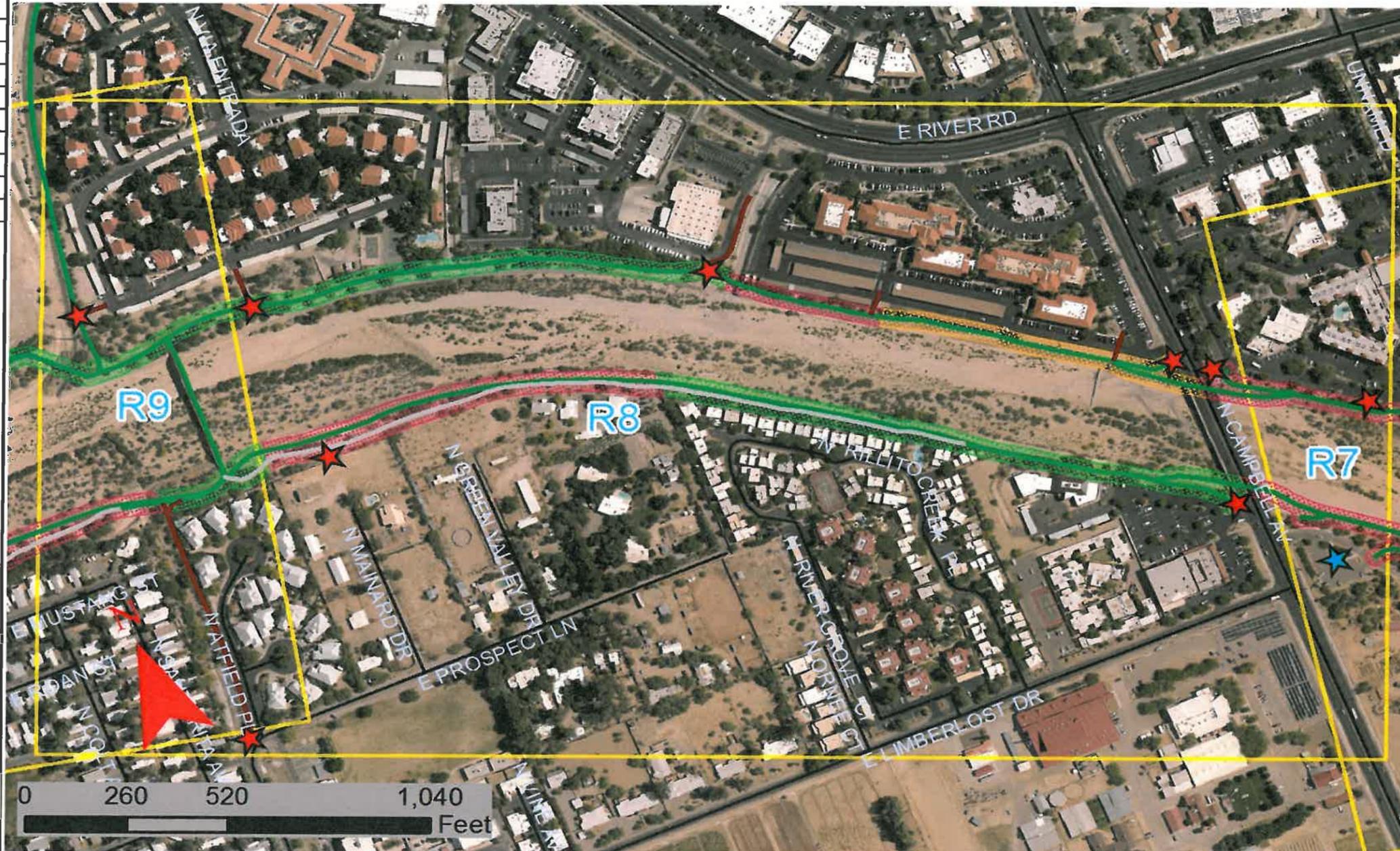
Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .5 Miles



Existing Conditions:

Landscaping:	B, C, D
Irrigation:	fair
Asphalt Cond. DG Path	9' to 10' not installed
Connectivity:	5E NC
Bridge:	1 - fail
Underpass:	1 - pass

Comments: Includes ideal segment of existing mature canopy B on both sides path which is off-set from rail. Reclaimed - 2 Meters supply Stone to Hacienda del Sol. A 10' paved path is in place on the north side (9' for the first 1,000 feet west of Campbell), with no soft path in this segment. Existing underpass width is adequate.

Enhancement Opportunities: Replace irrigation and provide additional landscape trees if pathway work disturbs existing canopy. Remove asphalt and replace. If the same path alignment is used, can fit within cleared area. Mill and replace underpass paving at Campbell underpass. Provide trailhead signage.

Estimated Cost:

Landscape	\$10,000.0
Irrigation	\$40,000.0
Ashpalt (& DG)	\$150,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$20,000.0
Other	\$70,000.0
Total	\$290,000.0

Map Legend:

Street	—
Map Sheet Reference	□
Open Pathway (Asphalt)	—
Open Pathway (Decomposed Granite)	—
Conceptual Pathway (Asphalt)	- - -
Planned Pathway (Asphalt)	- - -
Existing Trailhead	▲
Proposed Trailhead	▲
Existing Neighborhood Connection	—
Proposed Neighborhood Connection	—
Access Pathway	—

Abbreviations:

A - Premium, 100% to 81% Tree Canopy	■
B - High, 51% to 80% Tree Canopy	■
C - Medium, 21% to 50% Tree Canopy	■
D - Low, 0% to 20% Tree Canopy	■
E - Undeveloped, in Planning or Construction	■

South Bank

Rillito River Park: Mountain to First Avenue - Map R9

North Bank

Existing Conditions:

Landscaping:	C
Irrigation:	excellent
Asphalt Cond. DG Path:	11' 8' for most
Connectivity:	4E NC
Bridge:	2 - pass
Underpass:	N/A

Comments: Reclaimed - 2 Meters supply Stone to Cactus Blvd. Irrigation recently upgraded including main line replacement. Both paths are present: an 11' paved path at rail, and an 8+ soft one, for just about the whole segment. Dual bridge installation meets requirements.

Enhancement Opportunities: Extent was installed in 2011, recommend no enhancement in this area.

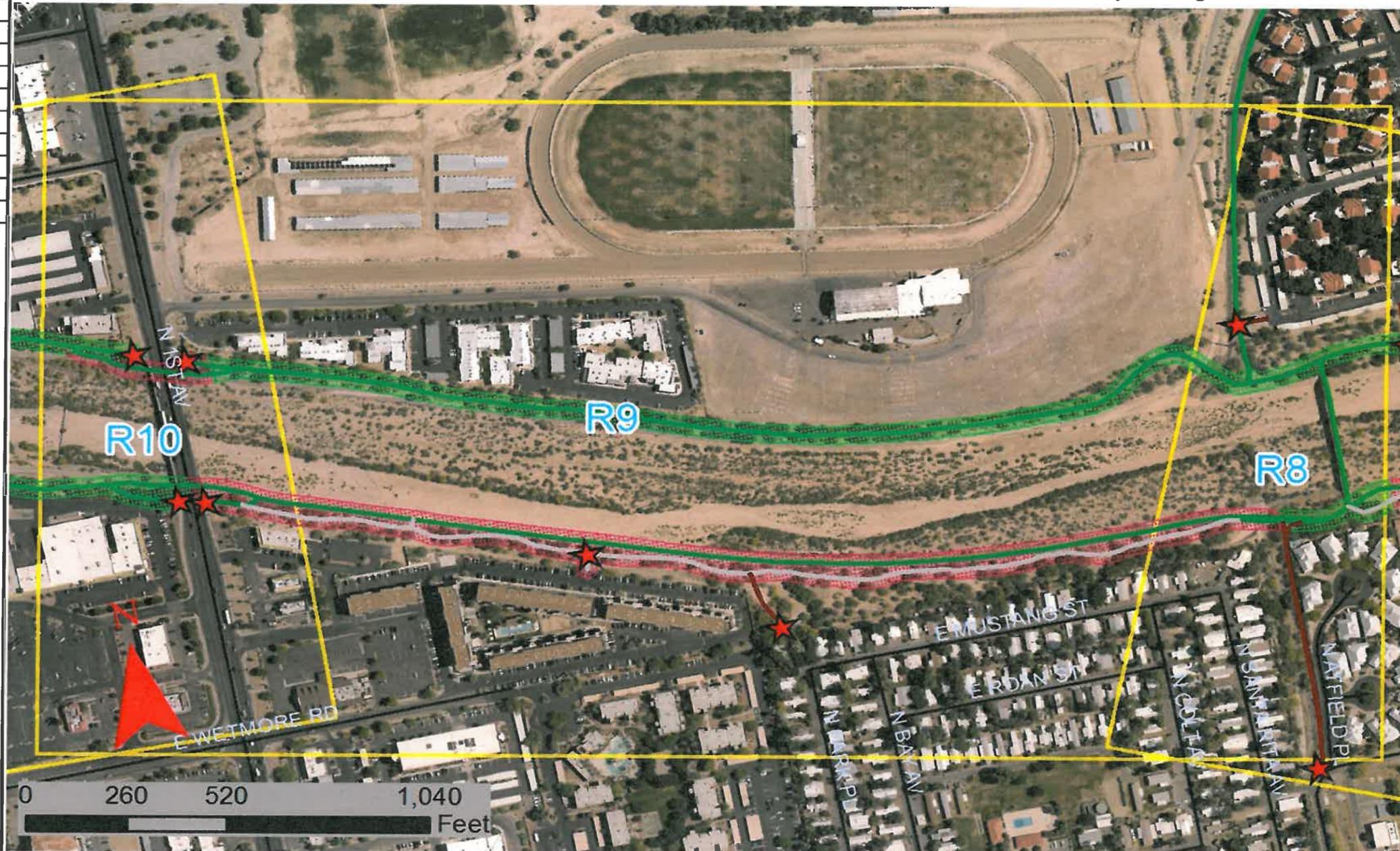
Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .5 Miles



Existing Conditions:

Landscaping:	B, C
Irrigation:	fair
Asphalt Cond. DG Path:	10' not installed
Connectivity:	2E NC
Bridge:	1 - fail
Underpass:	N/A

Comments: Landscape - entire segment: ideal condition of mature canopy, path off-set from rail w/ areas including soft path. Reclaimed - 2 Meters supply Stone to Hacienda del Sol. This segment features a 10' paved path with two foot DG shoulders and no soft path. Bridge is 8' wide.

Enhancement Opportunities: Landscape canopy is excellent, irrigation requires upgrades. Landscape may need enhancement due to disturbance of pathway. Remove and replace path with full width, install separated DG path that winds significantly through existing vegetation. Provide new trailhead parking w/ asphalt paving at Rillito Park. Provide trail signage installation.

Estimated Cost:

Landscape	\$10,000.0
Irrigation	\$40,000.0
Ashpalt (& DG)	\$150,000.0
Trailhead/ RR	\$100,000.0
Node	\$0.0
Fencing	\$175,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$190,000.0
Total	\$665,000.0

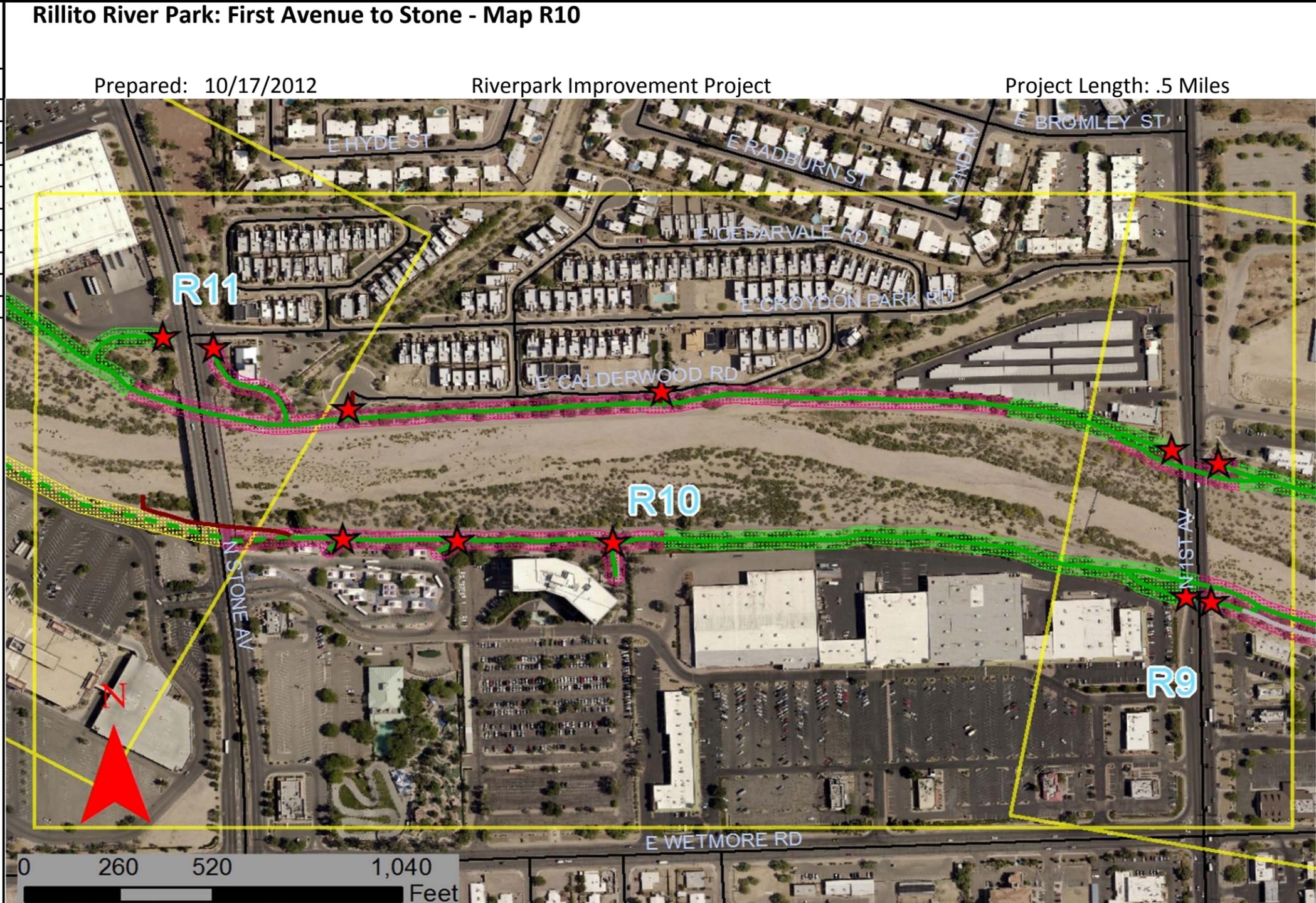
Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

South Bank	
Existing Conditions:	
Landscaping:	B, C
Irrigation:	fair
Asphalt Cond.	9' to 13'
DG Path	not installed
Connectivity:	3E NC
Bridge:	N/A
Underpass:	1 - fail
<p>Comments: Reclaimed - 2 Meters supply Stone to Cactus Blvd. This segment includes only a recently-paved 13' paved path, nicely off-set from the rail, for most of the way. The path drops to about 9' in width at the west (Stone) end, next to the bus facility. The underpass width is less than 12'.</p>	
<p>Enhancement Opportunities: The asphalt path was recently installed at 13', with little room for additional width without disturbing extensive foliage. The underpass at First Ave requires widening to minimum 12' width. Provide trail signage installation.</p>	



Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$85,000.0
	\$285,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

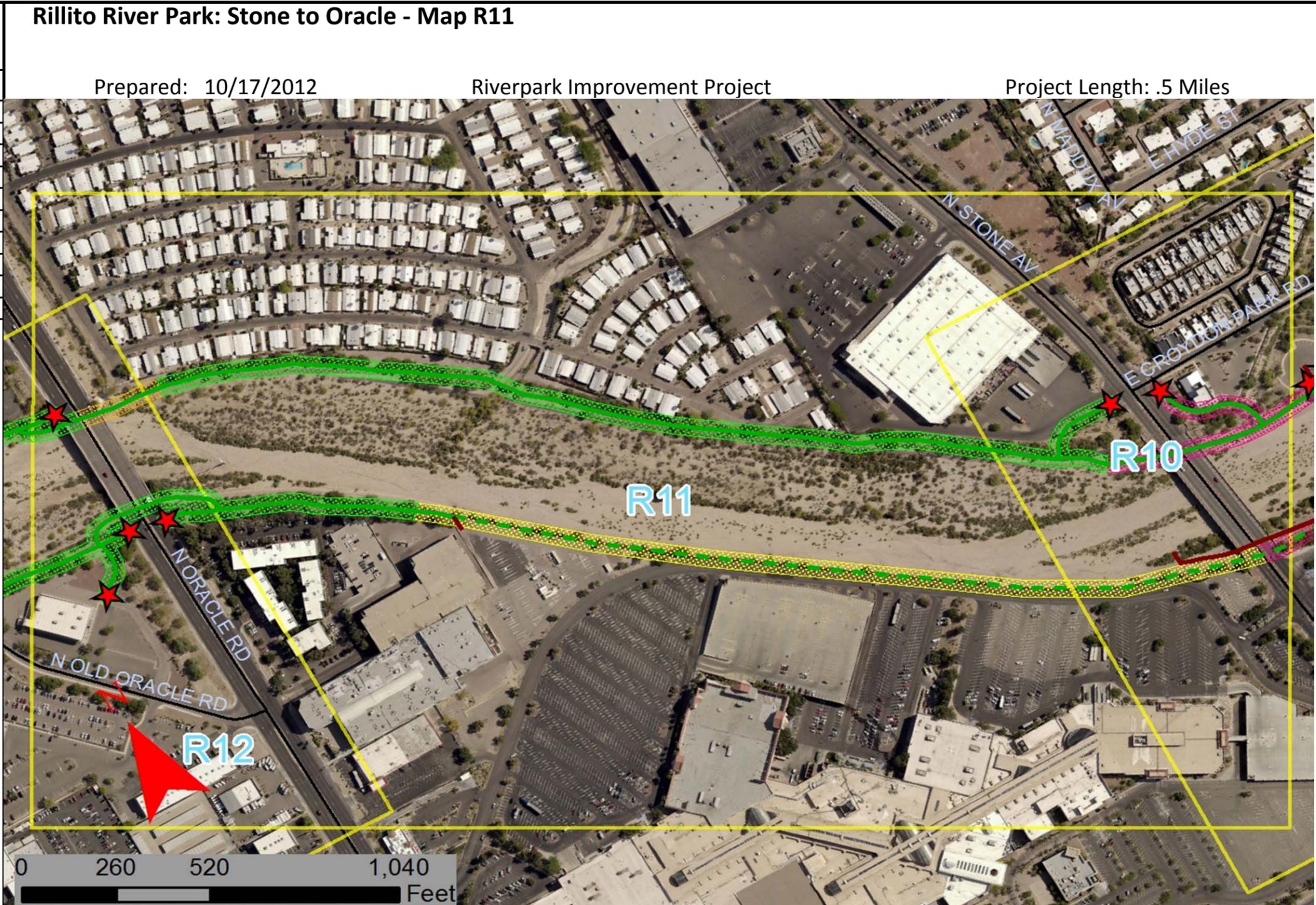
Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

North Bank	
Existing Conditions:	
Landscaping:	C, small B
Irrigation:	fair
Asphalt Cond.	10'
DG Path	not installed
Connectivity:	4E NC
Bridge:	1 - fail
Underpass:	1 - fail
<p>Comments: Typically, path off-set from rail. Reclaimed - 2 Meters supply Stone to Hacienda del Sol. This segment has one 10' paved path centered in a corridor. The racetrack wash bridge is under 8' wide. The underpass width is less than 12'.</p>	
<p>Enhancement Opportunities: Remove and replace existing asphalt path, widen using existing shoulders. Replace aging irrigation system and enhance vegetation where possible. Replace existing racetrack wash bridge. Widen existing underpass at First Ave. Provide trail signage installation.</p>	

Estimated Cost:	
Landscape	\$10,000.0
Irrigation	\$40,000.0
Ashpalt (& DG)	\$150,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$385,000.0
Underpass	\$200,000.0
Other	\$320,000.0
	\$1,105,000.0

South Bank	
Existing Conditions:	
Landscaping:	E, B
Irrigation:	fair
Asphalt Cond.	12' to 13' for part
DG Path	not installed
Connectivity:	3E NC
Bridge:	N/A
Underpass:	1 - fail
<p>Comments: Reclaimed - 2 Meters supply La Canada to Stone. For 2,200 west of Stone this site is owned by Tucson Mall and not developed. For last 800 feet to Oracle path is 12' to 13', no d.g. path. One of oldest riverpark segments- very mature canopy.</p>	
<p>Enhancement Opportunities: No landscaping or pathway changes recommended. Recently installed path at Oracle Road uses all available non-vegetated space. Underpass at Stone requires widening. Provide trailhead signage installation.</p>	



North Bank	
Existing Conditions:	
Landscaping:	B
Irrigation:	fair
Asphalt Cond.	10'
DG Path	not installed
Connectivity:	1E NC
Bridge:	1 - fail
Underpass:	1 - pass
<p>Comments: Nice mature canopy. Reclaimed - 2 Meters supply La Canada to Stone including Children's Memorial Park. Includes only 10' paved path, often off-set from rail.</p>	
<p>Enhancement Opportunities: Remove and replace asphalt with wider cross section. Replace irrigation and enhance disturbed landscape as needed. Provide replacement bridge at Friendly Village Wash. Provide upgraded drainage improvements to pathway connection to Oracle Road. Provide trail signage installation.</p>	

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$86,000.0
Total	\$286,000.0

Map Legend:

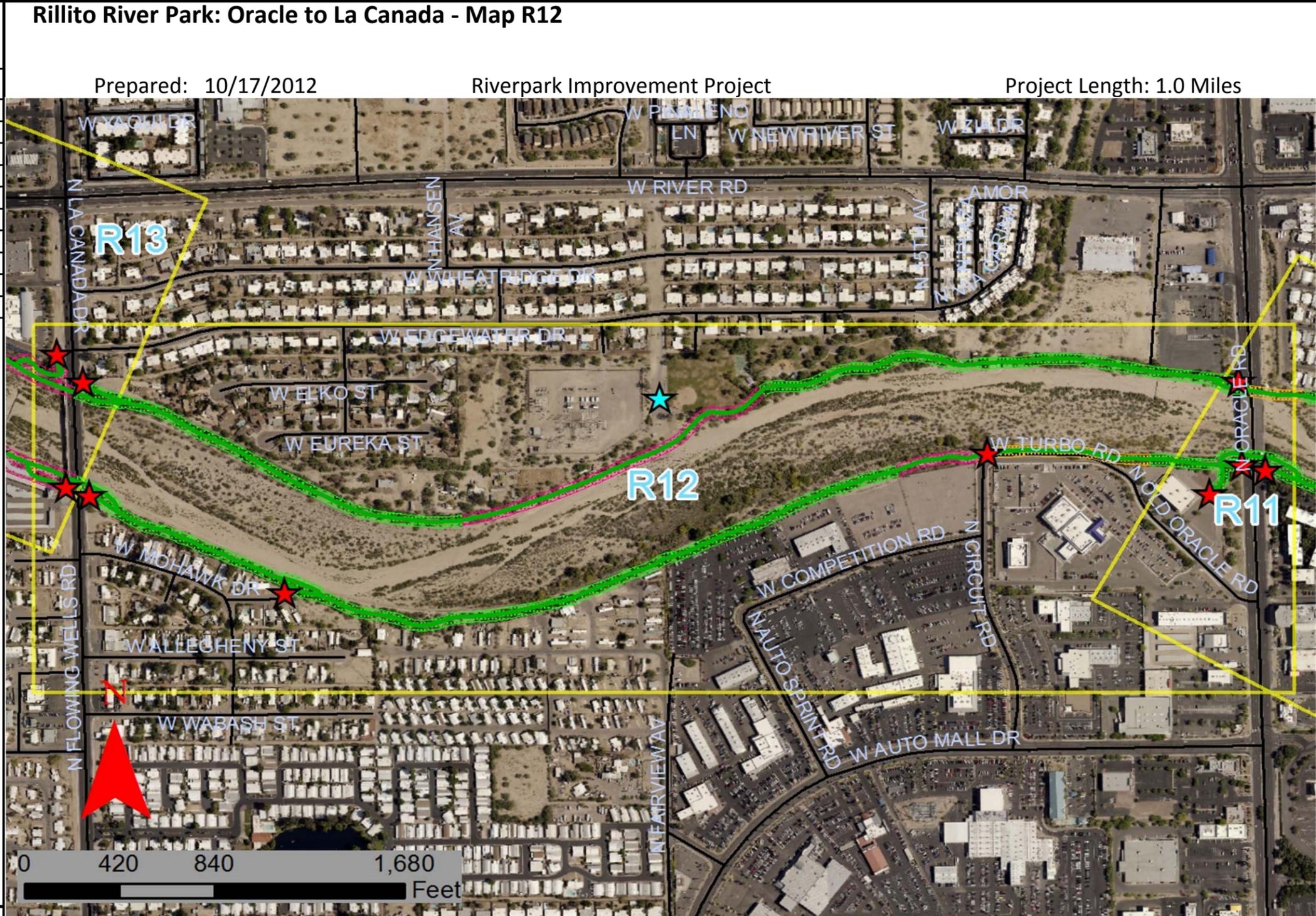
- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:	
Landscape	\$10,000.0
Irrigation	\$40,000.0
Ashpalt (& DG)	\$150,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$40,000.0
Bridge	\$135,000.0
Underpass	\$0.0
Other	\$150,000.0
Total	\$525,000.0

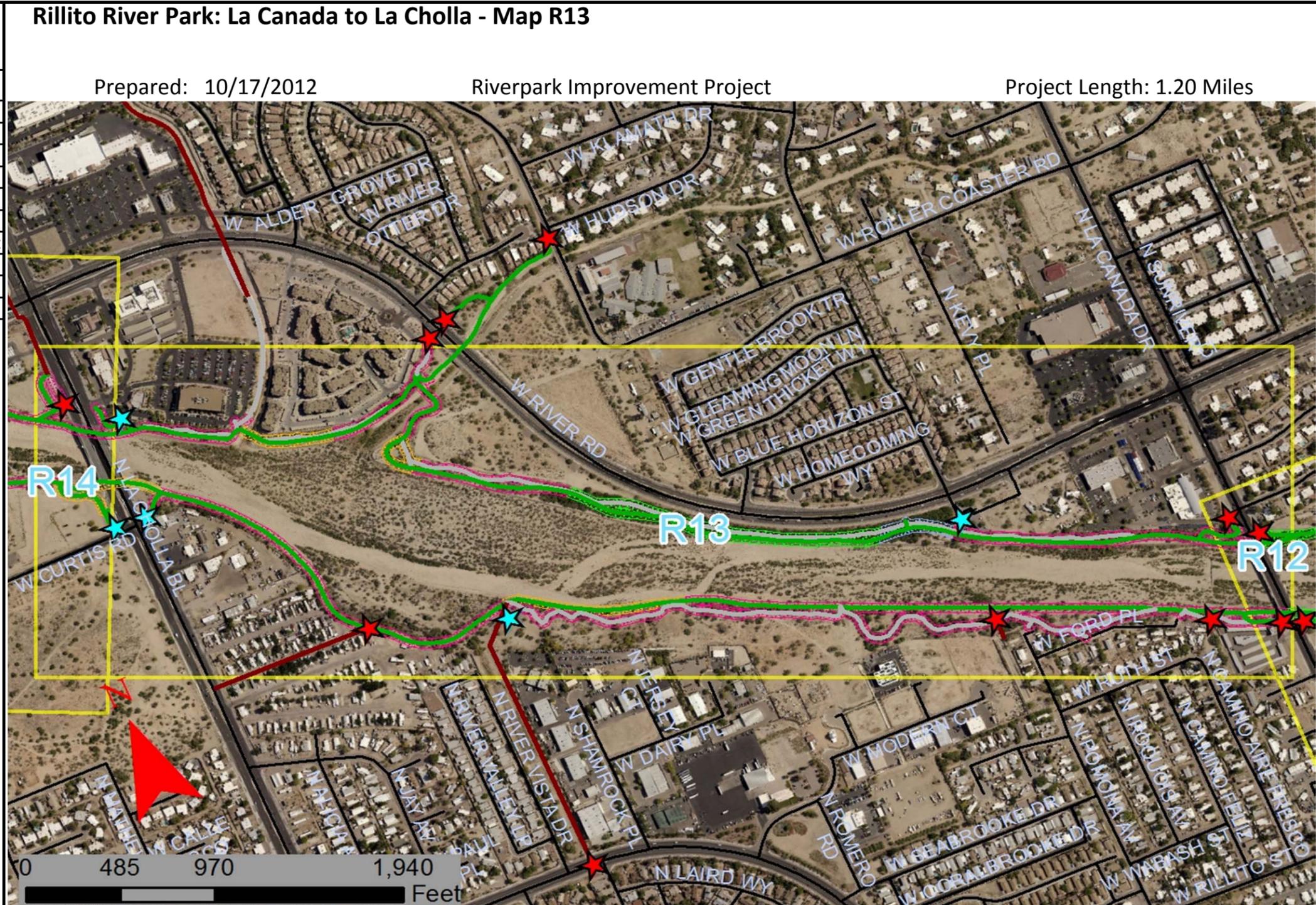
South Bank	
Existing Conditions:	
Landscaping:	Mostly B; C, D
Irrigation:	fair
Asphalt Cond.	10 to 13
DG Path	not installed
Connectivity:	4E NC
Bridge:	N/A
Underpass:	1 - fail
<p>Comments: Reclaimed - 2 Meters supply La Canada to Stone. The only path that exists in this segment is the paved path. It runs from 13' to 10 and back to 12' and is mostly off-set from rail. Underpass at Oracle Road requires widening.</p>	
<p>Enhancement Opportunities: Newly installed paving, no upgrades recommended. Provide new irrigation mainline and minor landscape enhancement. Provide upgrades to existing neighborhood connections along extents (4). Provide underpass widening at Oracle Road. Install erosion control, rip rap in areas as needed. Provide trail signage installation.</p>	
Estimated Cost:	
Landscape	\$15,000.0
Irrigation	\$85,000.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$45,000.0
Fencing	\$0.0
Drainage	\$20,000.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$135,000.0
	\$500,000.0



Map Legend:		Abbreviations:	
— Street	▲ Existing Trailhead	■ A - Premium, 100% to 81% Tree Canopy	
□ Map Sheet Reference	▲ Proposed Trailhead	■ B - High, 51% to 80% Tree Canopy	
— Open Pathway (Asphalt)	▲ Existing Neighborhood Connection	■ C - Medium, 21% to 50% Tree Canopy	
— Open Pathway (Decomposed Granite)	▲ Proposed Neighborhood Connection	■ D - Low, 0% to 20% Tree Canopy	
- - - Conceptual Pathway (Asphalt)	▲ Access Pathway	■ E - Undeveloped, in Planning or Construction	
- - - Planned Pathway (Asphalt)			

North Bank	
Existing Conditions:	
Landscaping:	Mostly B; C
Irrigation:	fair
Asphalt Cond.	10'
DG Path	not installed
Connectivity:	1E TH, 2E NC
Bridge:	1 - fail
Underpass:	1 - fail
<p>Comments: Ideal segment for canopy enhancement because path is off-set from rail and good percentage canopy mature. Good interface with Children's Memorial. Reclaimed - 2 Meters supply La Canada to Stone including Children's Memorial Park. This segment includes only a 10' paved pathway. Existing bridge is 8' wide.</p>	
<p>Enhancement Opportunities: Remove and replace asphalt and widen. Provide separated DG path where space allows around existing landscape. Provide new irrigation mainline and minor landscape enhancement if disturbed. Provide significant trailhead rehabilitation (paving improvements) at park. Provide replacement bridge at Pima Wash. Provide underpass widening at Oracle Road.</p>	
Estimated Cost:	
Landscape	\$15,000.0
Irrigation	\$85,000.0
Ashpalt (& DG)	\$300,000.0
Trailhead/ RR	\$50,000.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$200,000.0
Underpass	\$200,000.0
Other	\$345,000.0
	\$1,195,000.0

South Bank	
Existing Conditions:	
Landscaping:	Mostly C; and D, B
Irrigation:	fair
Asphalt Cond.	12'
DG Path	8' for most
Connectivity:	1E TH, 4E NC, 1N NC
Bridge:	N/A
Underpass:	1 - fail
<p>Comments: Reclaimed - 3 Meters supply La Canada to Shannon - looped system. New 12' paved path and 8' d.g. path most of way from La Canada west. Missing 700 feet of d.g. path from River Vista west. La Canada underpass requires widening.</p>	
<p>Enhancement Opportunities: Recently installed path is over 12', recommend no change until pathway pavement is older. Replace irrigation and enhance corridor with trees. Provide upgrades to node at trailer home park. Repave existing trailhead parking lot. Widen underpass at La Canada. Provide trailhead signage installation.</p>	



Estimated Cost:	
Landscape	\$25,000.0
Irrigation	\$95,000.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$20,000.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$135,000.0
Total	\$490,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

	A - Premium, 100% to 81% Tree Canopy
	B - High, 51% to 80% Tree Canopy
	C - Medium, 21% to 50% Tree Canopy
	D - Low, 0% to 20% Tree Canopy
	E - Undeveloped, in Planning or Construction

North Bank	
Existing Conditions:	
Landscaping:	Small A; B, C, D
Irrigation:	fair
Asphalt Cond.	10'
DG Path	8' for part
Connectivity:	2E TH, 4E NC
Bridge:	2 - fail
Underpass:	1 - fail
<p>Comments: Reclaimed - 3 Meters supply La Canada to Shannon and line crosses over the La Cholla bridge to supply the south bank to Camino de la Tierra including Flowing Wells and Curtis Parks - looped system. 10' paved and 8' d.g. starts 600 feet west of La Canada and goes almost to end.</p>	
<p>Enhancement Opportunities: Remove and replace asphalt path with widened cross section. Replace irrigation and enhance corridor with trees where disturbed or below standard. Enhance La Canada access point. Repave trailhead parking lot. Widen underpass at La Canada. Install two new bridges, at Rollercoaster wash and Casas Adobes Wash. Provide trail signage installation.</p>	

Estimated Cost:	
Landscape	\$30,000.0
Irrigation	\$90,000.0
Ashpalt (& DG)	\$360,000.0
Trailhead/ RR	\$20,000.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$245,000.0
Underpass	\$200,000.0
Other	\$400,000.0
Total	\$1,360,000.0

South Bank

Existing Conditions:

Landscaping:	good
Irrigation:	fair
Asphalt Cond.	10' to 12'
DG Path	not installed
Connectivity:	4E TH, 1E NC
Bridge:	1 - pass
Underpass:	1 - pass

Comments: Reclaim La Cholla - CDLT. Potable CDLT to railroad bridge (reclaim line at capacity). The paved path varies from 10' to 12' feet several times through the corridor.

Enhancement Opportunities: Paved path requires upgrades and enhancement; can widen where applicable. Replace irrigation and enhance tree canopy only as needed. Provide upgrades and maintenance to local nodes. Provide trail signage installation.

Estimated Cost:

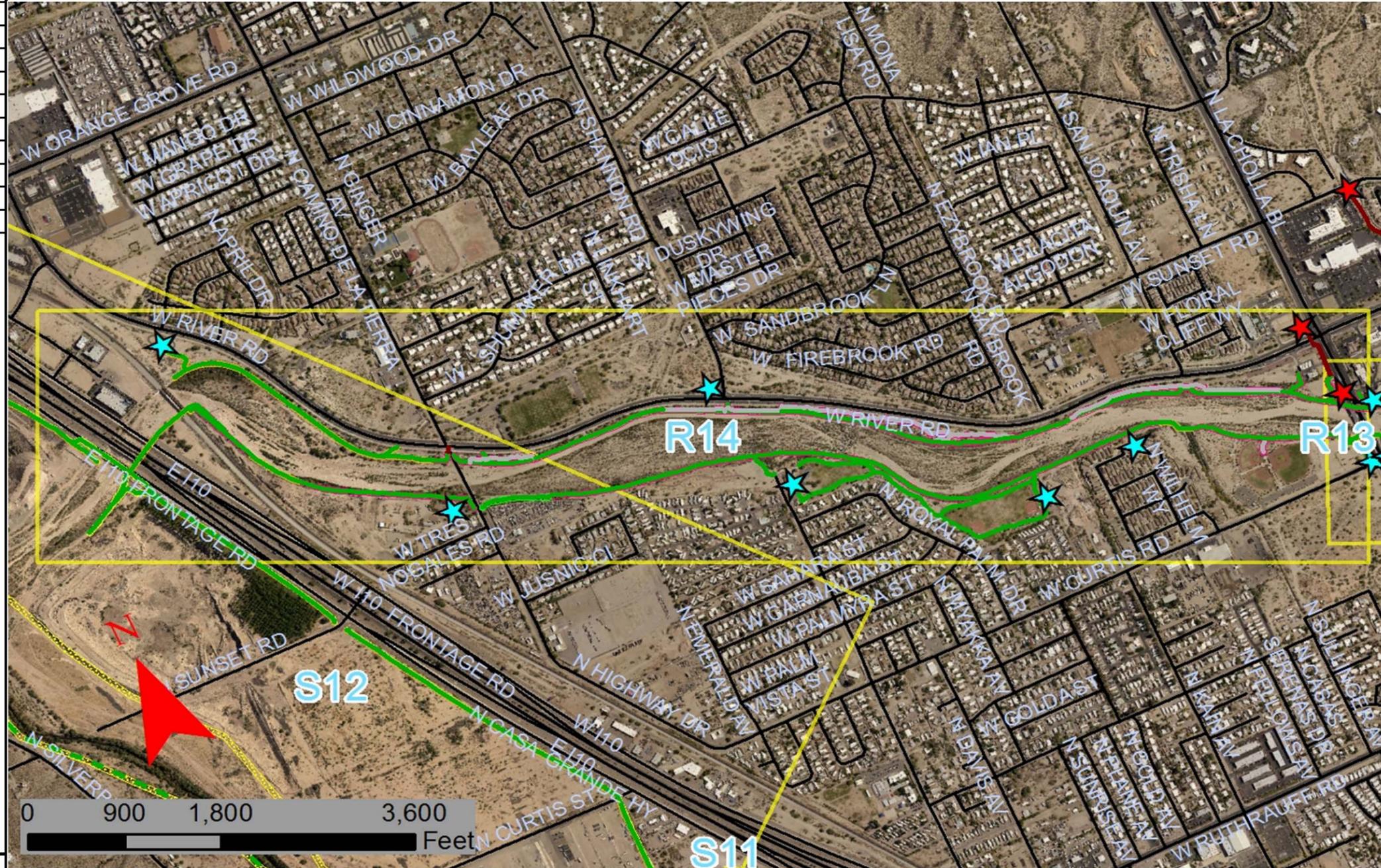
Landscape	\$45,000.0
Irrigation	\$115,000.0
Ashpalt (& DG)	\$240,000.0
Trailhead/RR	\$0.0
Node	\$45,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$110,000.0
Total	\$555,000.0

Rillito River Park: La Cholla to I-10 - Map R14

Prepared: 10/17/2012

Riverpark Improvement Project

Project Length: 1.60 Miles



Map Legend:

- Street
- ▭ Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

North Bank

Existing Conditions:

Landscaping:	good
Irrigation:	fair
Asphalt Cond.	12' to 13'
DG Path	8' for most
Connectivity:	2E TH, 1E NC
Bridge:	2 - pass
Underpass:	1 - pass

Comments: Reclaim to Shannon, non-potable Shannon to CDLT, and no irrigation CDLT - I-10. New 12' to 13' paved path; 8' d.g. path goes to CDLT but west of there no d.g. path.

Enhancement Opportunities: Recently installed asphalt, recommend no upgrade until asphalt ages. Provide new irrigation and minimal tree enhancement as needed. Provide upgrades and maintenance to Shannon Road connection. Provide trail signage installation.

Estimated Cost:

Landscape	\$45,000.0
Irrigation	\$115,000.0
Ashpalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$50,000.0
Total	\$225,000.0

South Bank

Tanque Verde Wash: Pantano Road to Sabino Canyon Road - Map T1

North Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	excellent
Bridge:	N/A
Underpass:	failing

Comments:
N/A - Not yet built.

Enhancement Opportunities:
Possible opportunity for riparian restoration if ROW acquired.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: X Miles

Existing Conditions:

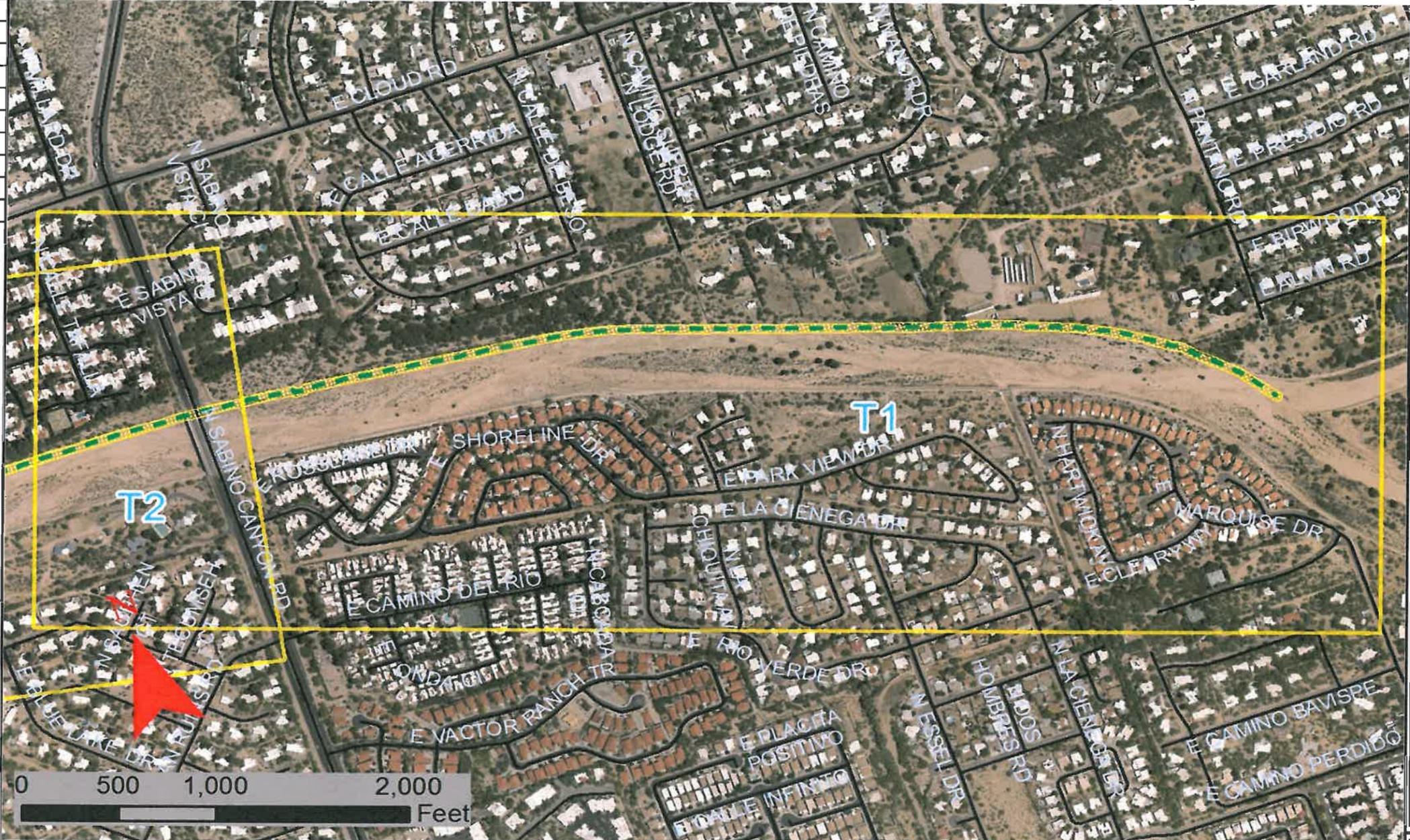
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	excellent
Bridge:	N/A
Underpass:	failing

Comments:
N/A - Not yet built.

Enhancement Opportunities:
Possible opportunity for riparian restoration if ROW acquired.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Ashpalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0



Map Legend:

	Street		Existing Trailhead
	Map Sheet Reference		Proposed Trailhead
	Open Pathway (Asphalt)		Existing Neighborhood Connection
	Open Pathway (Decomposed Granite)		Proposed Neighborhood Connection
	Conceptual Pathway (Asphalt)		Access Pathway
	Planned Pathway (Asphalt)		

Abbreviations:

	A - Premium, 100% to 81% Tree Canopy
	B - High, 51% to 80% Tree Canopy
	C - Medium, 21% to 50% Tree Canopy
	D - Low, 0% to 20% Tree Canopy
	E - Undeveloped, in Planning or Construction

One Pathway

Pantano Wash: Valencia to Irvington - Map P1.5

NA

Existing Conditions:

Landscaping:	E
Irrigation:	poor
Asphalt Cond. DG Path	12' - future not installed
Connectivity:	1N NC
Bridge:	N/A
Underpass:	N/A

Comments: Existing Desert Scrub. A 12' paved pathway will be built soon.

Enhancement Opportunities:
Existing project is planned to deliver a 12' asphalt path once state land is purchased - sometime in FY 14/15.

Other enhancements required include a DG path, landscape and irrigation. Provide trailhead at Valencia road for parking. Provide new HAWK type crossing for users at Valencia (other cost). Provide rural fence type to reduce vehicle use of path. Provide trail signage installation.

Estimated Cost:

Landscape	\$150,000.0
Irrigation	\$175,000.0
Asphalt (& DG)	\$195,000.0
Trailhead/RR	\$100,000.0
Node	\$0.0
Fencing	\$130,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$550,000.0
Total	\$1,300,000.0

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 3.25 Miles

Existing Conditions:

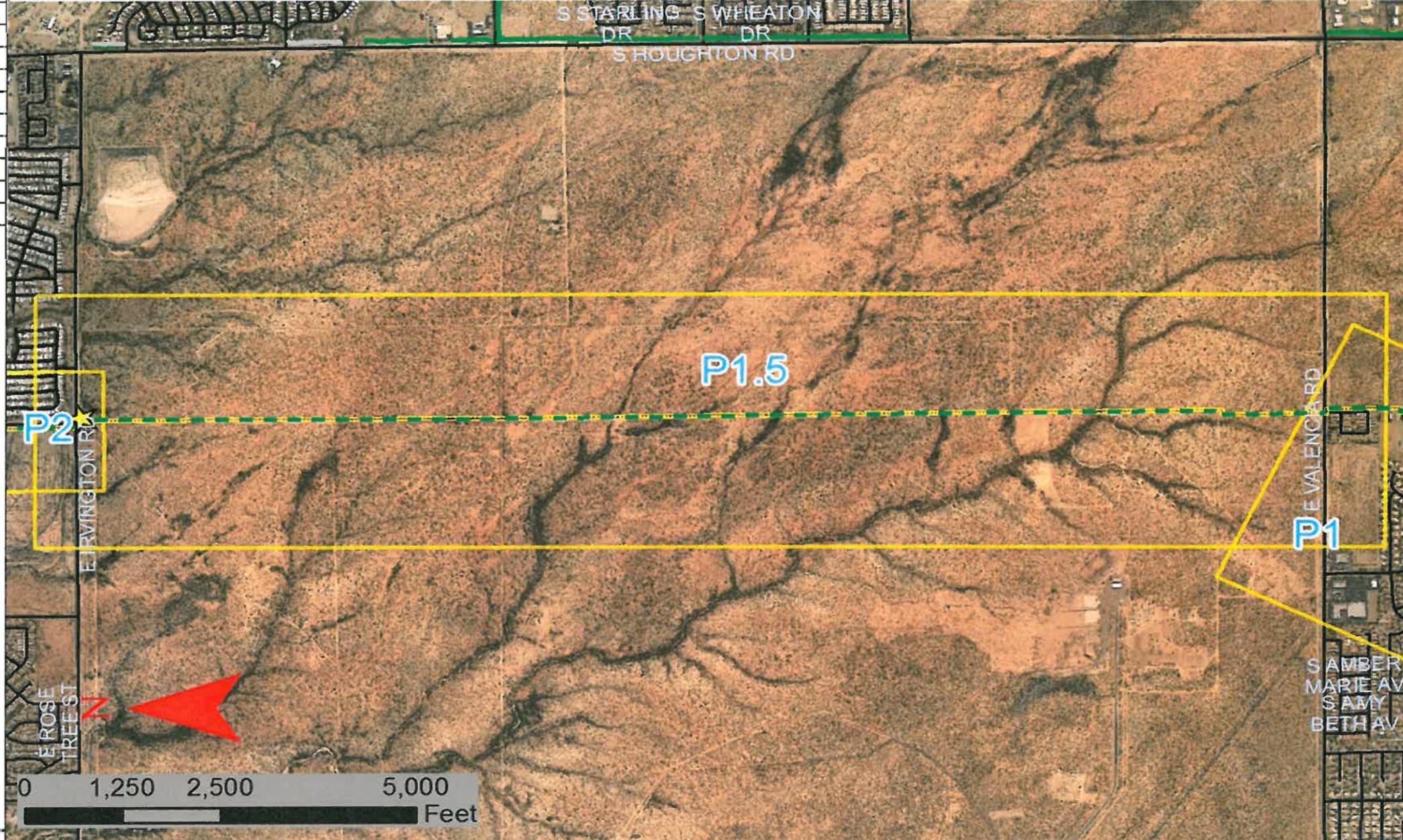
Landscaping:	
Irrigation:	
Asphalt Cond. DG Path	
Connectivity:	
Bridge:	
Underpass:	

Comments:

Enhancement Opportunities:

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank

Existing Conditions:

Landscaping:	C & small B
Irrigation:	fair
Asphalt Cond.	12'
DG Path	8'
Connectivity:	4E NC, 1N NC
Bridge:	N/A
Underpass:	1 - fail

Comments: "Borrowed" canopy from private property. A 12' paved path and an 8' soft path are in place.

Enhancement Opportunities: repair existing irrigation system and enhance landscape at Pantano Edge subdivision. Rehabilitate pathway surfaces at Pantano Edge subdivision. Install new neighborhood connection at Camino Seco (will require ROW purchase) and rehabilitate existing connections. Widen Golf Links underpass. Perform drainage improvements and erosion control.

Estimated Cost:

Landscape	\$10,000.0
Irrigation	\$2,500.0
Asphalt (& DG)	\$75,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$20,000.0
Drainage	\$55,000.0
Bridge	\$0.0
Underpass	\$200,000.0
Other	\$172,000.0
Total	\$549,500.0

Pantano Wash: Sellarole Road to Golf Links - Map P3

Prepared: 10/17/2012

Riverpark Improvement Project

Project Length: 1.5 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	D & C
Irrigation:	good
Asphalt Cond.	12'
DG Path	6' - 8'
Connectivity:	1E TH, 3E NC
Bridge:	N/A
Underpass:	N/A

Comments: Existing landscaping new D: minimal. C at north node. Reclaim Water line from Sellarole Rd. to Kenyon Rd.

Enhancement Opportunities: recently installed no recommended improvements at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

West Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed
Connectivity:	1N NC
Bridge:	2 - new
Underpass:	N/A

Comments:
 N/A - not developed. A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano river area. 404 permits would be required.

Enhancement Opportunities:
 limited ROW against built structures leaving no room for landscape. Extensive commercial ROW acquisition will be required to install pathways in the northern section. Install new 16 foot asphalt path with minimal DG installation. Provide drainage improvements where necessary. Provide neighborhood connection at 22nd Street and at Tucson Meadows subdivision. Will require bridges at the Atterbury and Guillermo Washes. Will need to fence off linear park corridor.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$390,000.0
Trailhead/RR	\$0.0
Node	\$30,000.0
Fencing	\$200,000.0
Drainage	\$0.0
Bridge	\$525,000.0
Underpass	\$0.0
Other	\$6,500,000.0
Total	\$7,645,000.0

Pantano Wash: Golf Links to 22nd Street - Map P4

Prepared: 10/17/2012 Riverpark Improvement Project Project Length: 1.5 Miles



Map Legend:

	Street
	Map Sheet Reference
	Open Pathway (Asphalt)
	Open Pathway (Decomposed Granite)
	Conceptual Pathway (Asphalt)
	Planned Pathway (Asphalt)
	Existing Trailhead
	Proposed Trailhead
	Existing Neighborhood Connection
	Proposed Neighborhood Connection
	Access Pathway

Abbreviations:

	A - Premium, 100% to 81% Tree Canopy
	B - High, 51% to 80% Tree Canopy
	C - Medium, 21% to 50% Tree Canopy
	D - Low, 0% to 20% Tree Canopy
	E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	C
Irrigation:	good
Asphalt Cond. DG Path	12' 8'
Connectivity:	3E TH
Bridge:	N/A
Underpass:	1 - pass

Comments:
 Much of canopy borrowed from roadway; some distant from pathway. A 12' paved path and a 8' soft path are in place. Reclaim Water line from Sellarole Rd. to Kenyon Rd.

Enhancement Opportunities:
 no recommended improvements at this time. Awaiting the commemorative tree plating installation.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

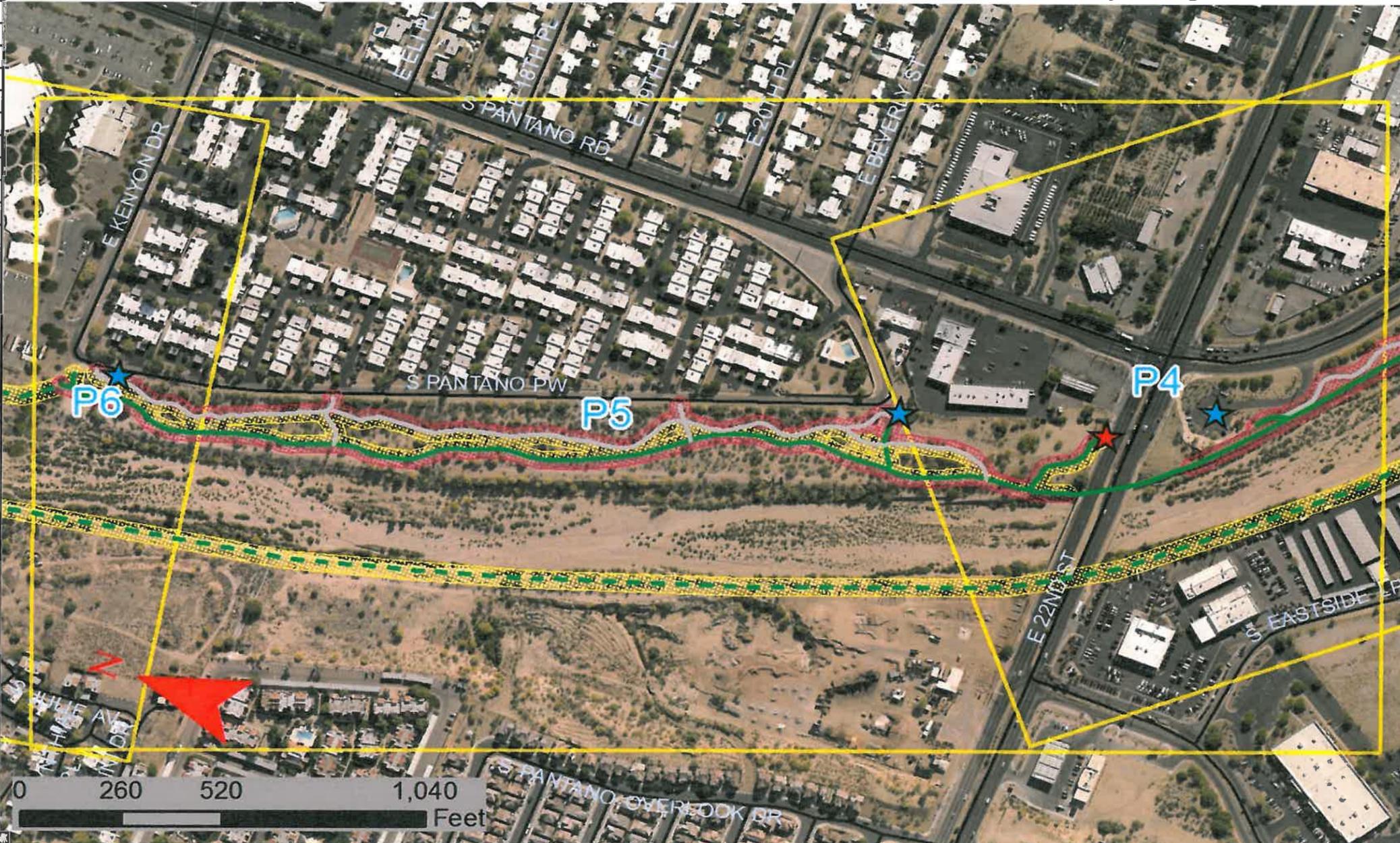
Pantano Wash: 22nd Street to Kenyon - Map P5

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: 0.5 Miles

West Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed not installed
Connectivity:	2N NC
Bridge:	N/A
Underpass:	1 - fail
<p>Comments: N/A - Undeveloped. A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required. This stretch is only partially bank protected.</p>	
<p>Enhancement Opportunities: install new bank protection. Install new irrigation and landscape. Install new 16 foot asphalt path and DG path where applicable. Provide new neighborhood connections at the YMCA and 22nd Street. Widen 22nd Street underpass. Connection at YMCA will require additional grading.</p>	



East Bank	
Existing Conditions:	
Landscaping:	C
Irrigation:	Good
Asphalt Cond. DG Path	12' 8'
Connectivity:	1E TH, 3E NC
Bridge:	N/A
Underpass:	1 - pass
<p>Comments: Spacious ROW allows development of habitat and/ or recreational nodes. There's a 12' paved and 8' soft pathway in place. Reclaim Water line from Sellarole Rd. to Kenyon Rd.</p>	
<p>Enhancement Opportunities: recently completed no recommended improvements at this time. Awaiting commemorative tree plating installation.</p>	

Estimated Cost:	
Landscape	\$20,000.0
Irrigation	\$30,000.0
Asphalt (& DG)	\$150,000.0
Trailhead/RR	\$0.0
Node	\$30,000.0
Fencing	\$20,000.0
Drainage	\$920,000.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$670,000.0
Total	\$2,140,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

West Bank

Pantano Wash: Kenyon to Broadway - Map P6

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	8' paved not installed
Connectivity:	Ext Park
Bridge:	N/A
Underpass:	N/A

Comments:
 Undeveloped except at Hilton 30% C borrowed from adjacent properties. An older section of river park that features an 8' paved path has been developed next to the Hilton Hotel. The area south of the Hilton lies adjacent to a landfill with no irrigation or landscape improvements recommended.

Enhancement Opportunities:
 Replace and install new 16 foot asphalt and DG paths. Install new connection to Hilton property. fence off landfill area. Provide trail signage.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$150,000.0
Trailhead/RR	\$0.0
Node	\$15,000.0
Fencing	\$10,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$55,000.0
Total	\$230,000.0

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 0.5 Miles

Existing Conditions:

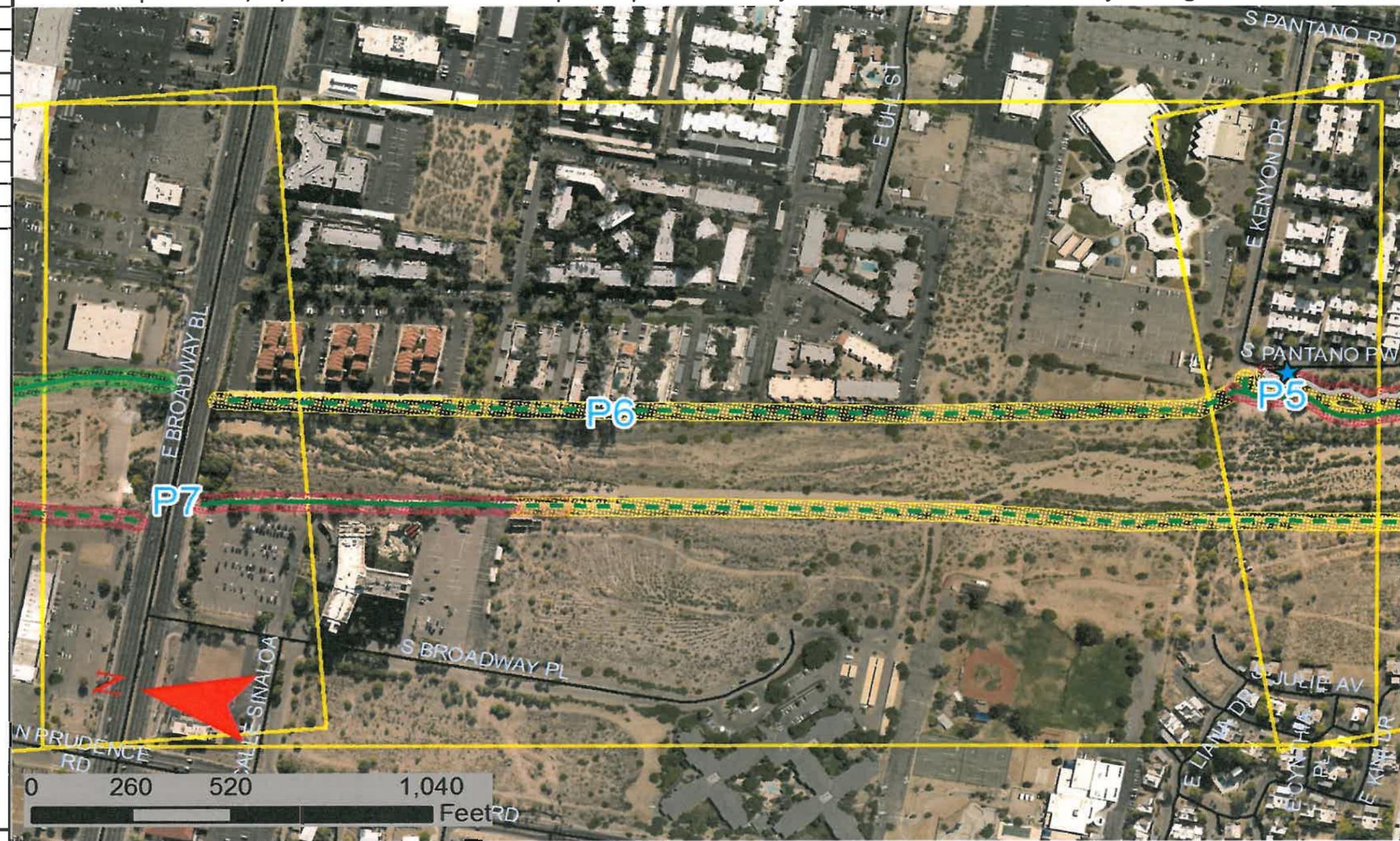
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	not installed not installed
Connectivity:	1E NC
Bridge:	1 - new
Underpass:	1 - new

Comments: Currently undeveloped with little ROW available for enhancements. A trail easement has been secured through several parcels south of Broadway but does not connect to Kenyon. There is no bank protection north of Kenyon for 850 feet.

Enhancement Opportunities:
 Acquire ROW from church properties. install irrigation and landscape at church properties only. Install asphalt path with reduced cross section north of church properties. Install new bridge over Kenyon drainage with additional drainage improvements. Install new underpass at Broadway. Provide corridor fencing.

Estimated Cost:

Landscape	\$10,000.0
Irrigation	\$15,000.0
Asphalt (& DG)	\$100,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$86,000.0
Drainage	\$60,000.0
Bridge	\$140,000.0
Underpass	\$495,000.0
Other	\$950,000.0
Total	\$1,856,000.0



Map Legend:

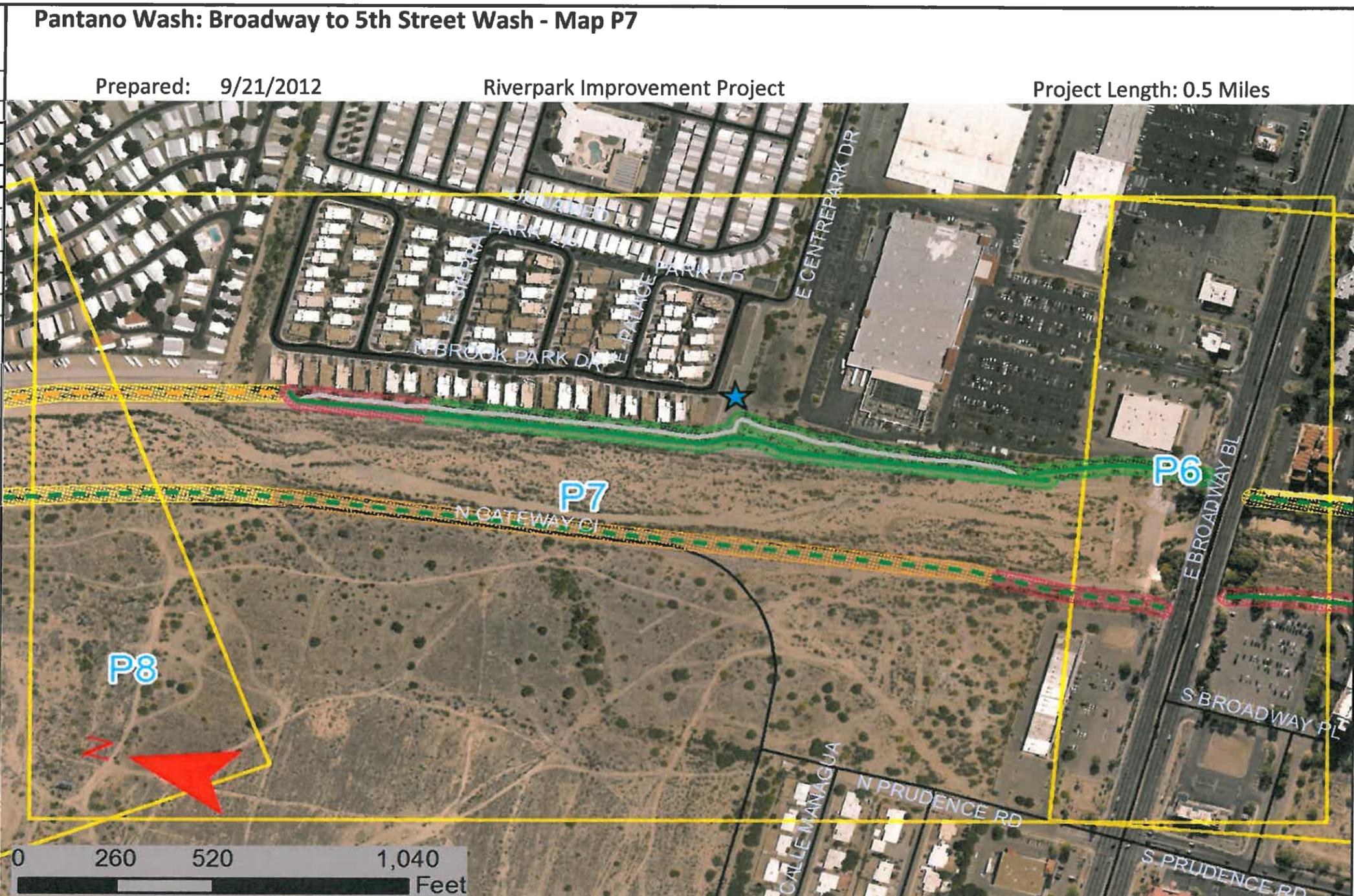
	Street
	Map Sheet Reference
	Open Pathway (Asphalt)
	Open Pathway (Decomposed Granite)
	Conceptual Pathway (Asphalt)
	Planned Pathway (Asphalt)

	Existing Trailhead
	Proposed Trailhead
	Existing Neighborhood Connection
	Proposed Neighborhood Connection
	Access Pathway

Abbreviations:

	A - Premium, 100% to 81% Tree Canopy
	B - High, 51% to 80% Tree Canopy
	C - Medium, 21% to 50% Tree Canopy
	D - Low, 0% to 20% Tree Canopy
	E - Undeveloped, in Planning or Construction

West Bank	
Existing Conditions:	
Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	12' not installed
Connectivity:	NC-Dev. to install
Bridge:	N/A
Underpass:	1 - new
<p>Comments: Landscape: Undeveloped due to private property ownership and closed landfill. Small area borrowed C at Broadway. The 12' paved path and 8' soft path are in place.</p>	
<p>Enhancement Opportunities: Rehabilitate and widen existing asphalt path and install new asphalt where applicable. Provide new Broadway underpass. install corridor fencing and signage.</p>	



Map Legend:		Abbreviations:	
	Street		A - Premium, 100% to 81% Tree Canopy
	Map Sheet Reference		B - High, 51% to 80% Tree Canopy
	Open Pathway (Asphalt)		C - Medium, 21% to 50% Tree Canopy
	Open Pathway (Decomposed Granite)		D - Low, 0% to 20% Tree Canopy
	Conceptual Pathway (Asphalt)		E - Undeveloped, in Planning or Construction
	Planned Pathway (Asphalt)		Existing Trailhead
	Existing Neighborhood Connection		Proposed Trailhead
	Proposed Neighborhood Connection		Existing Neighborhood Connection
	Access Pathway		Proposed Neighborhood Connection

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$100,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$20,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$495,000.0
Other	\$280,000.0
Total	\$895,000.0

East Bank	
Existing Conditions:	
Landscaping:	C
Irrigation:	None - good
Asphalt Cond. DG Path	12' 8'
Connectivity:	1E TH, 1E NC
Bridge:	N/A
Underpass:	N/A
<p>Comments: Established segment. Some dg path. The 12' paved path and 8' soft path are in place.</p>	
<p>Enhancement Opportunities: Northern section under development. No recommended improvements at this time.</p>	

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

West Bank

Pantano Wash: 5th Street Wash to Speedway - Map P8

East Bank

Existing Conditions:

Landscaping:	E & C
Irrigation:	None
Asphalt Cond. DG Path	9' to 14' not installed
Connectivity:	1E TH
Bridge:	1 - new
Underpass:	N/A

Comments: Recreation opportunity. At Speedway: sporadic but mature "borrowed" C from private. The paved path is 9' to 14' wide, but there's no room for the soft path. The area south of the commercial property is privately owned and is a closed landfill.

Enhancement Opportunities: Rehabilitate existing pathways and install new asphalt where applicable. Install new pedestrian bridge south of the old Isoplex. Provide drainage improvements at commercial parking area. Install corridor fencing and signage.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$150,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$14,000.0
Drainage	\$10,000.0
Bridge	\$140,000.0
Underpass	\$0.0
Other	\$135,000.0
Total	\$449,000.0

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 0.75 Miles

Existing Conditions:

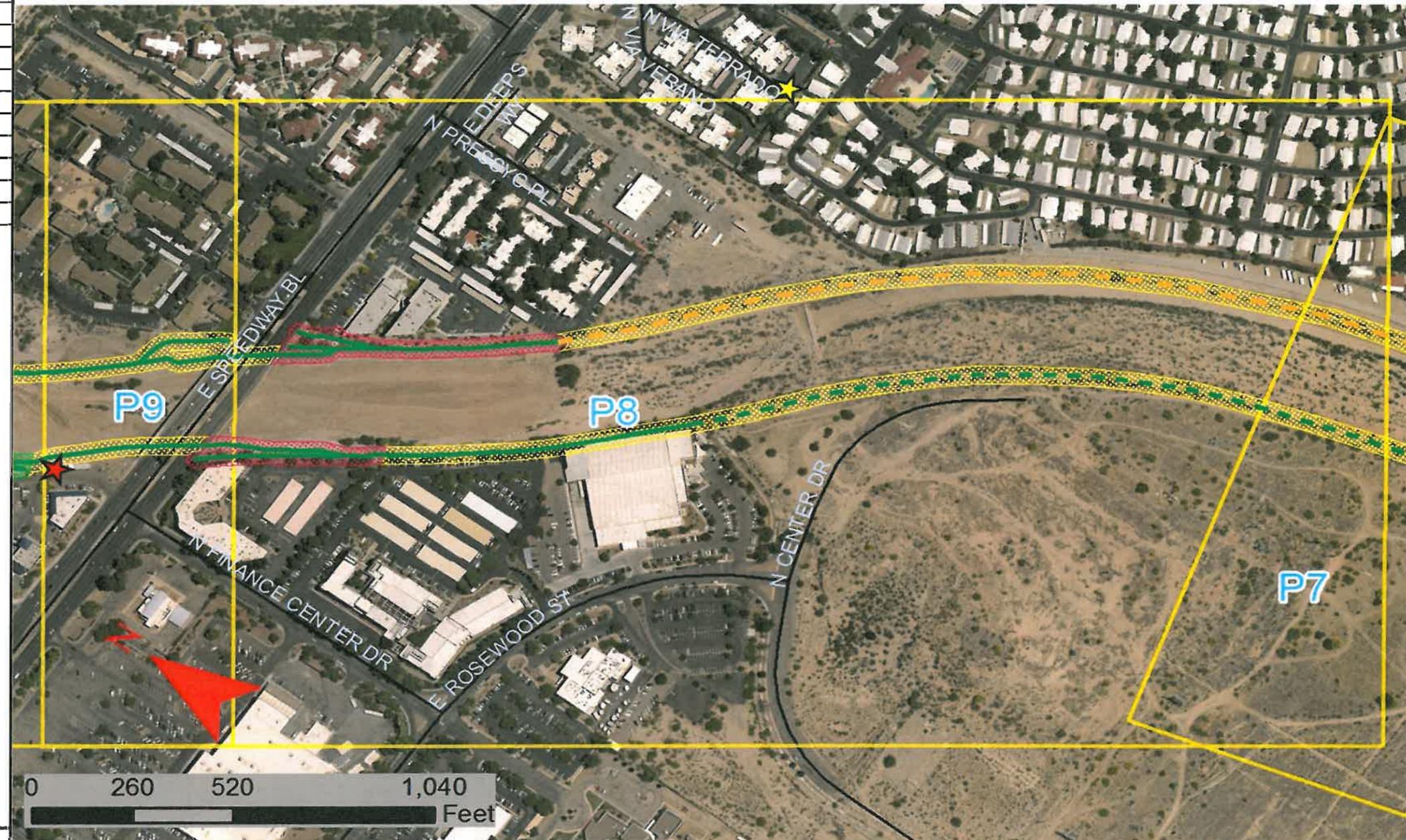
Landscaping:	E & C
Irrigation:	None
Asphalt Cond. DG Path	12' 8'
Connectivity:	1E NC
Bridge:	1 - new
Underpass:	N/A

Comments: Small node at Speedway 25% C. A 12' paved and a 8' soft path are in place for 670'.

Enhancement Opportunities: project under development to provide new irrigation, landscaping, paths, and a pedestrian bridge.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

West Bank

Existing Conditions:

Landscaping:	E, C, B
Irrigation:	None - good
Asphalt Cond. DG Path	9' to 12' not installed
Connectivity:	3E NC
Bridge:	N/A
Underpass:	1 - pass

Comments: Lowes B due to structures, art, canopy. Then small commercial C. Grade drop Lowes to commercial C. The 12' paved path exists most of the way. The 8' soft path exists only partially at Lowe's. Limited ROW due to existing structures.

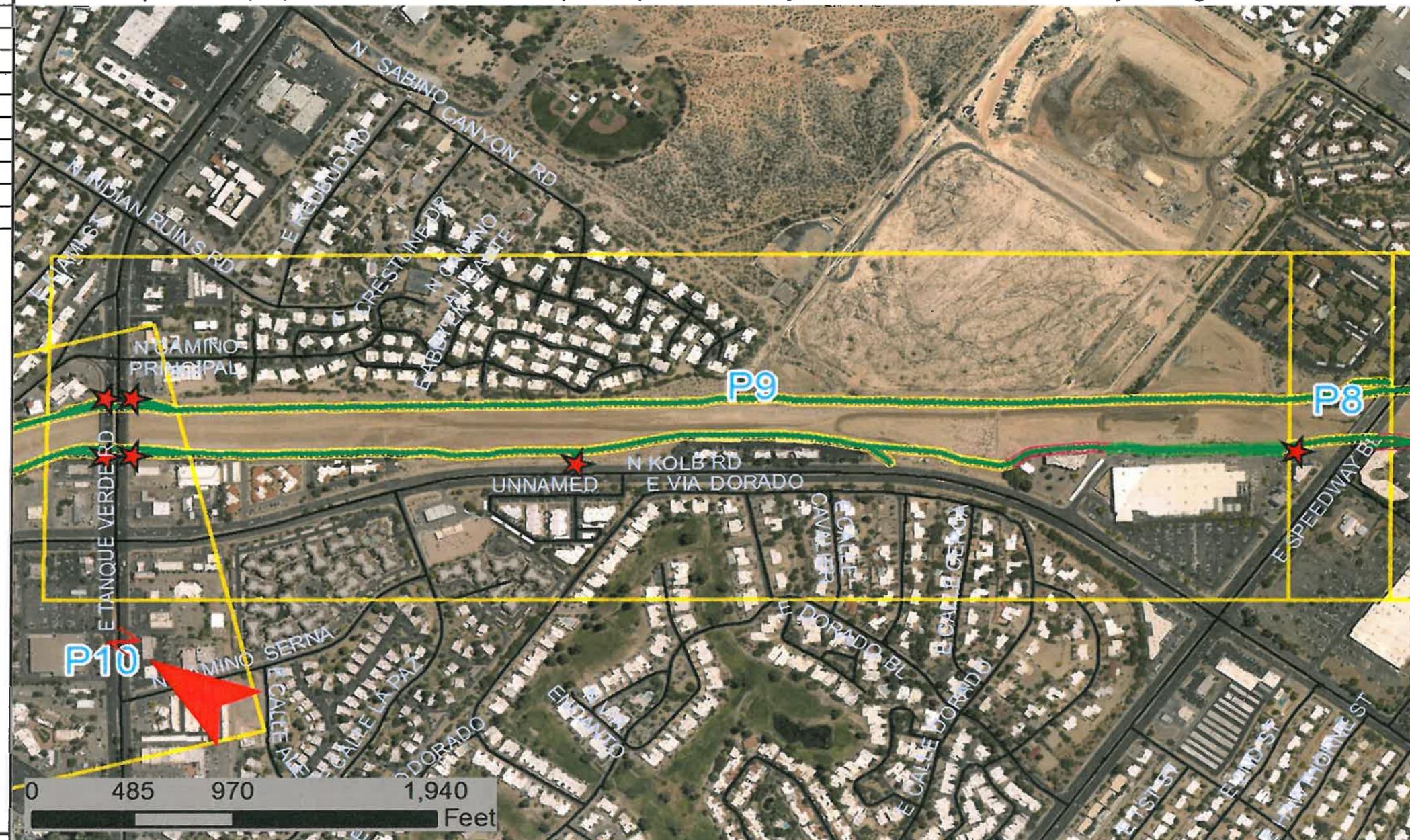
Enhancement Opportunities: project recently completed no recommended improvements at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Pantano Wash: Speedway to Tanque Verde - Map P9

Prepared: 8/27/2012 Riverpark Improvement Project Project Length: 1.15 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

East Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path	12' not installed
Connectivity:	2E NC
Bridge:	N/A
Underpass:	1 - pass

Comments: project recently completed. Limited ROW available for full build out due to existing structures and landfill. Limited plantings to the north.

Enhancement Opportunities: project recently completed no recommended improvements at this time.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

South Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path:	12' not installed
Connectivity:	2E NC, 2N NC, 1N TH
Bridge:	1 - new
Underpass:	N/A

Comments:
Project under development

Enhancement Opportunities:
project under development with ROW acquisition, bank stabilization, pedestrian bridge, asphalt and DG pathway, trailhead and neighborhood connections, irrigation and landscape as part of the scope.

Estimated Cost:

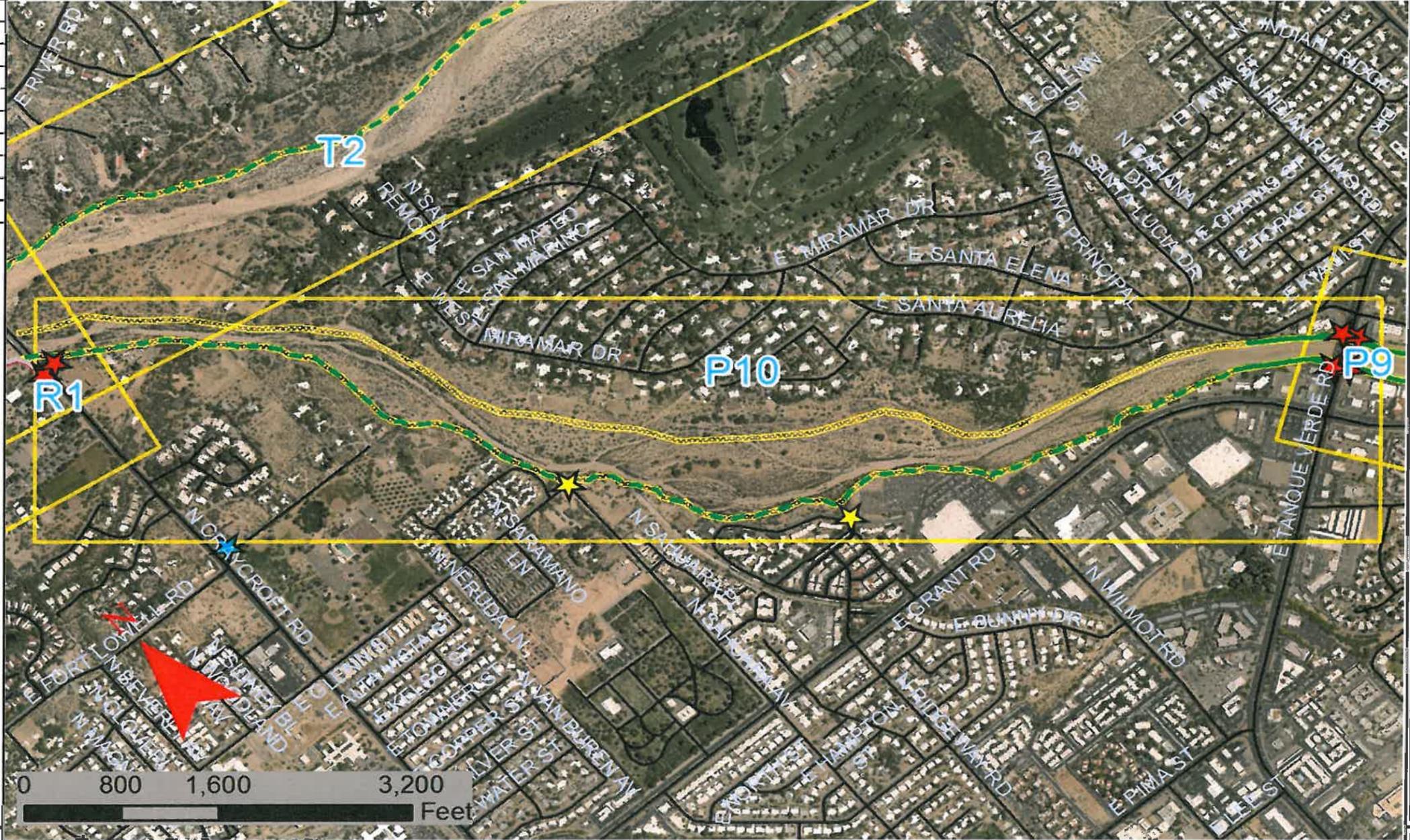
Landscape	\$100,000.0
Irrigation	\$120,000.0
Asphalt (& DG)	\$660,000.0
Trailhead/RR	\$250,000.0
Node	\$30,000.0
Fencing	\$400,000.0
Drainage	\$750,000.0
Bridge	\$150,000.0
Underpass	\$0.0
Other	\$3,250,000.0
Total	\$5,710,000.0

Pantano Wash: Tanque Verde to Craycroft - Map P10 Undeveloped

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 2.2 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

North Bank

Existing Conditions:

Landscaping:	E
Irrigation:	None
Asphalt Cond. DG Path:	not installed not installed
Connectivity:	None
Bridge:	N/A
Underpass:	1 - pass

Comments:
Project under development.

Enhancement Opportunities:
project under development with ROW acquisition, bank stabilization, limited pathway development and restoration as part of the scope.

Estimated Cost:

Landscape	\$100,000.0
Irrigation	\$120,000.0
Asphalt (& DG)	\$300,000.0
Trailhead/ RR	\$0.0
Node	\$30,000.0
Fencing	\$400,000.0
Drainage	\$750,000.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$3,250,000.0
Total	\$4,950,000.0

Julian Wash/Tucson Diversion Channel: Palo Verde Road to Country Club - Map J6

South Bank

North Bank

Existing Conditions:

Landscaping:	none
Irrigation:	None
Asphalt Cond. DG Path:	future
Connectivity:	1 N NC
Bridge:	N/A
Underpass:	1 - Future

Existing Conditions:

Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	N/A
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
Section is under design for 12' paved path. Not certain if sufficient ROW exists for d.g. path except in limited areas.

Comments:
N/A

Enhancement Opportunities:
The current planned project does not include landscape and irrigation, due to lack of ROW and to avoid potential issues with Julian Wash which is not bank protected in these areas. No future upgrades considered at this time.

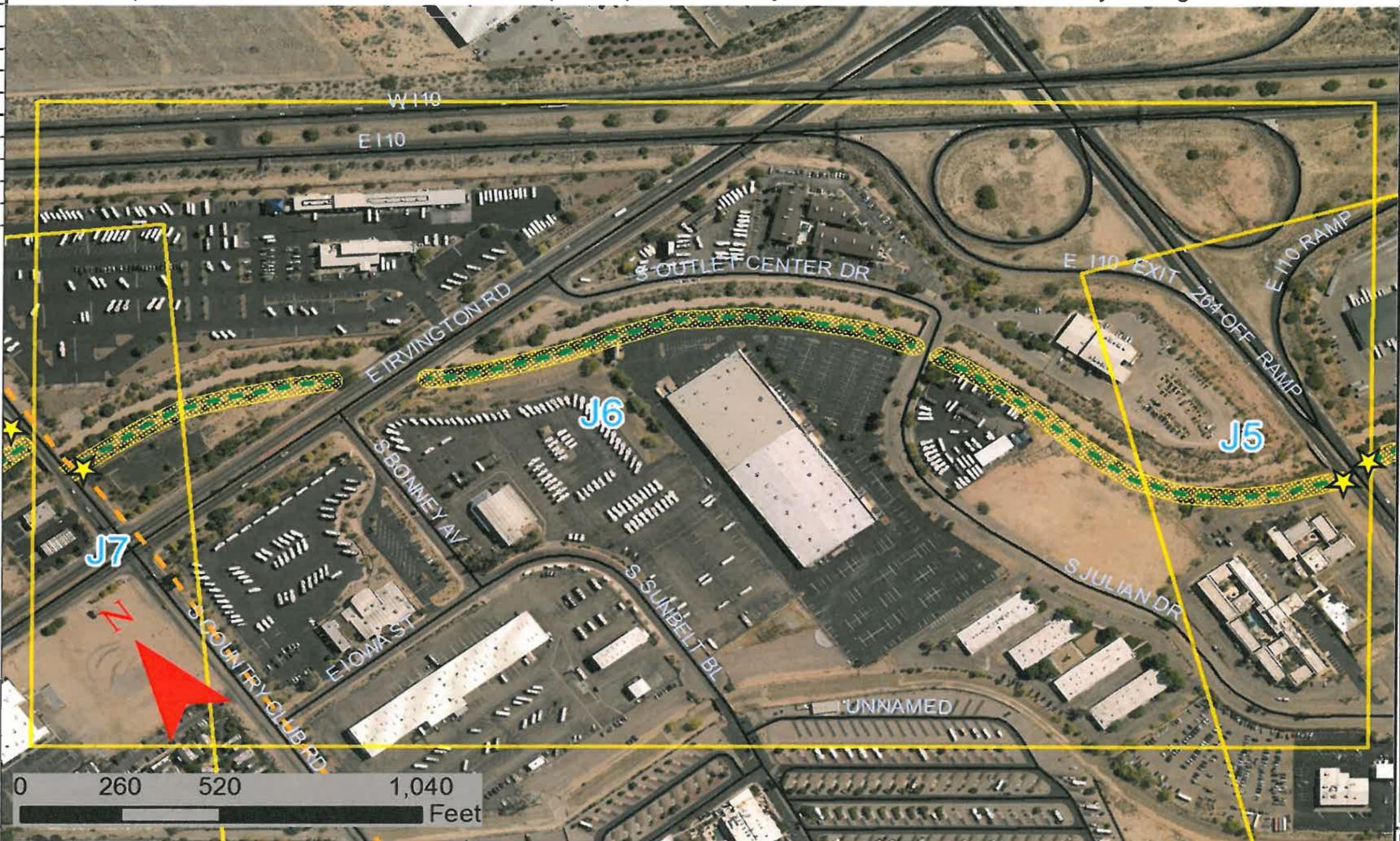
Enhancement Opportunities:
N/A

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0



Map Legend:

Street	Existing Trailhead	A - Premium, 100% to 81% Tree Canopy
Map Sheet Reference	Proposed Trailhead	B - High, 51% to 80% Tree Canopy
Open Pathway (Asphalt)	Existing Neighborhood Connection	C - Medium, 21% to 50% Tree Canopy
Open Pathway (Decomposed Granite)	Proposed Neighborhood Connection	D - Low, 0% to 20% Tree Canopy
Conceptual Pathway (Asphalt)	Access Pathway	E - Undeveloped, in Planning or Construction
Planned Pathway (Asphalt)		

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Prepared: 8/27/2012 Riverpark Improvement Project Project Length: 1.5 Miles

South Bank

Existing Conditions:

Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	N/A
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
N/A

Enhancement Opportunities:
N/A

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Julian Wash/Tucson Diversion Channel: Country Club to Tucson Diversion Channel - Map J7

Prepared: 8/27/2012 Riverpark Improvement Project Project Length: 1.4 Miles



Map Legend:

— Street
□ Map Sheet Reference
— Open Pathway (Asphalt)
— Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
▲ Existing Trailhead
▲ Proposed Trailhead
▲ Existing Neighborhood Connection
▲ Proposed Neighborhood Connection
— Access Pathway

Abbreviations:

■ A - Premium, 100% to 81% Tree Canopy
■ B - High, 51% to 80% Tree Canopy
■ C - Medium, 21% to 50% Tree Canopy
■ D - Low, 0% to 20% Tree Canopy
■ E - Undeveloped, in Planning or Construction

North Bank

Existing Conditions:

Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	Future N/A
Connectivity:	N/A
Bridge:	1 - Future
Underpass:	1 -Future

Comments:
Existing project under design plans to have a new underpass at Country Club road, 16' wide asphalt path and DG, and new bridge to the Tucson Diversion Channel north side.

Enhancement Opportunities:
Future enhancement project should include landscape and irrigation, DG path if current project can not provide installation, new trailhead and restroom. The existing land is developer owned, ROW would be needed for a trailhead, but is not included in the current estimate. New fencing and trail signage installation required.

Estimated Cost:

Landscape	\$70,000.0
Irrigation	\$70,000.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$250,000.0
Node	\$15,000.0
Fencing	\$115,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$220,000.0
	\$740,000.0

South Bank

Julian Wash/Tucson Diversion Channel: KERP Outlet to Campbell - Map J8

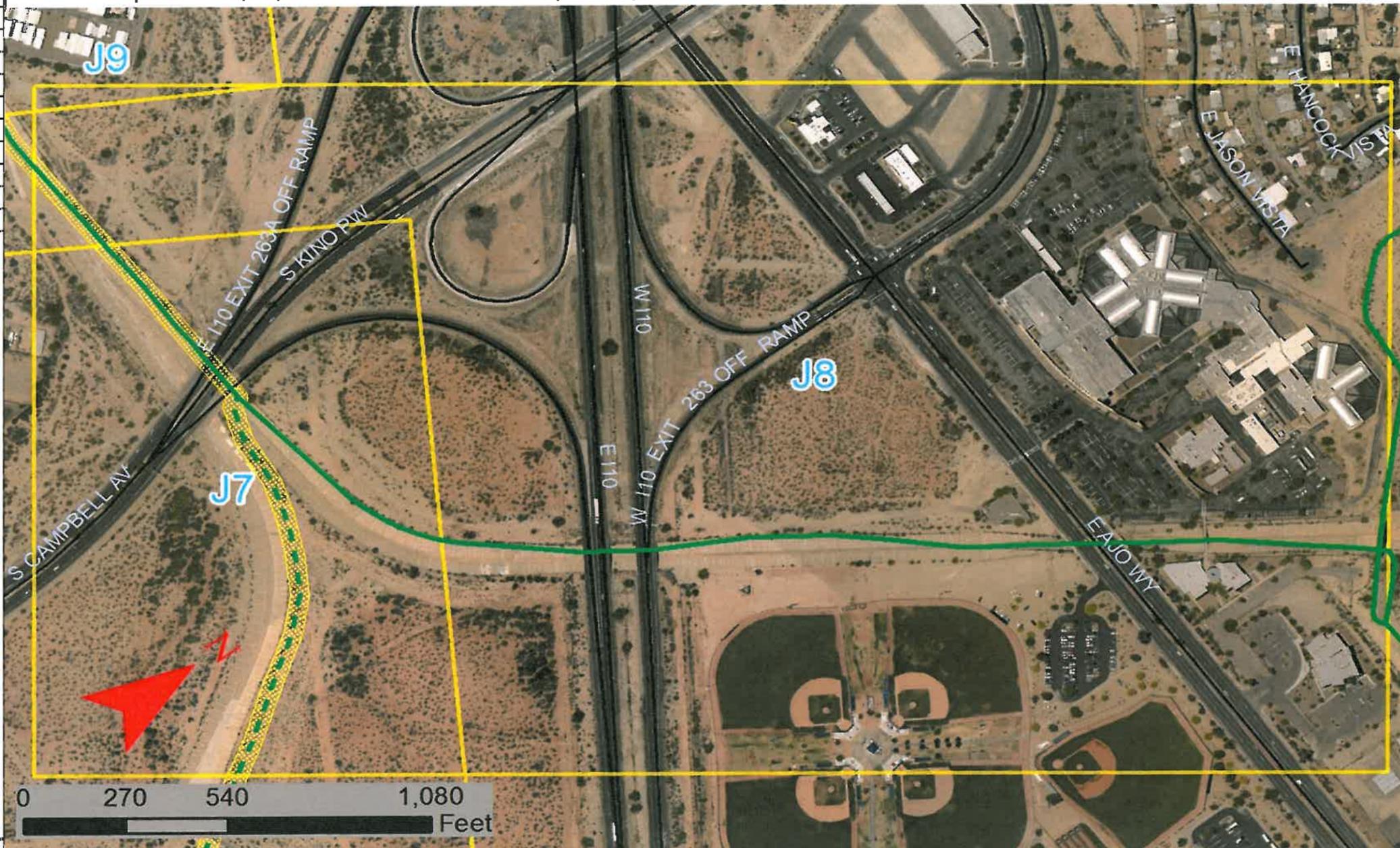
North Bank

Existing Conditions:

Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	N/A
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
N/A

Enhancement Opportunities:
N/A



Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: 1.4 Miles

Existing Conditions:

Landscaping:	none
Irrigation:	None
Asphalt Cond. DG Path:	10' not installed
Connectivity:	1 N - TH, 1 N - NC
Bridge:	N/A
Underpass:	2 - pass, 1 - fail

Comments:
Has existing 10' path, no d.g. path. Existing underpasses are available. No landscape space available on top of bank.

Enhancement Opportunities:
Provide new benched path development above low flow (raise path above bottom of channel). Provide new second ramp at Ajo up onto southbank for connectivity to Kino Sports/ Health Campus with local connection. Future internal paths are not part of this project. Provide widening of underpass at I-10 to increase from 10' width. Provide trail signage installation.

Provide additional improvements to Landscaping and irrigation at KERP itself for another 2.2 miles of existing trails.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:

Landscape	\$110,000.0
Irrigation	\$110,000.0
Asphalt (& DG)	\$295,000.0
Trailhead/ RR	\$0.0
Node	\$15,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$300,000.0
Other	\$280,000.0
	\$1,110,000.0

South Bank

Julian Wash/Tucson Diversion Channel: Campbell to Park\Ajo - Map J9

North Bank

Existing Conditions:

Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	N/A
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Comments:
N/A

Enhancement Opportunities:
N/A

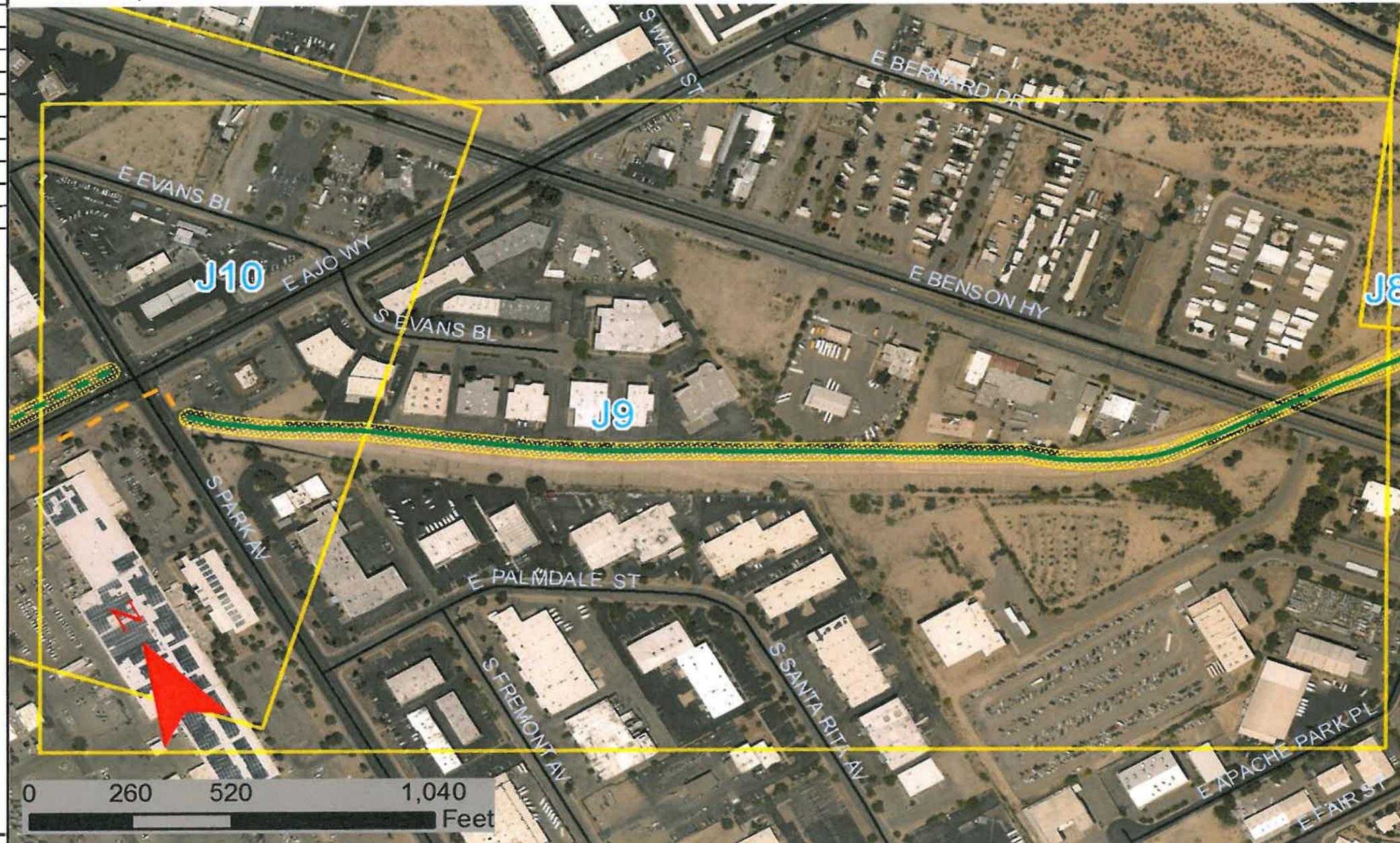
Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: .65 Miles



Existing Conditions:

Landscaping:	none
Irrigation:	None
Asphalt Cond. DG Path:	10' & 12' not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	1 - fail

Comments:
Has existing 10' to 12' path, no d.g. path. No room currently exists for landscape or irrigation enhancements.

Enhancement Opportunities:
Provide new benched path development above the low flow events to replace current bottom of channel path, with wider cross section. Provide upgrades to underpass area at Benson Highway for wider than 10' path. Provide trailhead signage installation.

Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$170,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$400,000.0
Other	\$240,000.0
Total	\$810,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)

- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

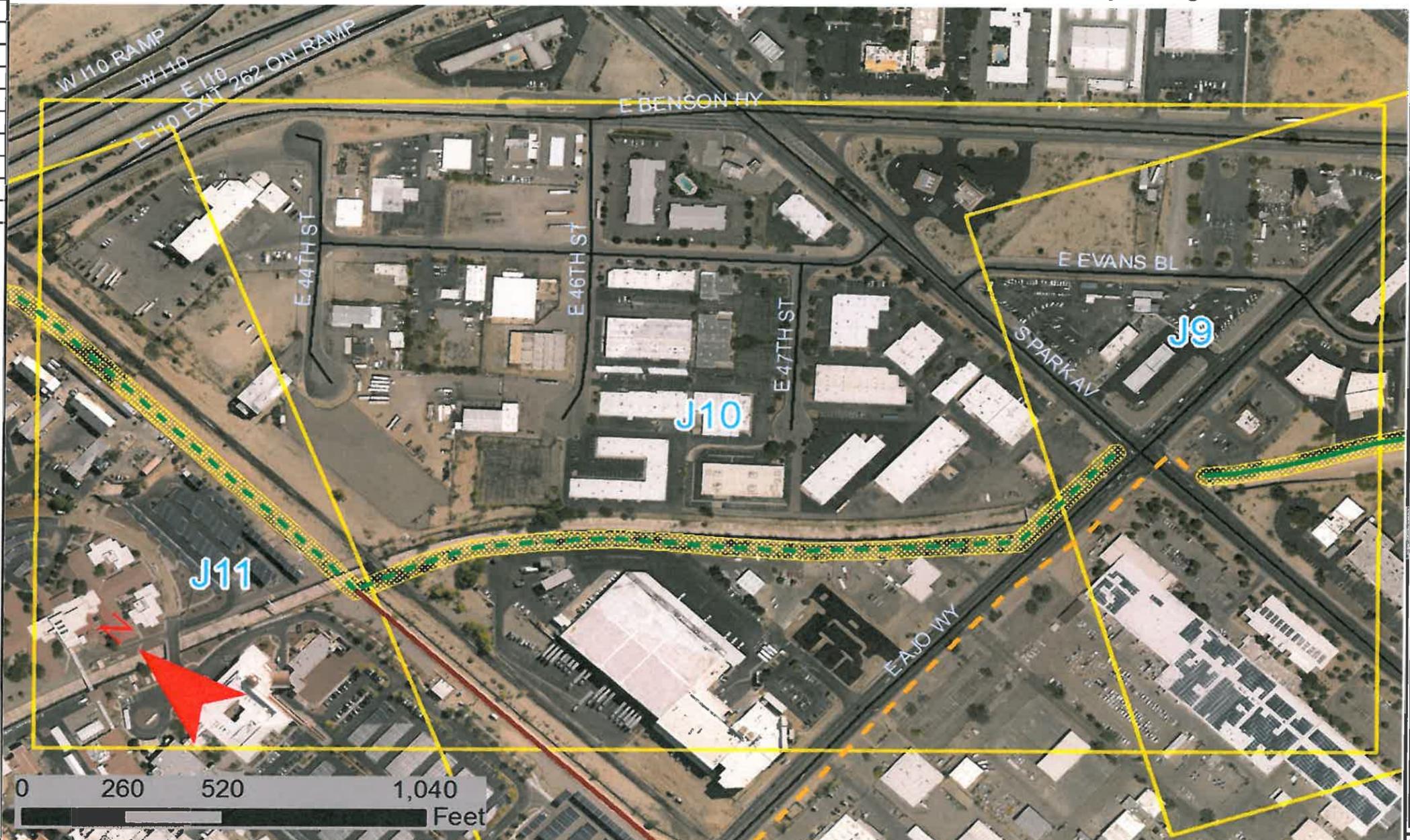
Julian Wash/Tucson Diversion Channel: Park/Ajo to UPRR - Map J10

Prepared: 9/21/2012

Riverpark Improvement Project

Project Length: .5 Miles

South Bank	
Existing Conditions:	
Landscaping:	None
Irrigation:	None
Asphalt Cond. DG Path:	not installed
Connectivity:	None
Bridge:	N/A
Underpass:	N/A
<p>Comments: This section along south side of Ajo Way is planned for construction 2012 with 12' path, no d.g. Will likely be built as 10' path due to ROW constraints, obstacles.</p> <p>Future path along Julian/Diversion Channel is under preliminary design.</p>	
<p>Enhancement Opportunities: These are for the alignment along the TDC up to the Julian Wash and UPRR crossing.</p> <p>Project includes new Asphalt and DG paths where available. Includes irrigation, landscaping and required security fencing. Installation of train signage required.</p>	



North Bank	
Existing Conditions:	
Landscaping:	N/A
Irrigation:	N/A
Asphalt Cond. DG Path:	N/A
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A
<p>Comments: N/A</p>	
<p>Enhancement Opportunities: N/A</p>	

Estimated Cost:	
Landscape	\$25,000.0
Irrigation	\$25,000.0
Asphalt (& DG)	\$230,000.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$95,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$125,000.0
Total	\$500,000.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- Conceptual Pathway (Asphalt)
- Planned Pathway (Asphalt)
- Existing Trailhead
- Proposed Trailhead
- Existing Neighborhood Connection
- Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:	
Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
Total	\$0.0

East Extent: Both Banks

Existing Conditions:

Landscaping:	good
Irrigation:	fair
Asphalt Cond.	10'
DG Path	N/A
Connectivity:	2E NC
Bridge:	2 - Pass
Underpass:	N/A

Comments:
 10' paved path exists on north side of Ajo and east side of 6th Ave, no room for d.g. path. This is a temporary section of "greenway". It gets used only moderately, so the 10' path will probably be acceptable for current and future use.

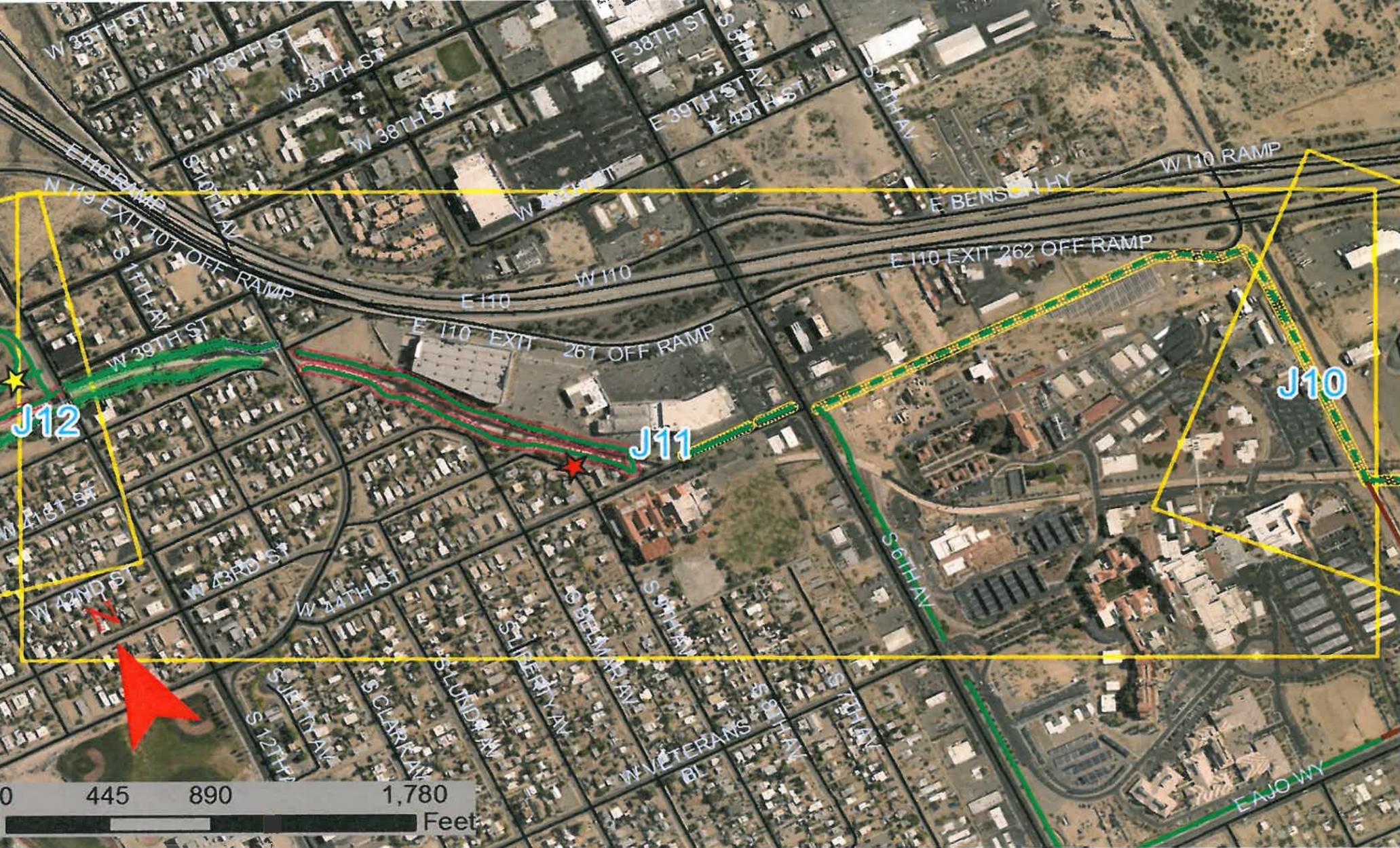
Enhancement Opportunities:
 Provide asphalt path rehabilitation and widening where available. Replace irrigation /upgrade existing landscaping where needed. Provide enhancement and cleanup of existing nodes. Provide trail signage installation.

Estimated Cost:

Landscape	\$180,000.0
Irrigation	\$50,000.0
Asphalt (& DG)	\$50,000.0
Trailhead/RR	\$0.0
Node	\$30,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$150,000.0
Total	\$460,000.0

Julian Wash/Tucson Diversion Channel: UPRR to 44th Street (through VA Hospital); 44th Street to 10th Avenue; 10th Avenue to 12th Avenue - Map J11

Prepared: 8/27/2012 Riverpark Improvement Project Project Length: 1.35 Miles



Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Through VA Campus

Existing Conditions:

Landscaping:	N/A
Irrigation:	None
Asphalt Cond.	N/A
DG Path	not installed
Connectivity:	None
Bridge:	N/A
Underpass:	1 - Future

Comments:
 Additional ROW is required for the project, but not included in the cost of the project. Coordination with VA Hospital is required for ROW; VA will not provide area that will allow enough room for more than 10' path.

Enhancement Opportunities:
 Install new varying width Asphalt path and separated DG where ROW is available. Install irrigation and tree installation. Provide new underpass ramps at the UPRR to provide separated crossing using existing Julian Wash channel. Requires retaining walls, and metal cover. Provide security fencing, and trail signage installation.

Estimated Cost:

Landscape	\$30,000.0
Irrigation	\$35,000.0
Asphalt (& DG)	\$195,000.0
Trailhead/ RR	\$0.0
Node	\$0.0
Fencing	\$104,000.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$278,000.0
Other	\$190,000.0
Total	\$832,000.0

Santa Cruz Confluence

**Julian Wash/Tucson Diversion Channel: 12th Avenue to end Heritage Park Loop;
End Heritage Park Loop to Santa Cruz confluence - Map J12**

12th to Heritage, S&N

Existing Conditions:

Landscaping:	none
Irrigation:	none
Asphalt Cond. DG Path:	not installed
Connectivity:	N/A
Bridge:	N/A
Underpass:	N/A

Prepared: 8/27/2012

Riverpark Improvement Project

Project Length: .4 Miles

Existing Conditions:

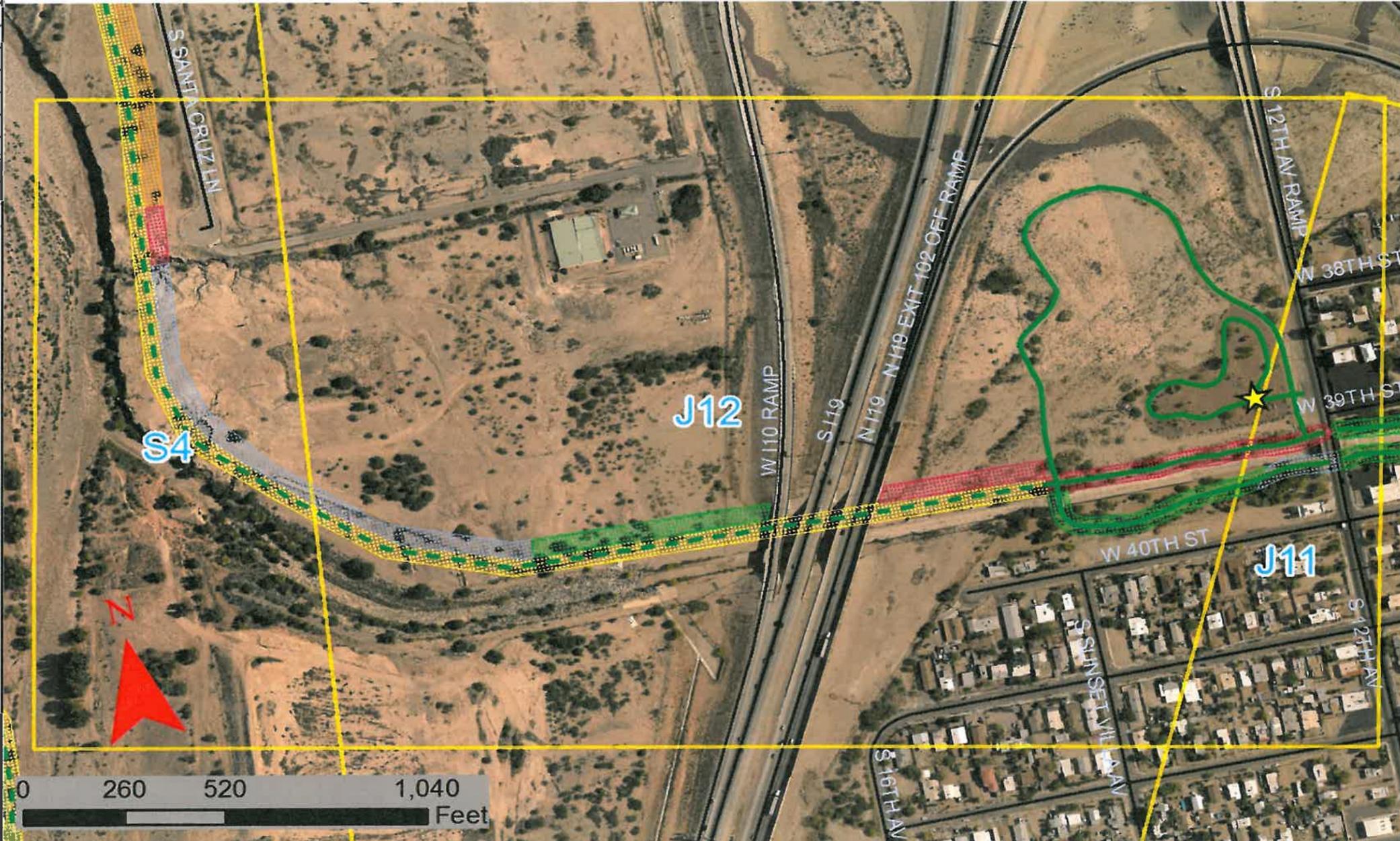
Landscaping:	good
Irrigation:	fair
Asphalt Cond. DG Path:	12' & 0'
Connectivity:	1N TH
Bridge:	N/A
Underpass:	failing

Comments:
West of Heritage park, including an underpass at I-19, is currently a project in design. This project, once completed will provide the required connection between the two rivers.

Enhancement Opportunities:
No enhancement project anticipated, assumed full installation as part of existing project.

Comments:
12th to Heritage is an existing portion of river path. Landscape is adequate in some areas.

Enhancement Opportunities:
Project could include asphalt path upgrades and rehabilitation where available (and not in conflict with historical site). Similarly, landscape and irrigation can be upgraded. Enhance existing neighborhood connections. Graffiti cleanup and trail signage are included.



Estimated Cost:

Landscape	\$0.0
Irrigation	\$0.0
Asphalt (& DG)	\$0.0
Trailhead/RR	\$0.0
Node	\$0.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$0.0
	\$0.0

Map Legend:

- Street
- Map Sheet Reference
- Open Pathway (Asphalt)
- Open Pathway (Decomposed Granite)
- - - Conceptual Pathway (Asphalt)
- - - Planned Pathway (Asphalt)
- ▲ Existing Trailhead
- ▲ Proposed Trailhead
- ▲ Existing Neighborhood Connection
- ▲ Proposed Neighborhood Connection
- Access Pathway

Abbreviations:

- A - Premium, 100% to 81% Tree Canopy
- B - High, 51% to 80% Tree Canopy
- C - Medium, 21% to 50% Tree Canopy
- D - Low, 0% to 20% Tree Canopy
- E - Undeveloped, in Planning or Construction

Estimated Cost:

Landscape	\$30,000.0
Irrigation	\$30,000.0
Asphalt (& DG)	\$45,000.0
Trailhead/ RR	\$0.0
Node	\$30,000.0
Fencing	\$0.0
Drainage	\$0.0
Bridge	\$0.0
Underpass	\$0.0
Other	\$40,000.0
	\$175,000.0

ATTACHMENT B

Pima County's New Main Street: The Loop

The River Park and the Loop creates opportunities to link public and private land use(s), adjacent to the River Park, to Loop users like bicyclists and pedestrians. Both existing and new residential development is encouraged to provide access to the Loop so residents can take advantage of the Loop's recreational and commuter options. Employment centers and smaller employers near and along the Loop are encouraged to provide bike access, bike infrastructure, and bike facilities to encourage their employees to bike to work as an alternative to driving or "trailsportation" as explained in the Pima Regional Trail Master Plan. Encouraging people to get out of their vehicles and use alternate modes of transportation will help reduce the region's air pollution.

Mixed uses are encouraged along the Loop to allow destination points to attract all types of bicycle enthusiasts from the recreational riders to commuters to competitors. Employment Centers along the Loop are encouraged to incorporate retail uses, general services, and restaurants into their complexes. Shopping, errands, and food are inviting destination points for Loop users. Public agencies have, and will continue to, locate parks, plazas, and amphitheaters along the Loop to provide entertainment venues, events, mobile vendor opportunities, and informal community gathering places for Loop users. Campbell Avenue at the Rillito River is an excellent example offering multiple uses for Loop users offering an event to watch the bats, shop and dine at this commercial node.



Figure 1 St. Philip's Plaza

This section of the report identifies opportunities along the River Park especially the multipurpose paths to further connect land uses and expand opportunities with the private sector to fully optimize the biking/walking infrastructure. This section of the report includes the following:

- Privately owned vacant parcels along the River Park located in the jurisdiction of unincorporated Pima County.
- Those parcels have been added to the Bike Route MapGuide for quick identification. The zoning layer can also be accessed to identify the current zoning use for those parcels.
- Opportunity and Constraint summary sheets with identified land uses have been created for the unincorporated portions of the River Park. Each sheet identifies surrounding land uses, major employers, key tourism opportunities or retail centers.
- Ideas have been generated for each type of land use. Not all of these ideas need to be initiated by Pima County, but could be forwarded to interested stakeholders or other non-governmental entities to pursue.
- Policy recommendations to consider which will enhance the River Park experience and encourage adjacent land uses to develop park-oriented design of their projects.

Opportunities for Specific Pima County (unincorporated) Private Vacant Parcels and Adjacent Land Uses

#

Development of vacant parcels and redevelopment of existing parcels adjacent to the River Park can contribute and enhance amenities provided by the Loop. Proximity to the River Park will not only economically benefit the property owners through association and promotion of the Loop, but will also benefit Loop users.

Development regulations to encourage those amenities will need to be adopted and regulations that prohibit desirable uses and amenities will need to be removed. Regional standards will need to be adopted to ensure consistent policies and regulations. All jurisdictions could adopt a unified River Park Overlay Zone with similar allowed land uses and development regulations. Signage can be consistent throughout the Loop, even through different jurisdictions. The City of Tucson and the Town of Oro Valley have already adopted elements and/or policies to encourage increased bicycle use and regional coordination. These can be utilized to adopt a unified River Park Overlay Zone with similar allowed land uses and development regulations. There are similar communities attempting to develop guidelines for land uses. Bike route and open space studies from Portland, Oregon, San Diego, California, Atlanta, Georgia, and Asheville, North Carolina, were reviewed for their approach to expanding Park uses. Some of these ideas are reflected in this report.

Map Guide now has the privately owned vacant parcels located as a layer (see the purple shading below). Those parcels could be used for either potential future acquisition by Pima County for the Loop, or owners could be encouraged to develop accessory or ancillary retail for Loop users.

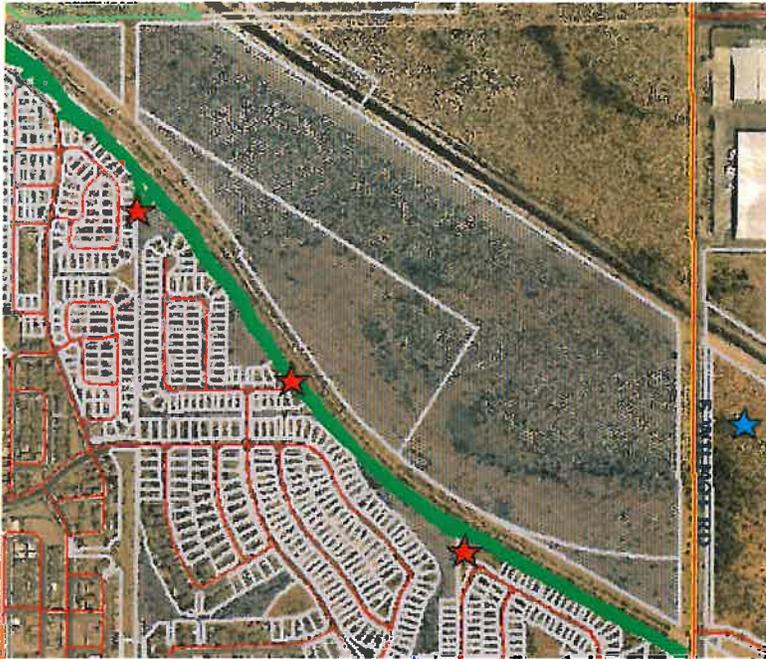


Figure 2 Vacant Parcels Layer

Attachment #1 in this report includes an land use opportunity map, a key map and individual project sheets that identify significant existing and some proposed land uses, vacant and privately-owned adjacent parcels, with opportunities and constraints of each reach location along the Loop.

Opportunities and Strategies for Existing Adjacent Land Uses in General

- **Commercial Uses:** While not exclusive to urban parks, bicycle-oriented businesses are developing in Portland, Oregon as the private sector responds to the number of bicyclists using bikeways. Businesses such as the Hopworks Bikebar benefits from 3,000 riders per day. Parking for 75 bicycles is provided. Other encouraged uses include restaurants, retail, and general services.

Strategies:

- Allow private signage along the route (regulated by adopted standards)
 - Help form a coalition of businesses such as a merchants association for the River Park. This could be formed with the Chamber of Commerce assistance. Merchants could sign up or list themselves via a Loop website to promote their location along the Loop.
 - Enter into cooperative agreements with office complex owners for trash pickup in exchange for parking use on weekends
 - Encourage farmer's markets, concerts, and events at the adjacent retail centers along the route
 - Merchants could encourage users to stop and shop with the use of plazas, shady landscaping, and such amenities as bike facilities (lockers), shower facilities, and drinking fountains
- **Existing Regional Shopping Centers:** Tucson Mall is the most obvious candidate for opportunities to integrate with the River Park path. Efforts should continue to

reach out the mall ownership and management to explore how to optimize their adjacent location to the River Park for the convenience of their shoppers, tenants and the community. This location would serve as a major connection to the bike trail with safe and secure access.

Strategies:

- Create incentives for the mall to dedicate the trail including parking reductions, landscaping, or incentives for shifting their rear travel lanes inward
- Demonstrate how other malls in the nation integrated with River Parks and bike trails
- Demonstrate how the existing roadway can be utilized for both vehicular and bicycle traffic
- Talk to individual retail tenants (REI) on how to connect to the bike trail
- Illustrate connections to the mall, including to the food court, traffic flows etc.



Figure 3 Stanford Shopping Center Mall in Palo Alto, Ca.

- **Industrial Areas:** The Julian Wash River Park is adjacent to most of the Pima County industrial zoned land and shovel ready land such as Century Park/Port of Tucson and the U of A Tech Park. This park would provide an amenity and sense of place to an emerging major employment center.

Strategies:

- Bike to Work opportunities to promote the “Trailsportation” concept
- Provide landscape buffering from the uses or building
- Avoid barb wire and walls traditional to industrial uses
- Develop easy, visible access to the park trails.

- **Hotels, Motels and RV Resorts:**

Strategies:

- Bike rental opportunities for their guests and an adjacent amenity to attract future guest/visitors
- Physical fitness/wellness programming options for their guests
- Advertise adjacency to the Loop
- Make sure the hotels have Loop maps and destinations points map

- **Residential Areas including single and multi-family:** Providing connection points to the paths are the most important strategy.

Strategy:

- Develop convenient connection points
- Accommodate a wide range of activities and have dog clean-up stations
- Reduce speeds for safety near subdivisions
- Possibly increase security amenities including augmented lighting, plant placement and graffiti abatement
- Address gated community access issues
- Enlist HOA assistance in patrolling, maintaining, and input

• **Schools, all K-12**

Strategies:

- Safe Route to Schools integration of use of the bike paths
- Outreach and bike education in the schools
- Field trips to routes with interpretative signage and vegetation

• **Multi-modal transportation:** The bus stops are depicted on MapGuide layers. In 2013, the addition of the Modern Street Car will expand Bike and Ride opportunities.

Strategies:

- Use all Bike Loop publicity material to demonstrate how to load bikes on and off buses
- Encourage co-location of car parking, bus stop and Loop connection points.
- Potential bike locker locations at bus stops to offer bus and bike commuter options

• **Mobile Vendors:** Mobile Vendors are prominently displayed in the Loop videos, however Pima County currently does not have a policy to allow vendors on public or private lands. Pima County will need to identify the appropriate places, appropriate times, appropriate services to allow and assure the vendors don't impede or create nuisances for trail traffic or adjacent residential neighbors. The vendor policy should try to cluster the vendors to create a sense of place and at reasonable intervals along the trail. Each jurisdiction has various policies in place to deal with temporary vendors.

Strategies:

- Create a policy to accommodate mobile vendors along the River Park with sensitivity to existing neighborhoods.
- Permit procedure for public lands which may differ from a process for private property
- Other jurisdictional input is needed for consistency throughout the county

• **Points of Interest and Tourism Opportunities:** Points of interest have been identified on the MapGuide layers. These points include adjacent sports and tourism centers such as the Kino Sports Complex and Pima Air and Space Museum but also include destinations and retail centers.

Strategies:

- 2 maps could be developed that will highlight the key characteristics of each reach of the Loop system in pocket size or mobile version. One map could focus on employment centers, schools and commuters. The other map would be tourism focused.

- Opportunity to create more attractions with interpretative signage along the Loop including cultural resource sites
- Further plan the large tracts of public land along the Santa Cruz (El Corazon) to include a variety of land uses including private hotel/attractions/sport venues
- Further plan large tracts of land along the Julian Wash to enhance the Kino Sports complex as a destination
- Collaborate with the Santa Cruz Valley Heritage Alliance and Metropolitan Convention and Visitors Bureau to include the Loop and River Park as a major site for visitors, nature walks, birding and bicycling

#

Policy Recommendations

Background: As early as 1978 the City of Tucson recognized the potential of the River Park and the need for inter-governmental cooperation when they adopted Resolution 10423 to include the Santa Cruz River Park Plan as an element in the City's General Plan. In the implementation chapter, Item 3 stated that "A zoning designation to allow development flexibility and encourage innovative site planning, such as a River Development District, should be developed and applied to the river and adjacent key parcels. The City's current General Plan Element 10 includes policies for an interconnected trail system and to "...encourage cooperation between local governments". Coordination ... "with Pima County to develop River Park Design Guidelines" is part of Policy 2.4 in Element 14-4.

The Town of Oro Valley included Policy 5.9 in their General Plan "To insure the development of the bikeway system and encourage its use." And Policy 5.9.3 while not directly referencing the Loop states that "The Town shall eliminate gaps and barriers in the pedestrian and bikeway systems..." The Town recognizes the need for regional cooperation in Policy 5.9.8: "The Town shall collaborate with Pima County and Tucson to enact consistent standards and design guidelines for pedestrian and bicycle facilities that will provide for a safe and sensible network." The Town of Marana developed a Marana Parks, Recreation, Trails and Open Space Master Plan in 2010 which reflects the linear park route and enhancement plans in the town.

Multiple jurisdictions participated in the creation of the Pima Regional Trail System Master Plan (2010, updated 2012) that identified opportunities and priorities, and promoted designs, maps and policies for a comprehensive multiple use trail system. That plan examined the regions policies and long range planning documents for conformity with the goals and objectives of a master trail system. This previous collaboration and shared goals sets the stage for further efforts to standardize and develop consistent land use regulations and enhancements along the River Park.

Recommendation: The following proposed policies will need further development upon County Administrator review and direction. Our options include:

1. Do we move forward by incorporating this policy into the comprehensive plan update and its implementation?

2. Do we proceed with some of the current proposals and implementation strategies for the Board of Supervisors to consider for adoption and immediate action separate from the comp plan update?

GOALS

The following **Goals** are adapted from the 2006 RTA Plan bikeway criteria and are echoed in the Pima County Regional Trail System Master Plan. Goal 1 is a county wide goal and Goals 2 – 5 apply to the unincorporated county areas. They have the dual objectives of setting Goals for the River Park and consistency with the RTA (The RTA criteria is highlighted in bold font) and have been adapted for consideration with new development or private parcels:

GOAL 1 Serve the population by providing recreational activities and reducing automobile use for shopping, entertainment or commuting

- Policy 1.1 Adopt a policy statement similar to the USDOT/FHWA Policy Statement that directs agencies to develop a policy statement that affirms their commitment to improving conditions for bicycling and walking, and to go beyond minimum design standards. As recommended by ADOT, it should affirm that local transportation agencies and communities shall go beyond minimum design standards and requirements to create safe, attractive, sustainable, accessible, and convenient bicycling and walking networks.
- Policy 1.2 Conduct public outreach and encourage adjacent employers to increase bike usage by employees.
- Policy 1.3 Consider development standards to require facilities for commuters to securely store their bikes and have access to shower and locker facilities.
- Policy 1.4 When considering transportation improvements, private or public, encourage bike friendly connections across obstacles, such as train tracks and I-10, installing bike signals at busy street crossings and signage.
- Policy 1.5 Support the development of bicycle/pedestrian connections to adjacent commercial sites, such as retail shopping centers, whenever possible
- Policy 1.6 Install bicycle signage along bikeways where needed to define the route and/or direct bicyclists to a destination or other bikeway. (City of Portland Comprehensive Plan).

GOAL 2 Provide economic benefits by allowing adjacent properties to benefit from River Park connectivity and activity

- Policy 2.1 Remove zoning barriers that prohibit bike park-oriented businesses such as appropriate clusters of mobile vendors
- Policy 2.2 Provide public information venues for services and amenities that are available along the River Park.

- Policy 2.3 Incent the provision of bicycle access to adjacent employment (ex: Veterans Hospital), recreational centers (ex. County Parks), and commercial centers, (ex: retail shopping centers)
- Policy 2.4 Reduce required parking areas for developments providing access to the Loop and increased bike facilities, thereby reducing costs, use of resource materials, and heat island effect. (From L.A. County Healthy Design Ordinance and APA PAS Report 556 Smart Codes: Model Land Development Ordinances.)
- Policy 2.5 Initiate contact with adjacent municipalities to consider and adopt similar coordinated River Park Overlay Zone districts for each municipality to include development regulations and guidelines for future development adjacent to the River Park including signage and access requirements.
- Policy 2.6 Allow mixed uses by right for development that provide bike infrastructure and connectivity within the Overlay Zone to encourage reasonable access to the River Park from residential, commercial, and employment centers. Neighborhoods should be studied to determine an appropriate mix of types of uses to insure that development is truly mixed use type and not dominated by one use.
- Policy 2.7 Provide for clear and appropriately scaled signage directing users between the Loop and off-site businesses
- Policy 2.8 Promote development of bike ride-mapping websites and smartphone applications to list adjacent private and public services and amenities.
- Policy 2.9 Encourage the use of alternative paving materials and permeable surfaces and use shade and landscaping to reduce the heat island effect on the adjacent private properties.

GOAL 3 Establish and maintain **connectivity** to employment centers, offices, commercial and services, recreational centers, hotels, higher education institutions, transportation centers, neighborhood public gathering places, and existing and planned bikeways beyond the adjacent properties to the River Park

- Policy 3.1 Integrate the River Park with existing multi-modal transportation routes (transit centers, Park and Ride parking lots, etc.)
- Policy 3.2 Provide secure bike parking at transit centers such as the Tohono Transit Center adjacent to the Rillito River bike paths.
- Policy 3.3 Utilize design features such as those specified for the 2004 Pantano River Park segment between Broadway and Fifth St that connected a major arterial roadway, shopping areas and neighborhoods using the Pima County Divided Urban Pathway separated bicycle and pedestrian paths (www.wheatscharf.com/projects/parks-redcreation/pantano-river.park/)
- Policy 3.4 Seek opportunities to connect to multi-modal transportation facilities, such as the Greyhound bus station and the Amtrak train station.
- Policy 3.5 Consider expansion of the River Park/Loop network to key destinations in the region.

Policy 3.6 Create single and multi purpose maps to depict tourism interests, employment routes and other key destination points.

GOAL 4 Protect the **Safety** of Park users and adjacent properties

- Policy 4.1 Establish police and neighborhood watch crime prevention patrols.
- Policy 4.2 Establish graffiti abatement protocol
- Policy 4.3 Create trash control procedures
- Policy 4.4 Use Crime Prevention Consideration for Landscaping and Lighting (CEPTD) design guidelines within the River Park and on adjacent properties.

GOAL 5 Identify opportunities relatively **easy to implement** from the listed strategies and policies to potentially include the following:

- Policy 5.1 Prioritize connectivity to Bike Loop for all new development and redevelopment consistent with current and revised Zoning Code requirements for trails as a part of recreation plans.
- Policy 5.2 Utilize Federal Highway, AZDOT, and RTA grants and/funds to construct infrastructure on private and public properties.
- Policy 5.3 Adapt relevant sections of the Zoning Code, such as landscaping requirements, to provide amenities such as shade on the private parcels as a part of a standard landscape plan.
- Policy 5.4 Adopt standard rezoning policies that require bicycle parking, showers, and connections to the River Park on adjacent properties in exchange for lower impact fees or other incentives
- Policy 5.5 Adopt a standard as part of the Regional Green Building Program codes that require bicycle infrastructure with commercial building permits.
- Policy 5.6 Facilitate appropriate businesses to encourage River Park uses, such as hotels adjacent to the Park providing or renting bicycles to guests.

Submitted by: Development Services: Carla Blackwell, Sue Morman, Dan Signor, and Arlan Colton
Office of Tourism and Economic Development: Tom Moulton
Special Thanks to Communications Office

Addendum

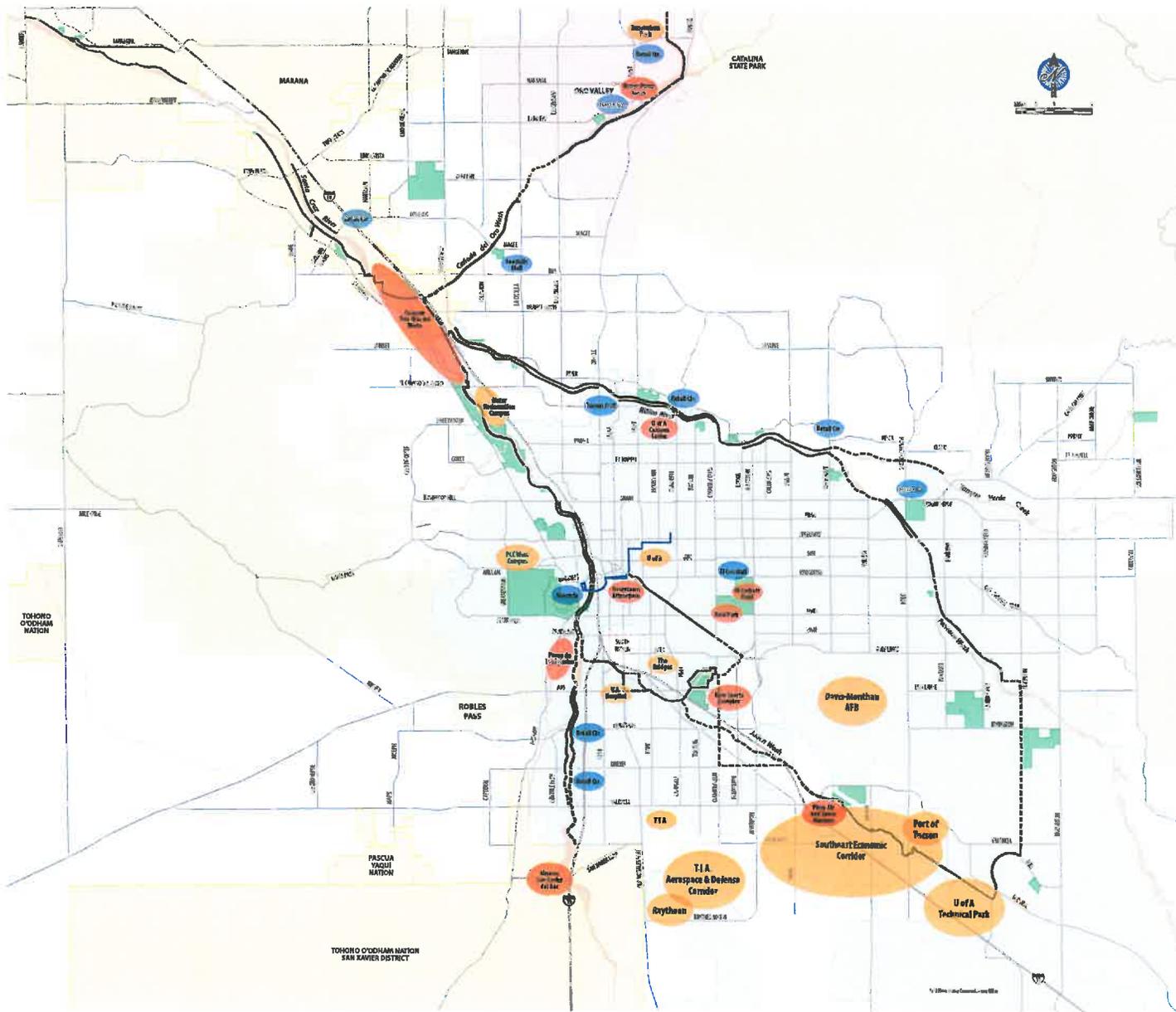
Land Use Opportunities Map: Entire Region

Key Map of River Park segments

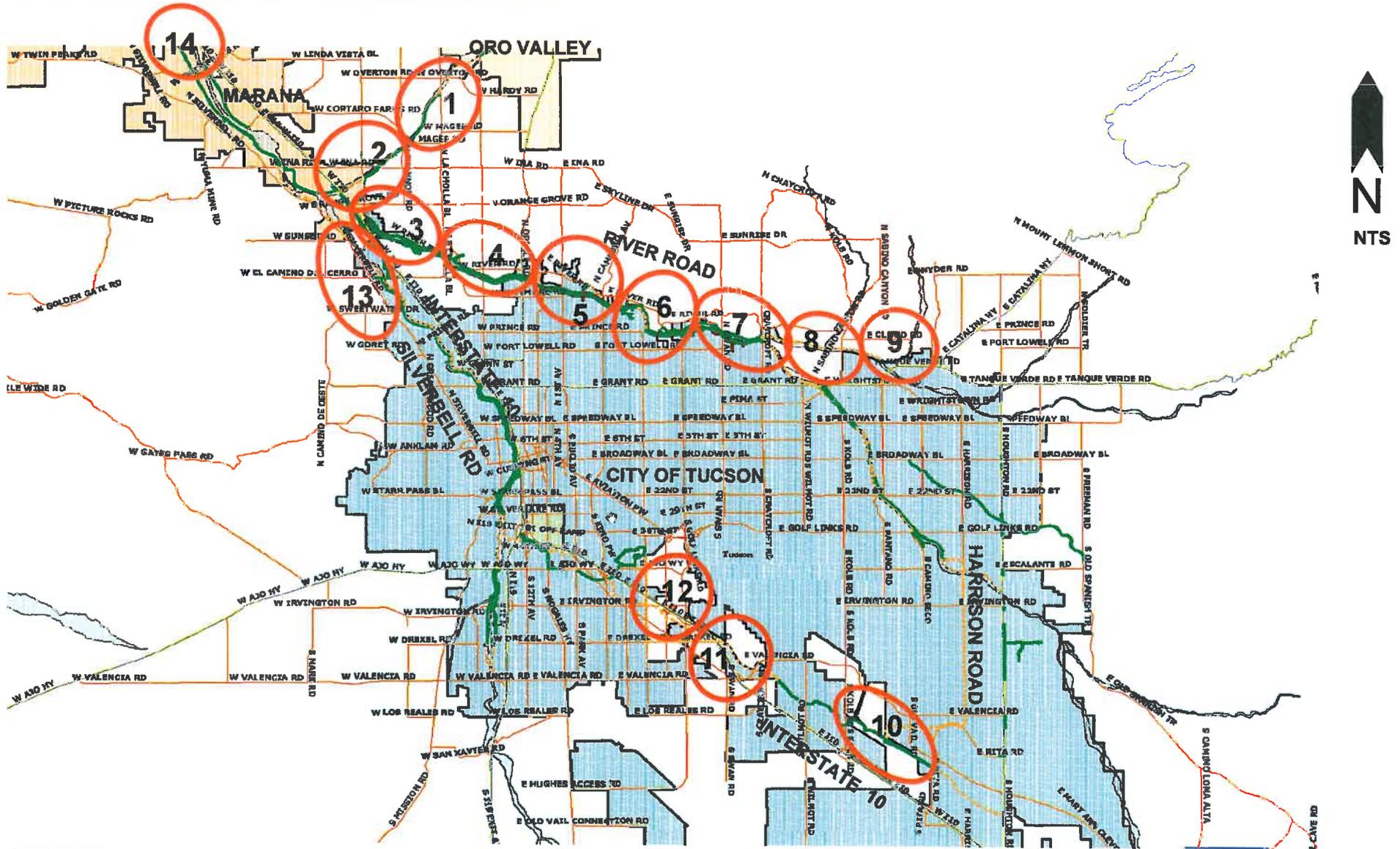
Maps 1 – 13 River Park segment maps of land use opportunities

Land Use Opportunities

-  Planning Areas & Major Employers
-  Attractions
-  Retail Centers
-  Parks
-  Municipalities, Forests, Reservations
-  The Loop
-  Modern Streetcar



LAND DEVELOPMENT OPPORTUNITIES KEY MAPS



LEGEND

- MAP 1 - Canada del Oro Wash from Overton Rd to Cortaro Farms Rd
- MAP 2 - Canada del Oro Wash from Cortaro Farms Rd to Santa Cruz River
- MAP 3 - Rillito Wash from Interstate 10 to La Cholla Blvd
- MAP 4 - Rillito Wash from La Cholla Blvd to Oracle Rd
- MAP 5 - Rillito Wash from Oracle Rd to Campbell Ave
- MAP 6 - Rillito Wash from Campbell Ave to Alvernon Way

- MAP 7 - Rillito Wash from Alvernon Way to Craycroft Rd
- MAP 8 - Pantano/Tanque Verde from Craycroft Rd to Sabino Canyon Rd
- MAP 9 - Tanque Verde from Sabino Canyon Rd to Tanque Verde Rd
- MAP 10 - Julian Wash from Kolb Rd to Rita Rd
- MAP 11 - Julian Wash from Alvernon Way to Craycroft Rd
- MAP 12 - Julian Wash from Country Club Rd to Alvernon Way
- MAP 13 - Santa Cruz River from Rillito Wash to Sweetwater Drive
- MAP 14 - Santa Cruz River from Tangerine Rd to Twin Peaks Rd

CANADA DEL ORO WASH - OVERTON RD TO MAGEE RD



VACANT PRIVATE PARCELS in unincorporated PC

24	225-44-0670	28	225-06-031A
25	225-25-112B	29	225-06-0200
26	225-06-042A	30	225-06-014A
27	225-06-0320		

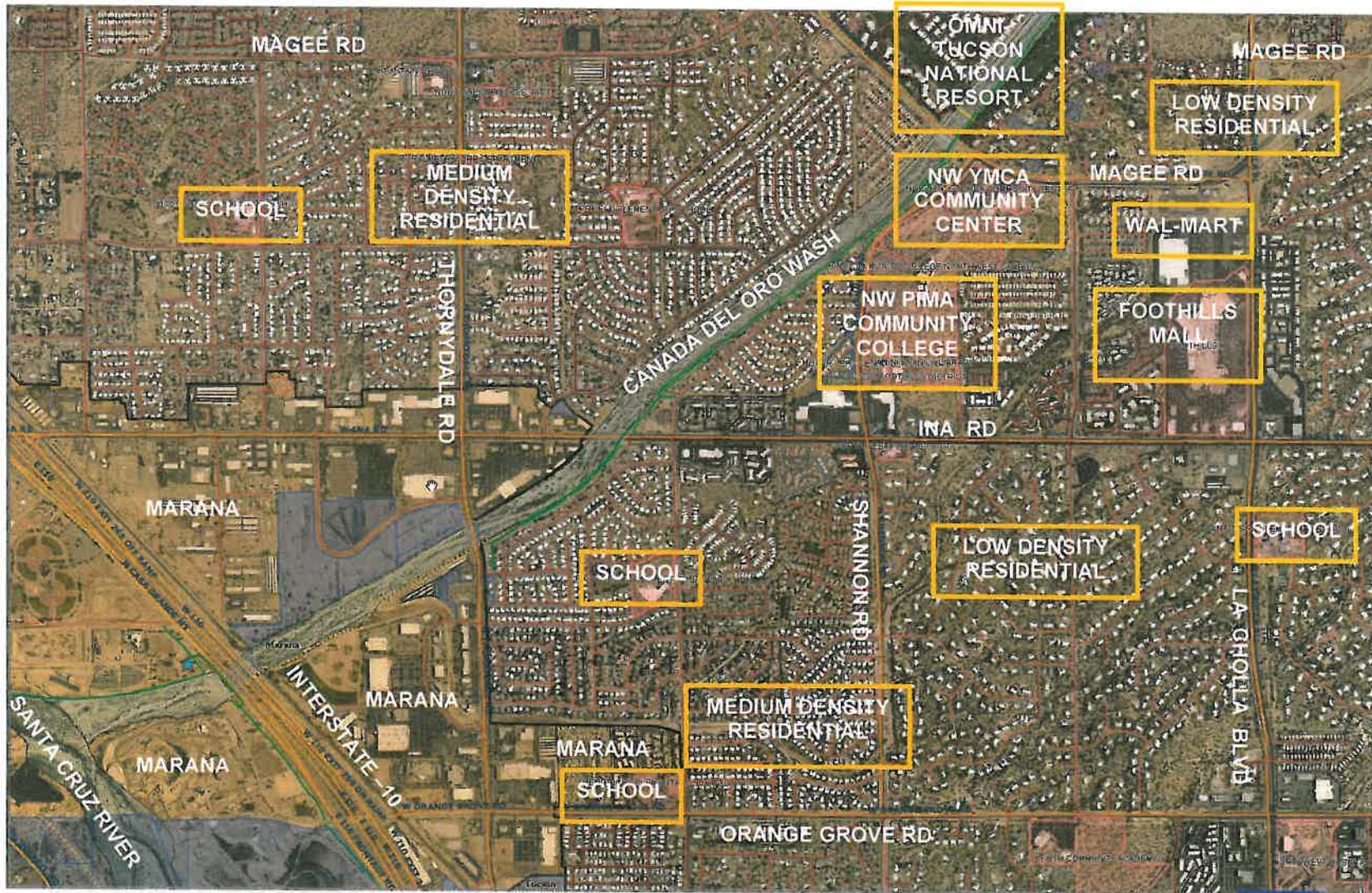
CONSTRAINTS

- 50% of the Loop is through the Resort
- Security Concerns
- Gated Communities
- Built-out residential a range of densities from low single family detached to apartments

OPPORTUNITIES

- Capitalize on connections with tourism
- Recreation, trail, and park enhancement
- Bike to Work and School
- Residential Connectivity
- Bike Loop on both sides of

CANADA DEL ORO WASH - MAGEE RD TO SANTA CRUZ RIVER



NO VACANT PRIVATE PARCELS in unincorporated PC within this stretch adjacent to the Loop

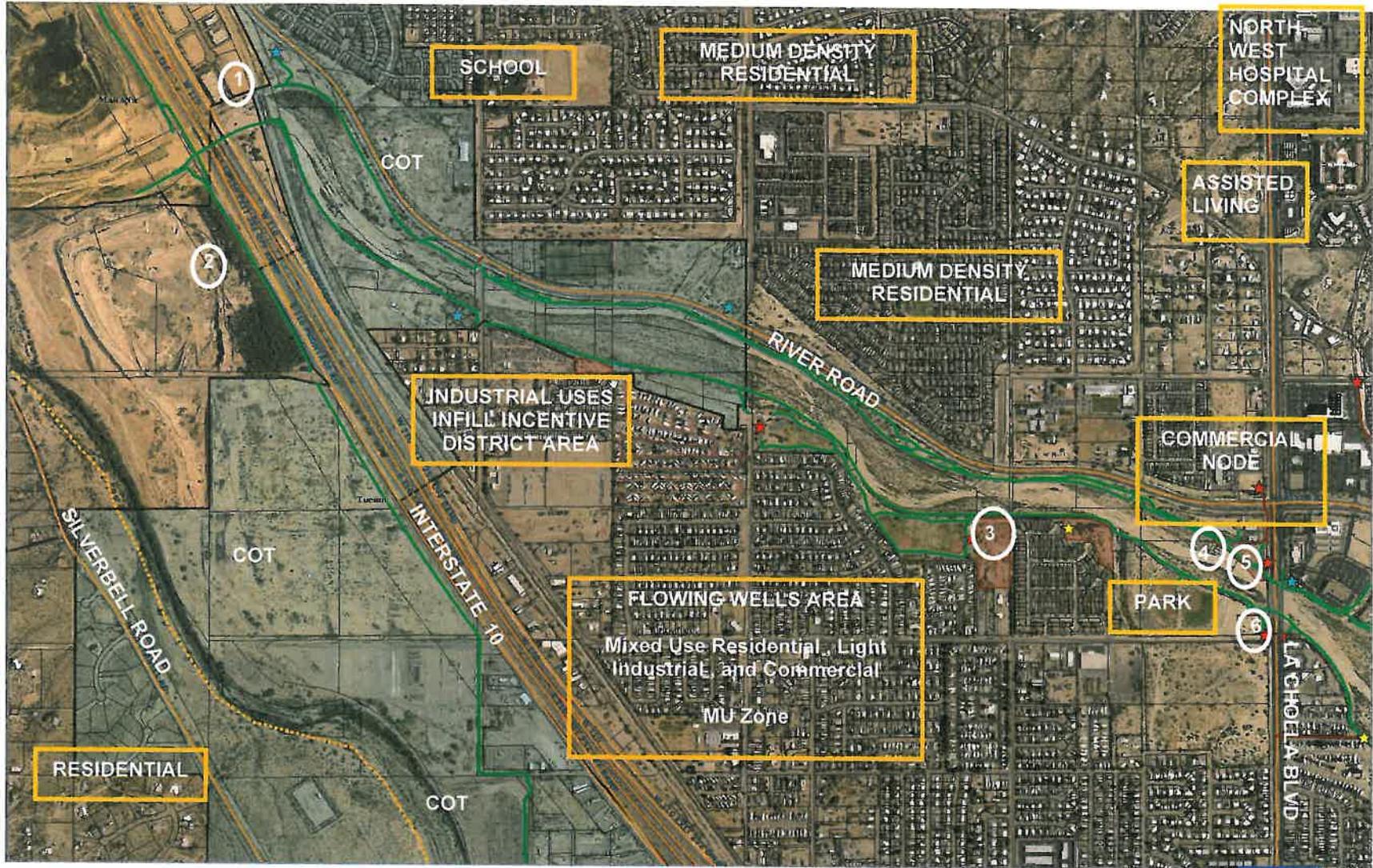
CONSTRAINTS

- Gated Communities
- Area 100% built-out to average medium density residential; a range of housing types from single family detached- town homes- to apartments

OPPORTUNITIES

- Bike to Work and School
- Residential Connectivity
- Bike Loop on both sides of Wash

RILLITO WASH - INTERSTATE 10 TO LA CHOLLA BOULEVARD



VACANT PRIVATE PARCELS in unincorporated PC

- | | |
|---------------|---------------|
| ① 101-06-0610 | ⑤ 101-13-015N |
| ② 101-06-004P | ⑥ 101-13-016D |
| ③ 101-14-313H | |
| ④ 101-13-014e | |

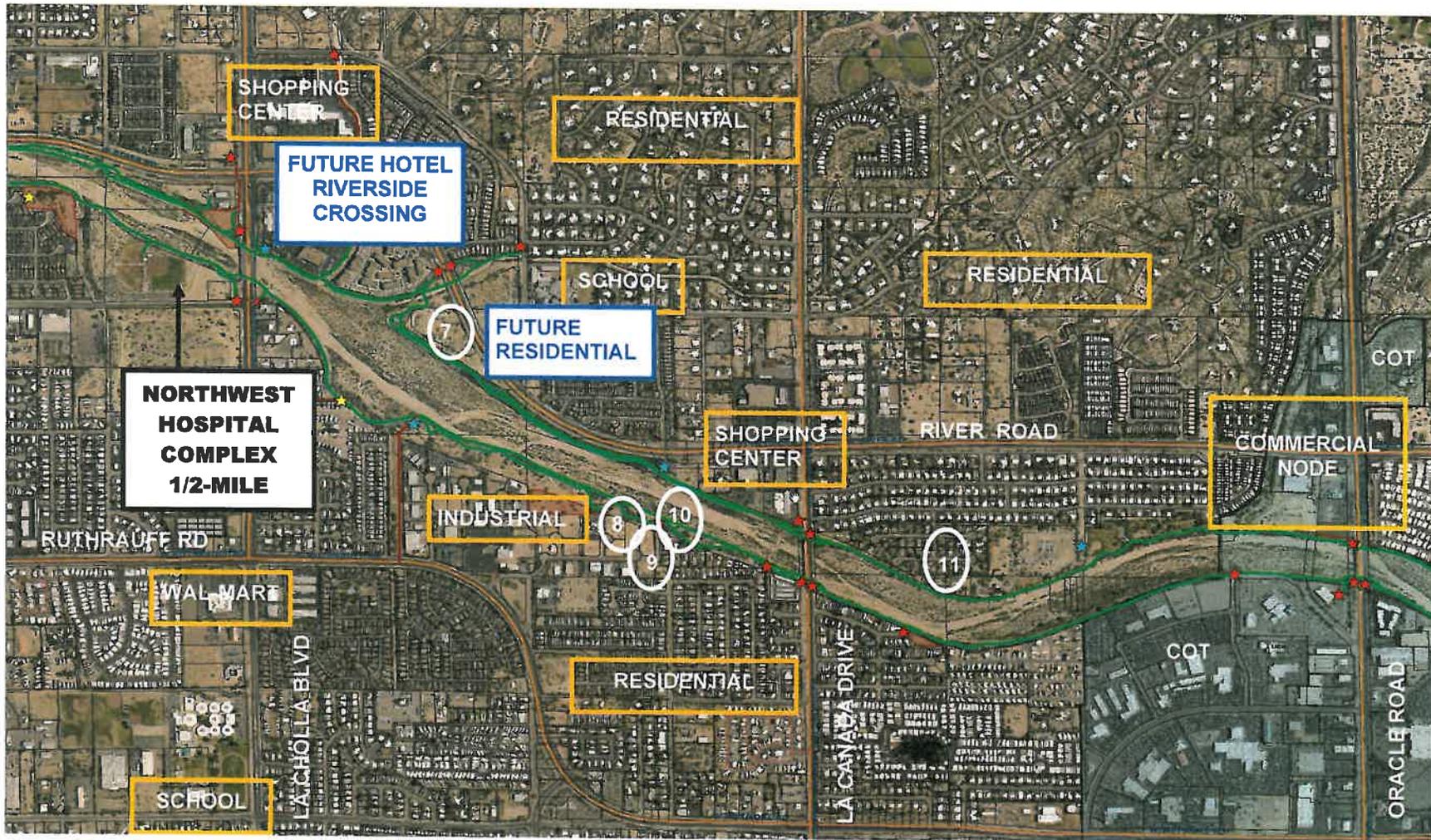
CONSTRAINTS

- Not buffered from industrial uses such as outdoor storage and salvage
- Mostly built-out adjacent to Bike Loop
- Brownfields
- Half of this stretch is in COT other half is in unincorporated PC

OPPORTUNITIES

- Employee Users/Industrial Revitalization
- Community Services in the Area
- Business & commercial services in the area to support existing residential
- Additional land designated for trail and park expansion
- Landscaping

RILLITO WASH - LA CHOLLA BOULEVARD TO ORACLE ROAD



VACANT PRIVATE PARCELS in unincorporated PC

- | | | | |
|---|-------------|----|-------------|
| 7 | 104-01-104G | 10 | 104-01-7030 |
| 8 | 104-01-7060 | 11 | 104-10-007B |
| 9 | 104-01-7040 | | |

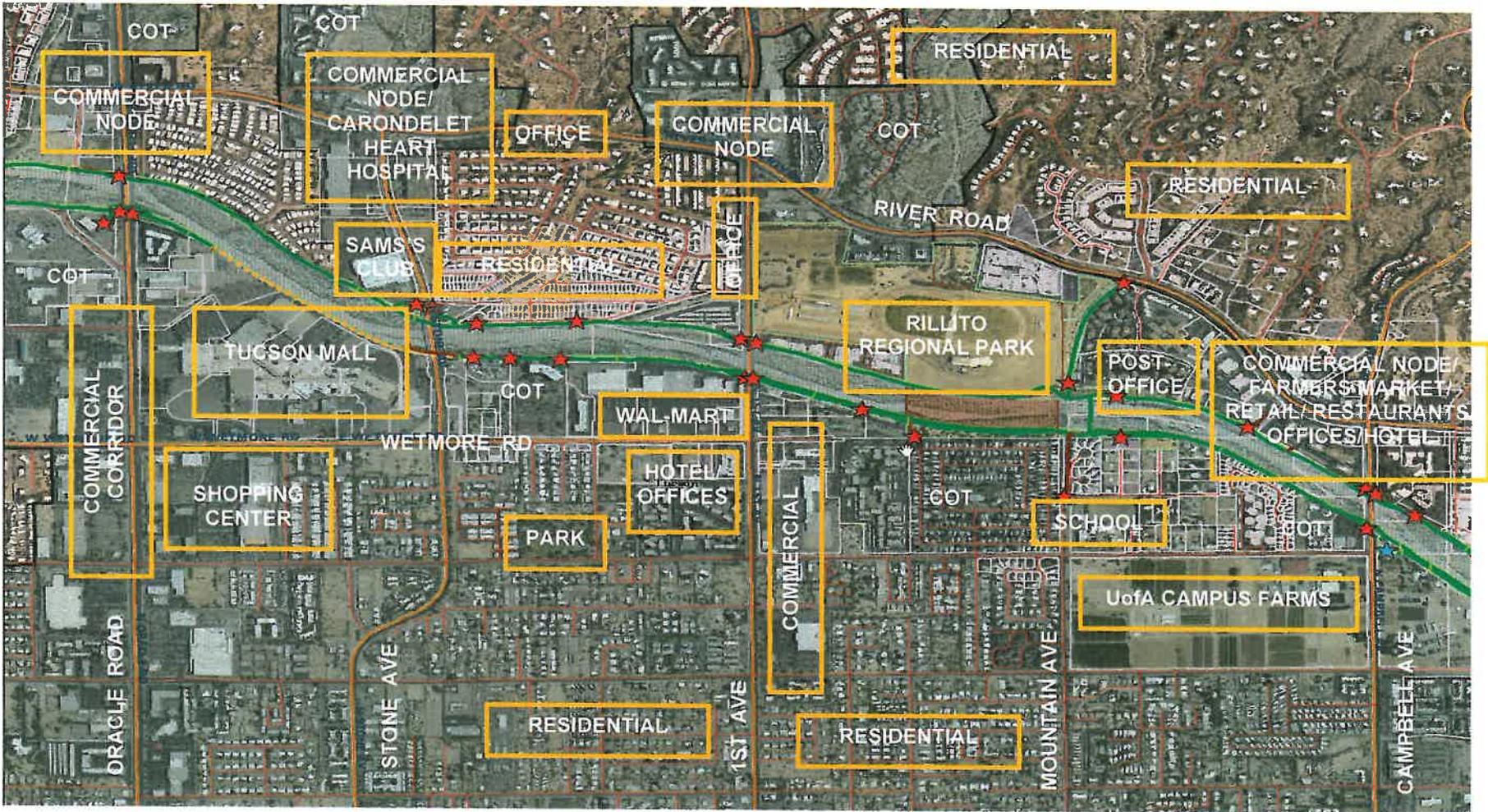
CONSTRAINTS

- Not buffered from industrial
- Mostly built-out along the Bike Loop
- Connectivity to existing residential
- Brownfields

OPPORTUNITIES

- Vacant industrial along Bike Loop
- Community Services in the Area
- Employment and commercial services in the area to support existing residential
- Mixed Use residential
- Landscaping
- Additional land designated for trail and park enhancement
- Adjacent use connections

RILLITO WASH - ORACLE ROAD TO CAMPBELL AVENUE



VACANT PRIVATE PARCELS

No Vacant Private Parcels along this Bike Loop stretch in unincorporated Pima County.

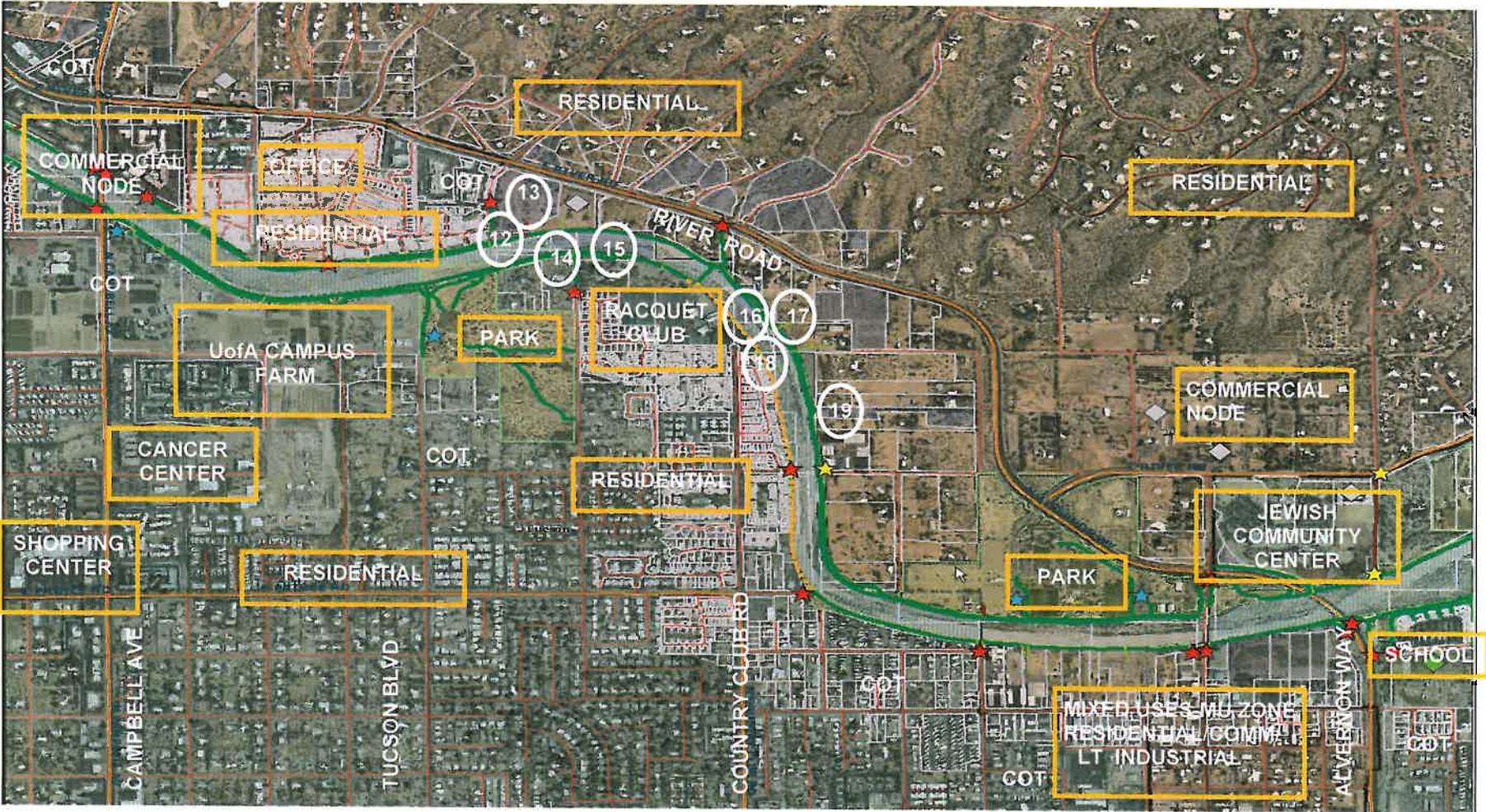
CONSTRAINTS

- 90+% Built-out along the Loop
- Lacks Connectivity to existing adjacent residential, commercial, and retail employment centers
- South half entirely in the COT

OPPORTUNITIES

- Promote business/restaurants along and connecting to Bike Loop
- Bike to Work, UofA, or School
- Activities, Recreation, and Events
- Additional land available for trail and park expansion
- Connection to Tucson Mall-Food Court
- Shop Walk Experience along the Bike Loop
- Med-high density residential

RILLITO WASH - CAMPBELL AVENUE TO ALVERNON WAY



VACANT PRIVATE PARCELS in unincorporated PC

12	108-24-013G	16	108-26-006D
13	108-24-008E	17	108-26-006B
14	108-24-013B	18	108-26-009B
15	108-24-0220	19	111-02-008B

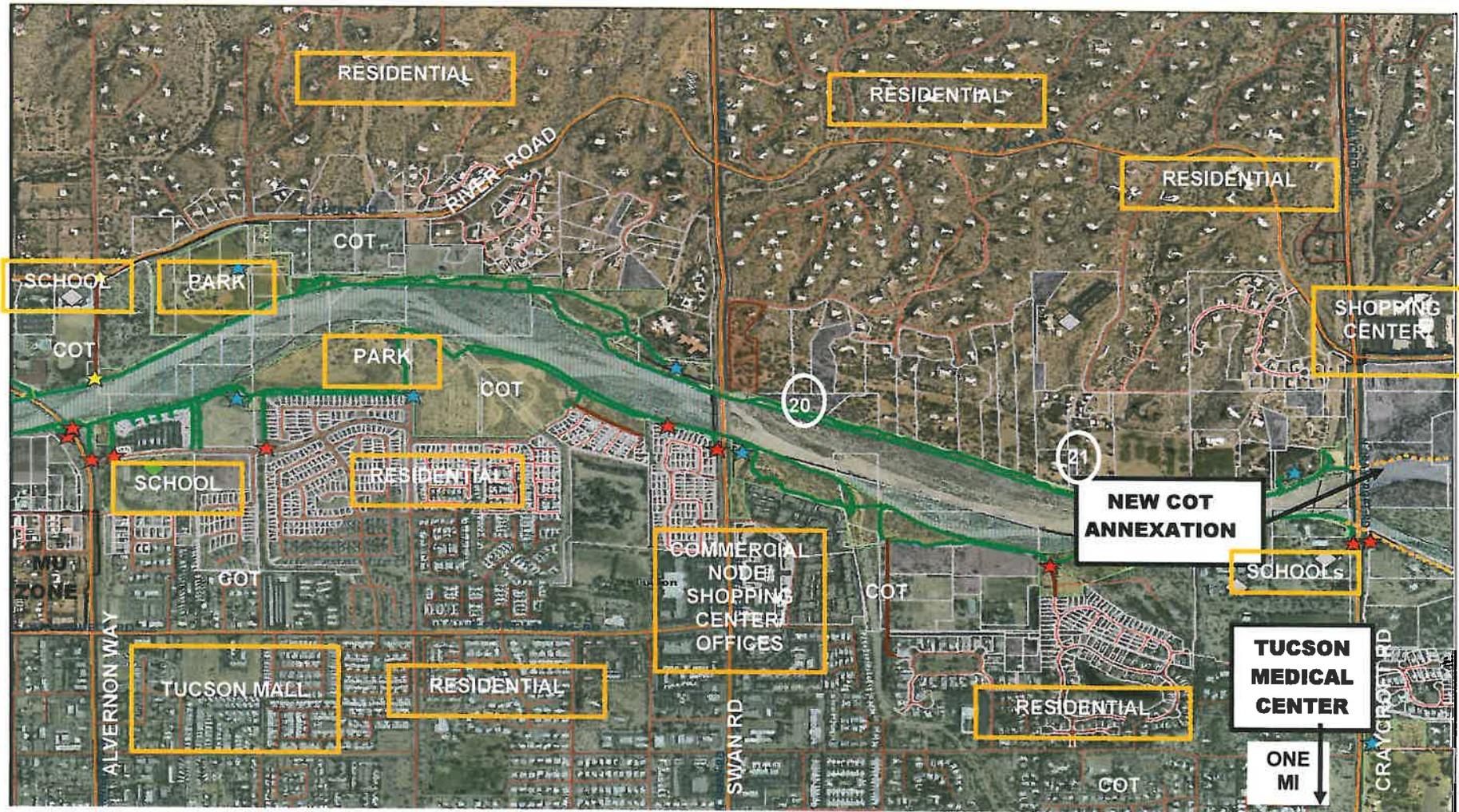
CONSTRAINTS

- Portions where trail right-of-way not acquired for Loop Construction
- Connectivity to existing adjacent residential
- Most of South Side in COT

OPPORTUNITIES

- Encourage new business/restaurants along Bike Loop
- Bike to Work, UofA, or School
- Activities, Recreation, and Events
- Signage
- Southside predominantly med-high density residential; north side predominantly low density residential.

RILLITO WASH - ORACLE ROAD TO CAMPBELL AVENUE



VACANT PRIVATE PARCELS in unincorporated PC

- 20 109-24-028G
- 21 109-26-005N

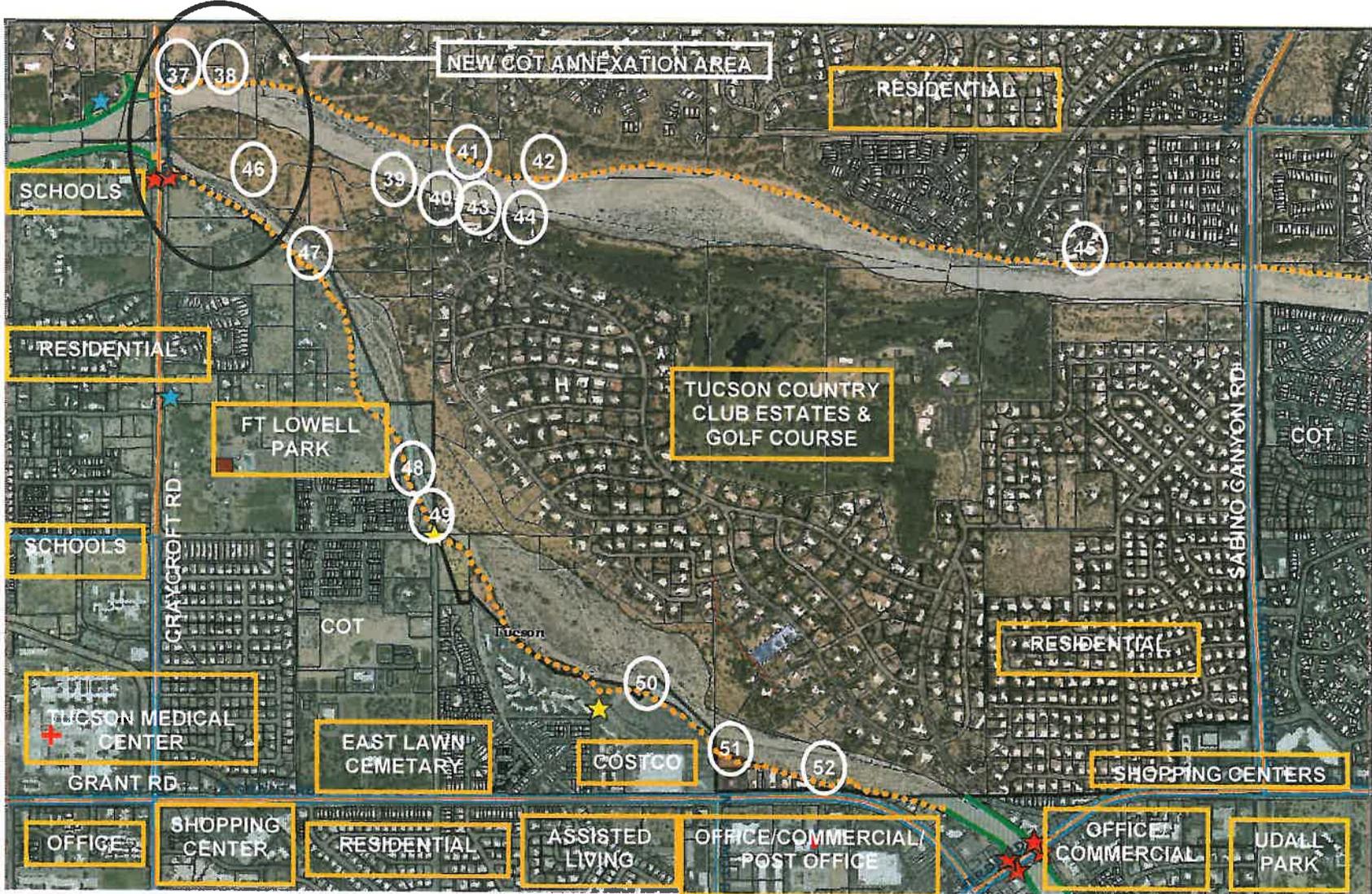
CONSTRAINTS

- Trail Completed through this stretch of Bike Loop
- Mostly residential within this stretch of Bike Loop
- Connectivity

OPPORTUNITIES

- Recreation, Health
- Residential Connections
- Bike to Work or Safe Route to School
- Signage
- Additional land for available for trail and park enhancement/expansion.
- Activities, events

PANTANO/TANQUE VERDE WASH CRAYCROFT ROAD TO SABINO CANYON ROAD



VACANT PRIVATE PARCELS in unincorporated PC

37	109-22-009B	41	109-22-017E	45	114-37-097D	49	110-14-0560
38	109-22-005C	42	109-22-2800	46	109-22-020M	50	110-16-2200
39	109-22-0190	43	109-22-016B	47	109-22-020Q	51	114-40-286N
40	109-22-018B	44	109-22-015A	48	110-14-1540	52	114-40-286R

OPPORTUNITIES

- Multi-use area for employment
- Diversity of user types to support bike path
- Great scenic view sheds

CONSTRAINTS

- New COT Annexation
- Portions where trail right-of-way is not acquired

TANQUE VERDE CREEK - SABINO CANYON ROAD TO TANQUE VERDE ROAD



VACANT PRIVATE PARCELS in Unincorporated PC

53

114-30-009J

CONSTRAINTS

- All residential no commercial in the area
- Mostly built-out along the Loop
- Lacking connectivity to existing adjacent residential on the south side
- Loop only on north side of the wash

OPPORTUNITIES

- Encourage small business/restaurants along and connecting to Bike Loop
- Recreation
- Bike to School Program
- Provide connections to Loop on North side
- Equestrian Access

JULIAN WASH—COUNTRY CLUB ROAD TO ALVERNON WAY



VACANT PRIVATE PARCELS (COT & PC)

- | | | | |
|----|-------------|----|-------------|
| 61 | 132-20-002H | 65 | 140-06-2800 |
| 62 | 140-08-033A | | |
| 63 | 140-08-0060 | | |
| 64 | 140-07-0160 | | |

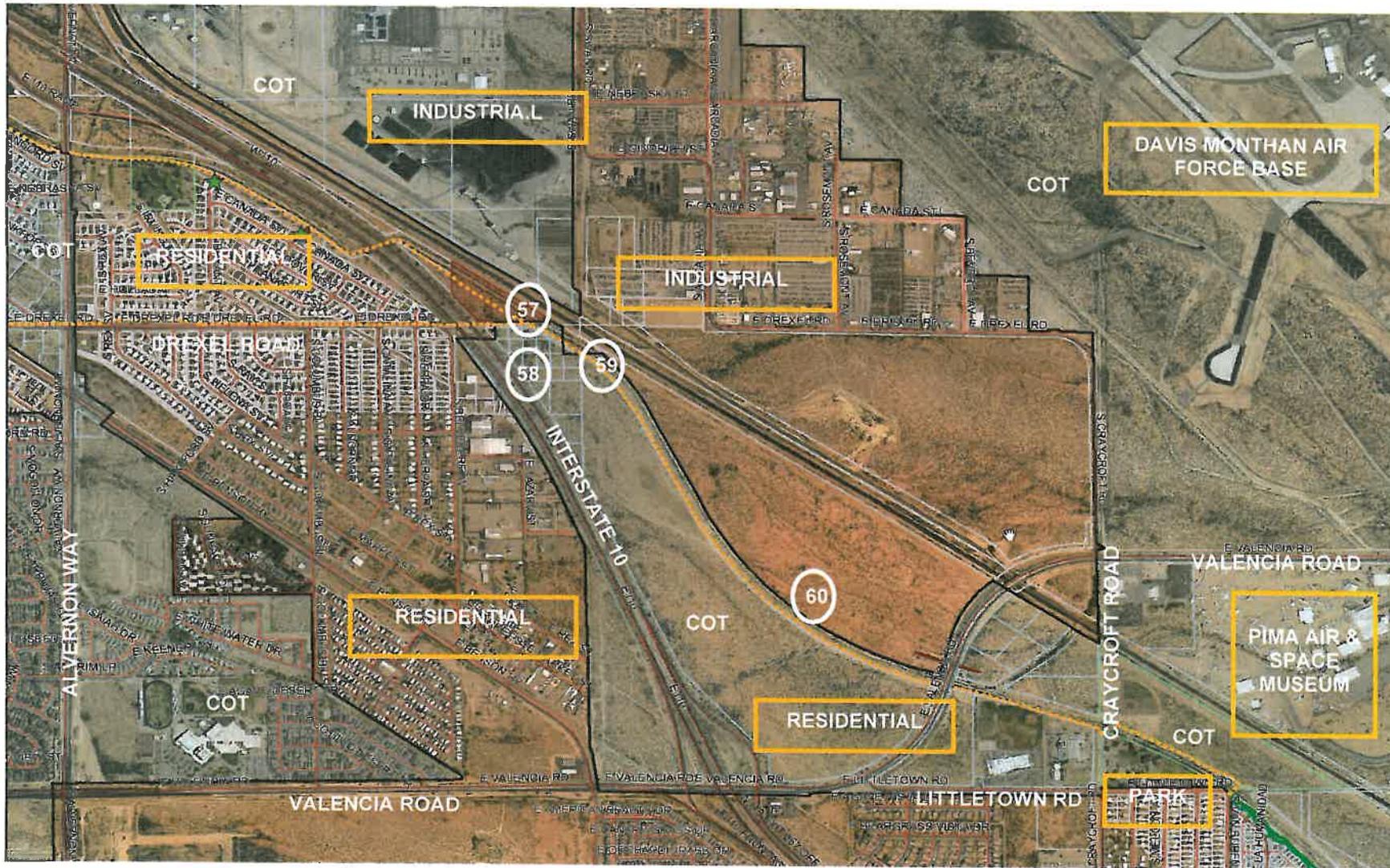
CONSTRAINTS

- Poor Scenic Quality—Not buffered from industrial or I-10
- Long Distant views blocked by TEP Plant
- Railroad Noise
- Air Quality

OPPORTUNITIES

- Capitalize on connections with tourism
- Additional land designated for trail and park expansion
- General area offers a diversity of user types to support bike path
- Landscaping to buffer uses
- Bike to Work & School Programs

JULIAN WASH—ALVERNON WAY TO CRAYCROFT RD.



VACANT PRIVATE PARCELS in unincorporated PC

57	140-02-023C
58	140-34-0760
59	140-34-0810
60	140-36-004C

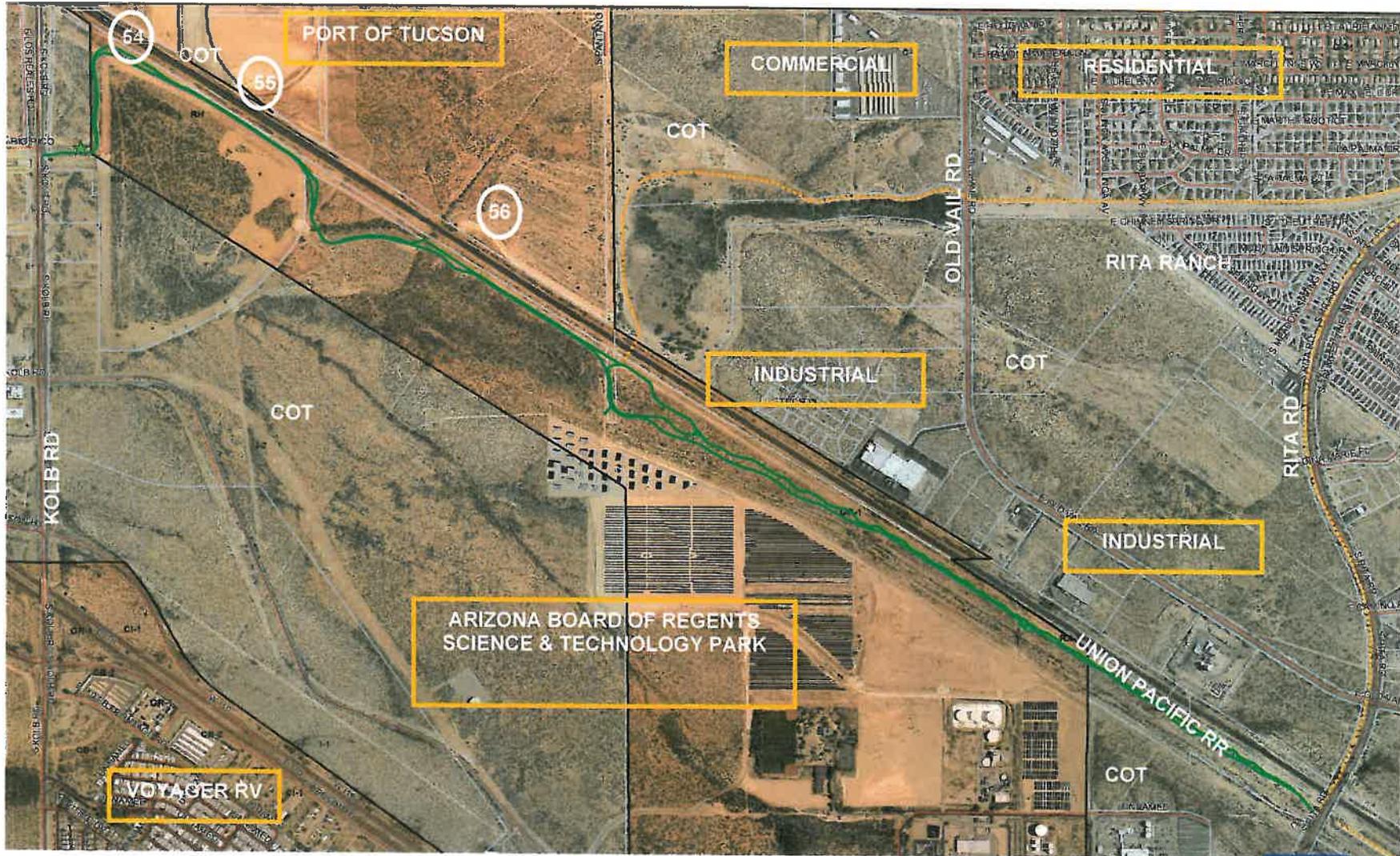
CONSTRAINTS

- Poor Scenic Quality—Not buffered from industrial or I-10
- Long Distant views blocked by Davis Monthan Airport
- Airport & Railroad Noise

OPPORTUNITIES

- Capitalize on connections with tourism
- Additional land designated for trail and park expansion
- General area offers a diversity of user types to support bike path
- Landscaping

JULIAN WASH—KOLB ROAD TO RITA ROAD



VACANT PRIVATE PARCELS in unincorporated Pima County

- 54 141-07-0340
- 55 141-07-0370
- 56 141-10-001B

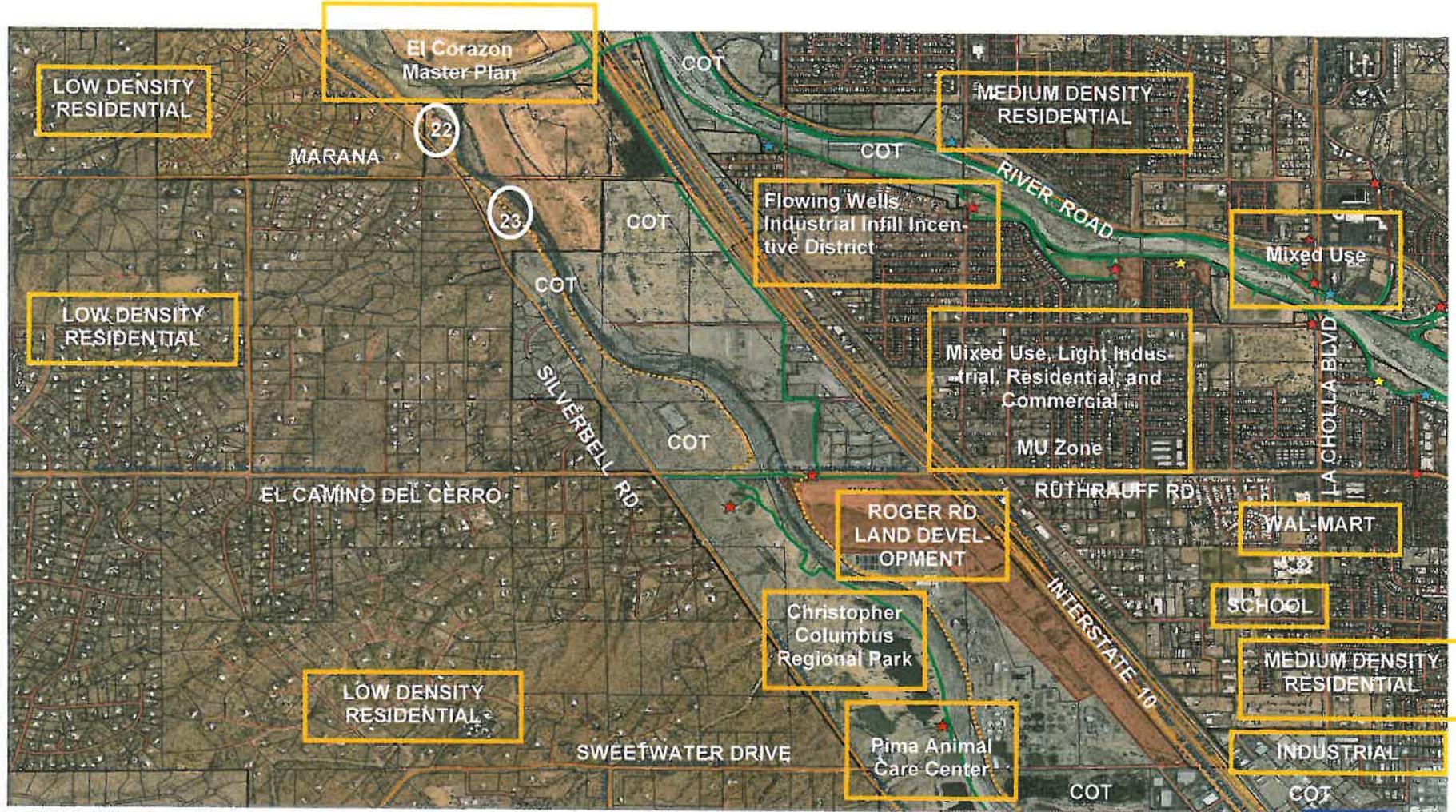
CONSTRAINTS

- Poor Scenic Quality—Not buffered from industrial
- Open, Barren
- Railroad Noise
- Poor connections across RR tracks

OPPORTUNITIES

- Residential and Employee Users—Bike to Work
- UofA Research & Development Center
- Landscaping to buffer uses
- Large Undeveloped Industrial Parcels
- Tourism with RV Resort
- Housing for workforce

SANTA CRUZ RIVER FROM RILLITO WASH TO SWEETWATER DRIVE



VACANT PRIVATE PARCELS in Unincorporated PC

- 22 101-06-009F
- 23 101-21-0010

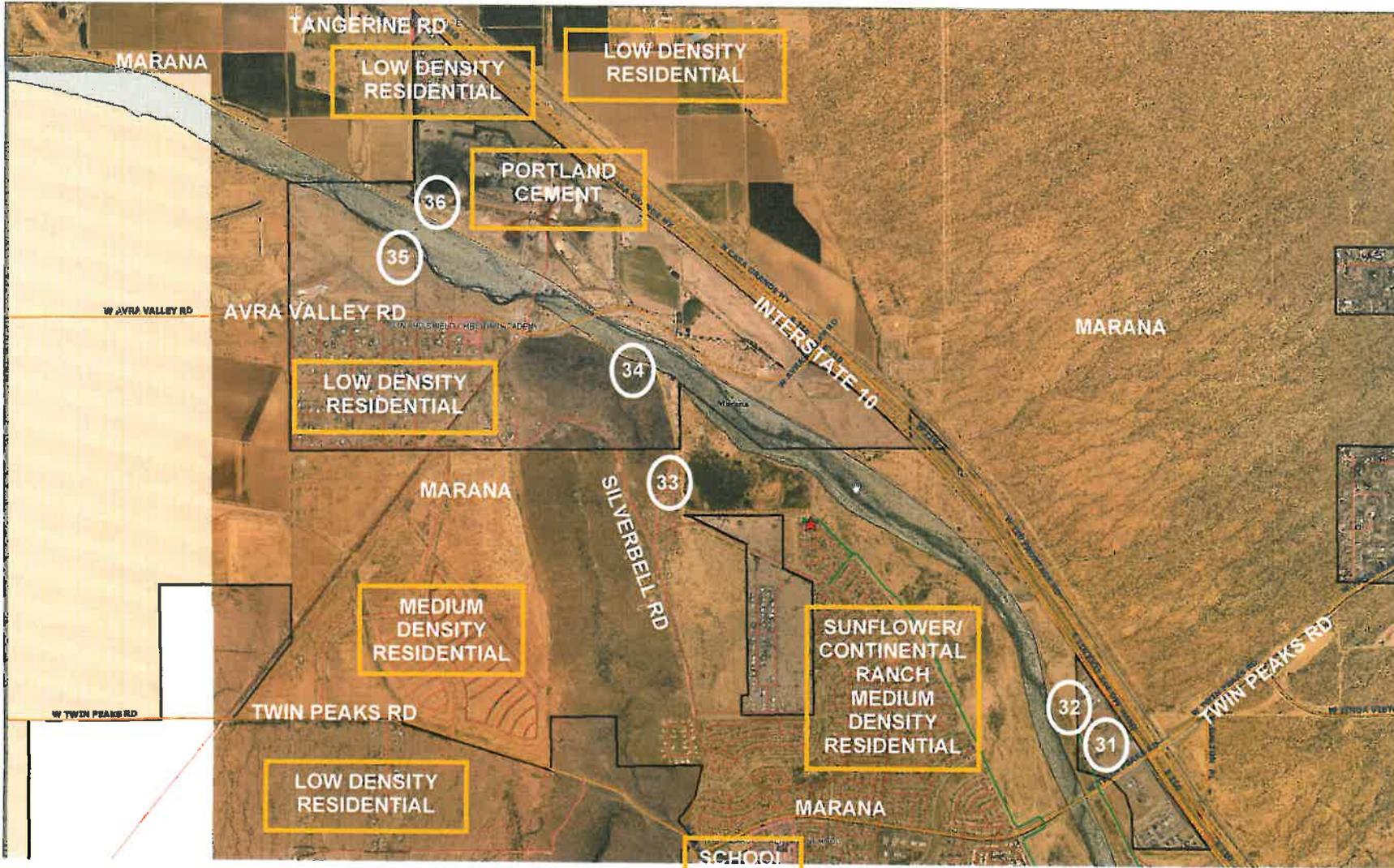
CONSTRAINTS

- Limited Connectivity to existing adjacent residential and across I-10
- No Commercial or Retail Centers along the Loop in this area
- The Loop is not completed on both sides of Santa Cruz River
- Majority of Loop in this stretch is in COT; small portion in PC and small portion is in Marana

OPPORTUNITIES

- Activities, Recreation, and Events
- Signage
- Dog Walking
- Enhance Connection to Rillito Wash and Silverbell Road

SANTA CRUZ RIVER - FROM TANGERINE RD TO TWIN PEAKS RD



VACANT PRIVATE PARCELS in unincorporated PC

31	226-15-015A	35	216-14-0020
32	226-08-010B	36	216-14-001G
33	226-03-0170		
34	226-01-009K		

CONSTRAINTS

- Lacking visual quality

OPPORTUNITIES

- Recreation, trail, and park enhancement
- Bike to Work and School
- Connectivity for existing Residential
- Landscaping
- Open farm land
- Long Distance Views

ATTACHMENT C

LANDSCAPE DESIGNATION- Premium

A

Characteristics

ROW 80' to 100' and beyond

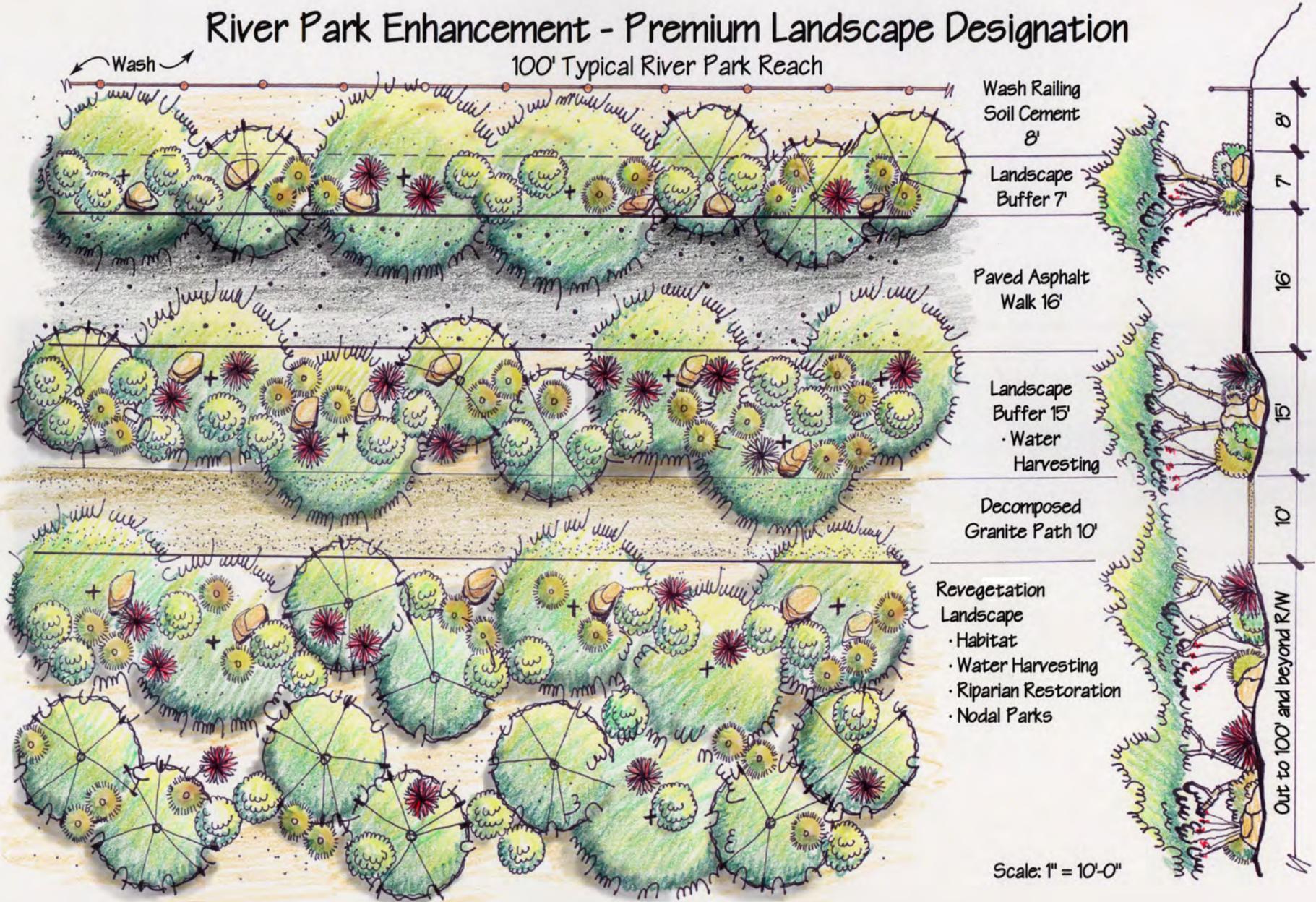
CANOPY 80% TO 100%

1. Canopy provides significant shade along pathway on both sides or often arbors over pathway
2. Opportunity for or existing restoration and/ or recreation located adjacent
3. Pathway typically meanders
4. Understory is consistent; dense in many areas
5. Pedestrian node may be dense or moderately shaded and include amenities
6. In some areas water, turf, structures and / or art may be visible nearby



River Park Enhancement - Premium Landscape Designation

100' Typical River Park Reach



LANDSCAPE DESIGNATION - High

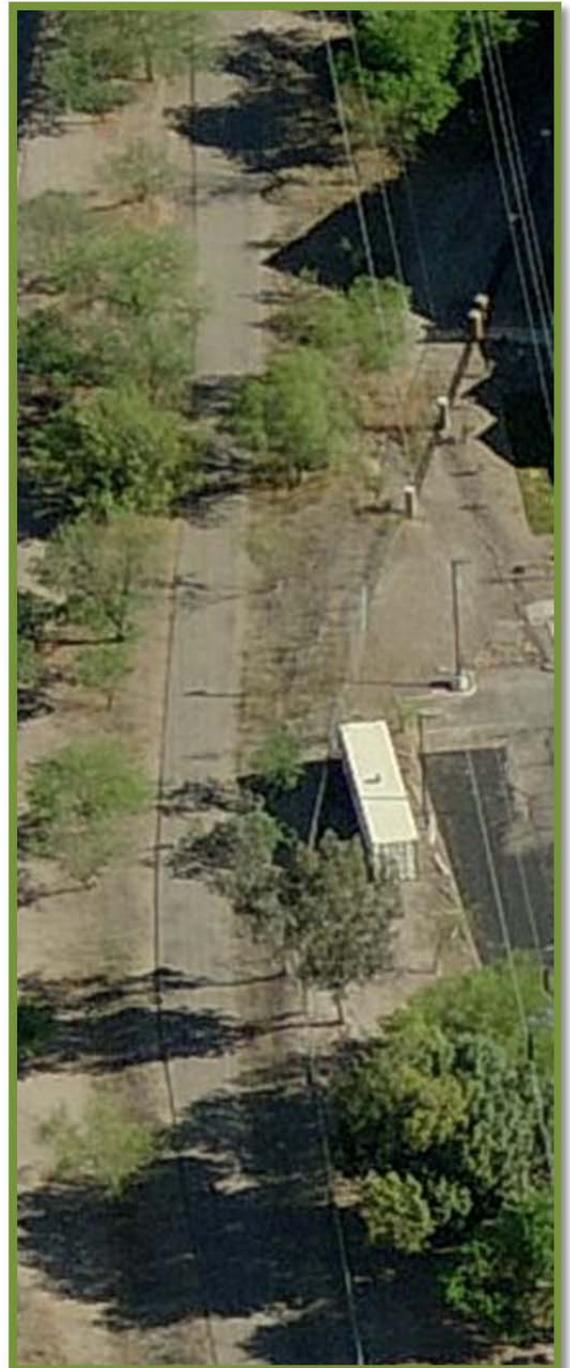
B

Characteristics

ROW 60' to 80'

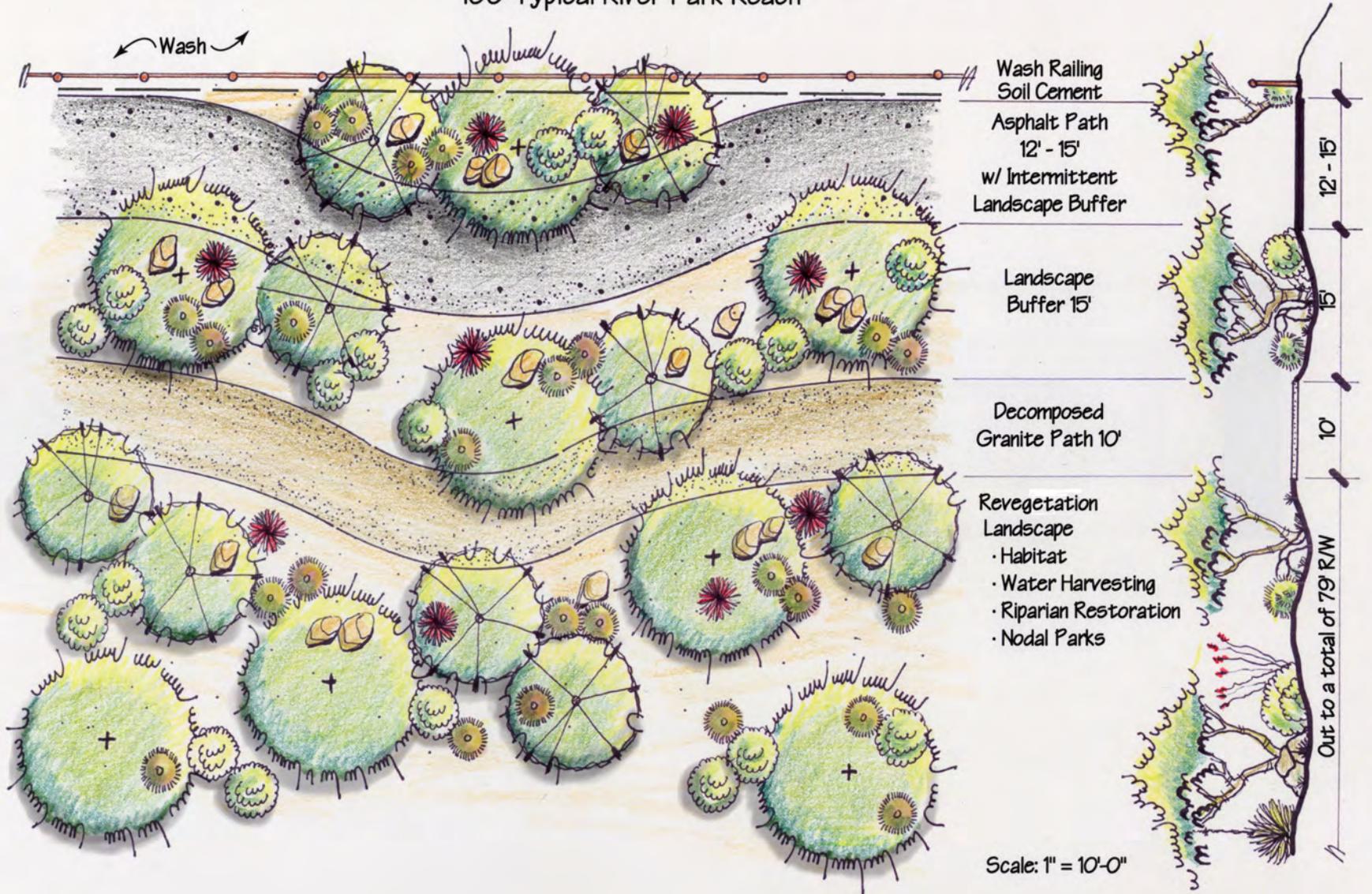
CANOPY 50% to 80%

1. Canopy is typically consistent on both sides of pathway providing recurrent shade
2. Occasionally canopy is very mature/ dense on only south side of pathway
3. Pathway usually meanders away from top of bank railing
4. Understory is somewhat consistent and moderately dense
5. Some restoration potential
6. If seating areas are present, typically there is some shade



River Park Enhancement - High Landscape Designation

100' Typical River Park Reach



LANDSCAPE DESIGNATION – Medium



Characteristics

ROW 25' to 60'

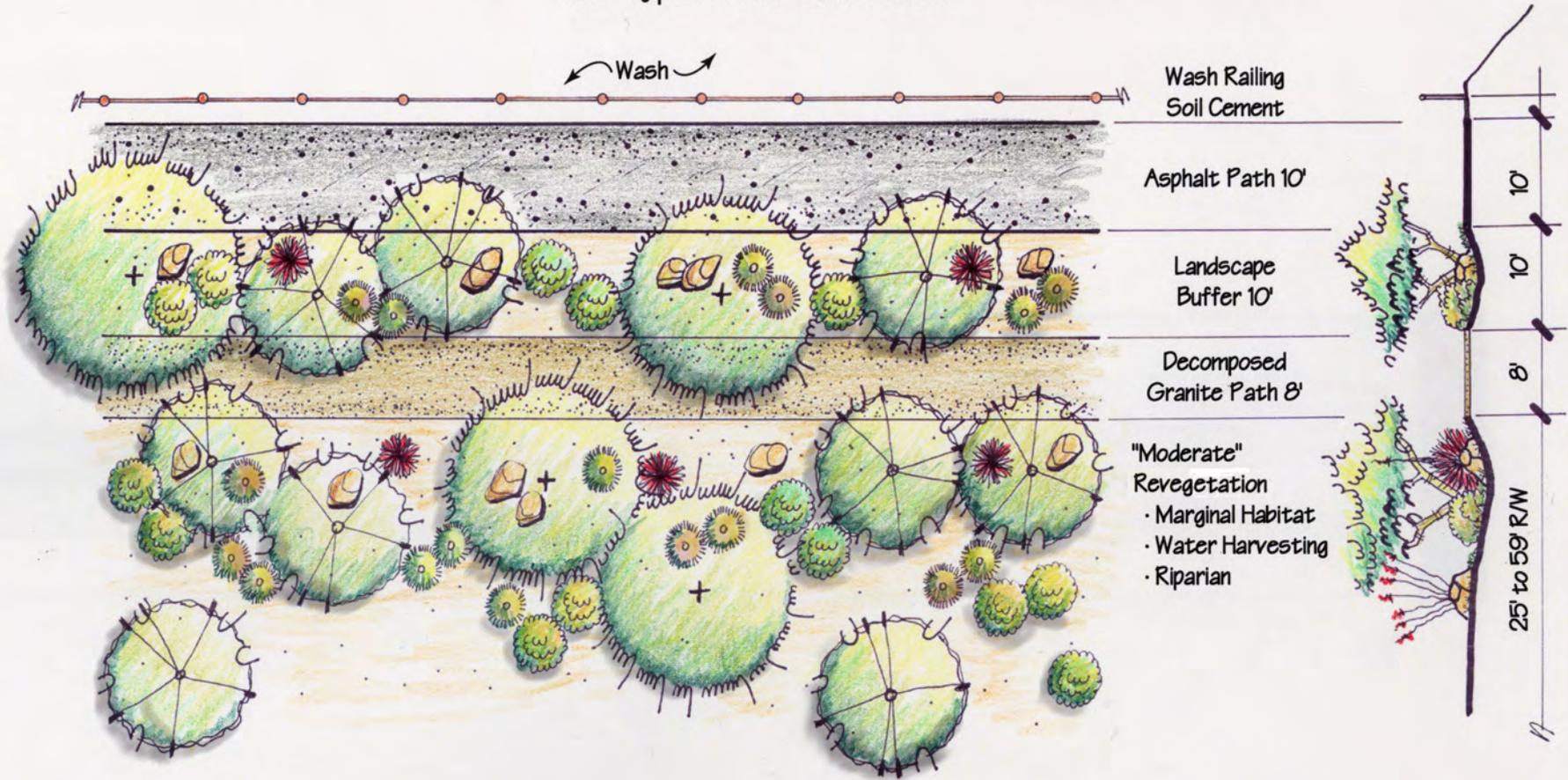
CANOPY 20% TO 50%

1. Canopy is sporadic providing infrequent shade along pathway
2. Canopy may also be consistent but on only one side of path; shade is present either in morning or afternoon
3. Pathway typically is located at top of bank by railing
4. Understory may be moderate to low density
5. If seating areas are available, may not be shaded



River Park Enhancement - Medium Landscape Designation

100' Typical River Park Reach



Scale: 1" = 10'-0"

LANDSCAPE DESIGNATION- Low

D

Characteristics

ROW $\leq 24'$

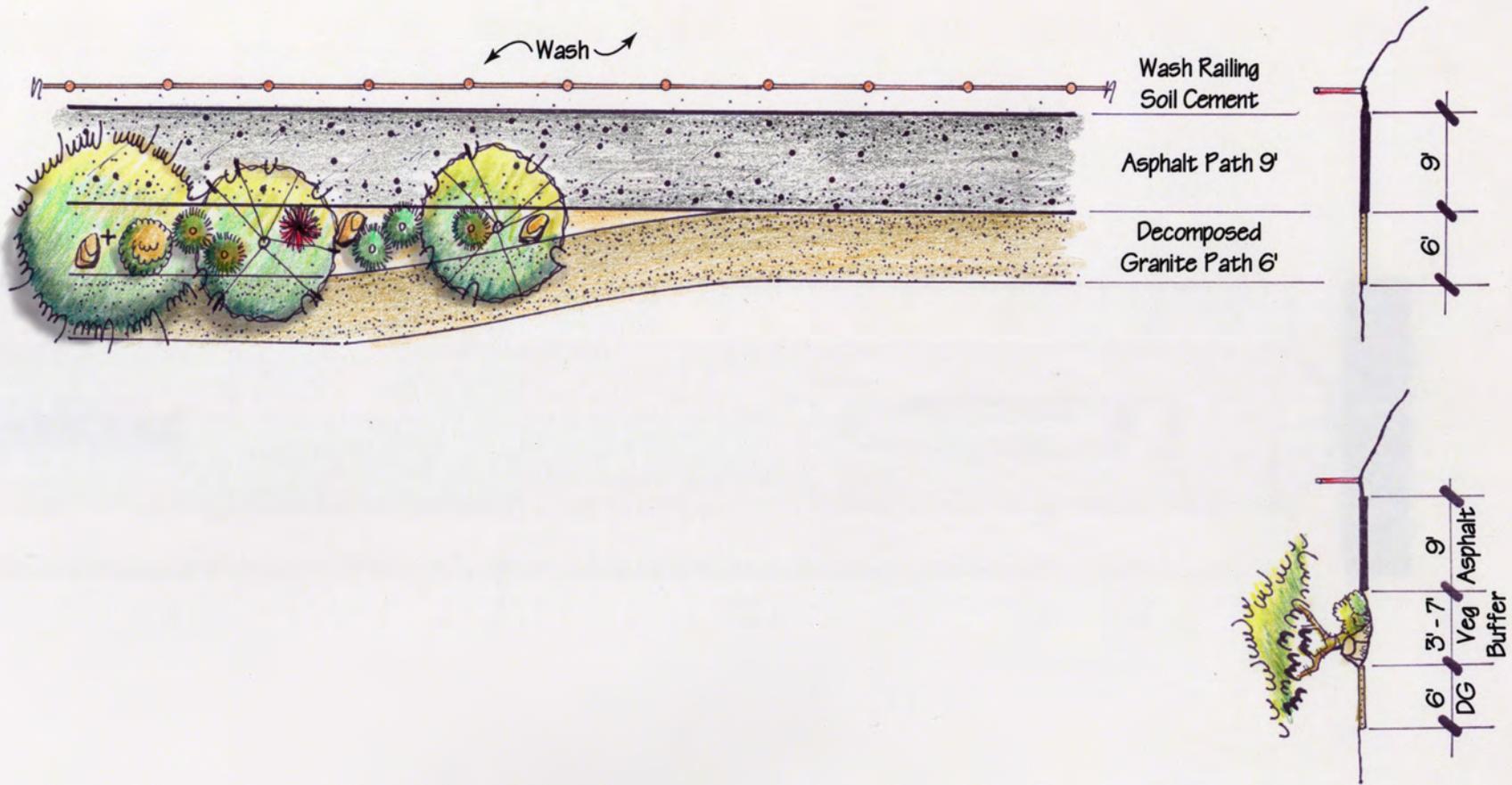
CANOPY 0% to 20%

1. No canopy or very isolated spacing
2. Pathway has no shade or respite from heat
3. Understory density is sparse to none
4. Predominant feature is hardscape



River Park Enhancement - Low Landscape Designation

100' Typical River Park Reach



Scale: 1" = 10'-0"

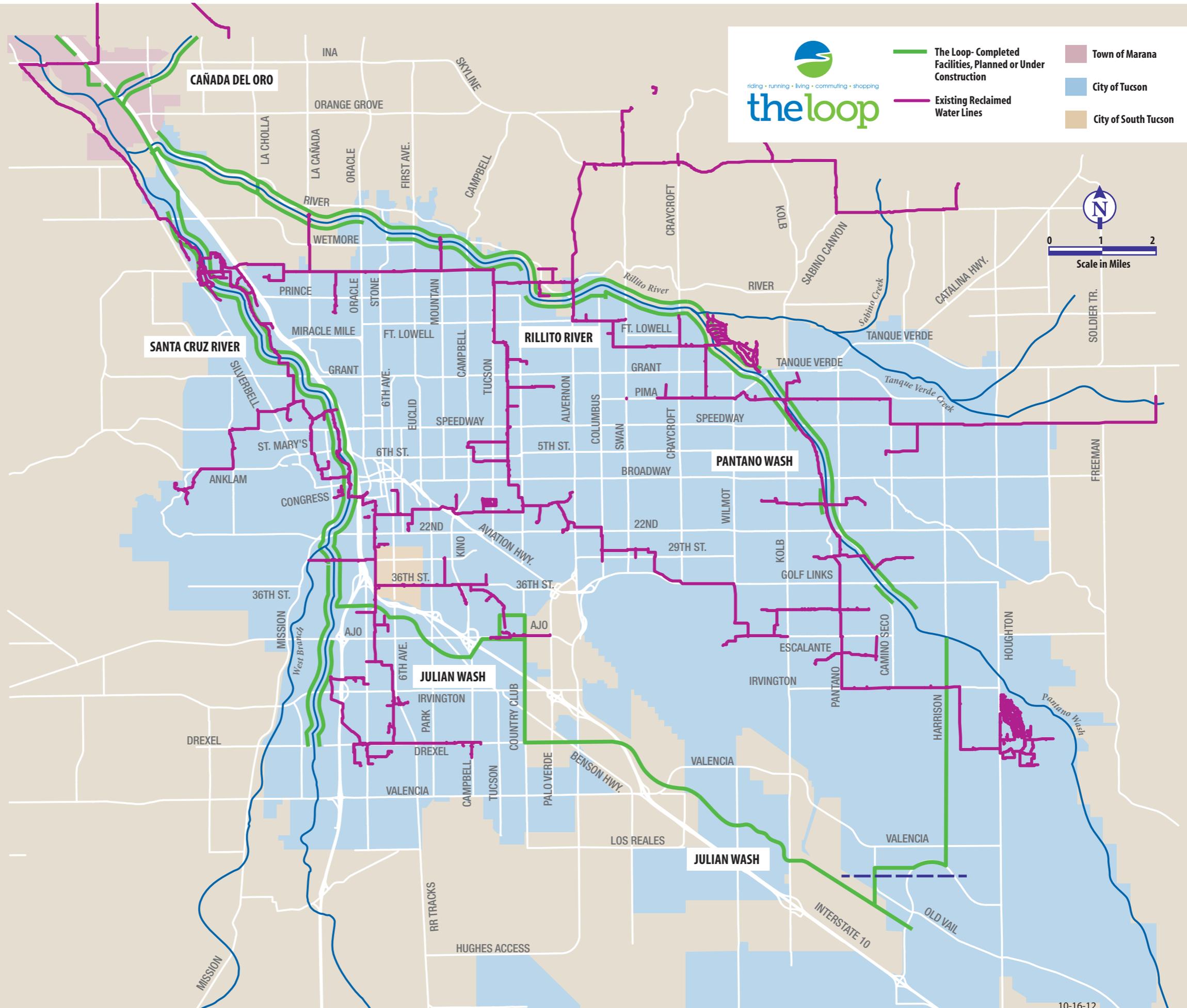
ATTACHMENT D



The Loop- Completed Facilities, Planned or Under Construction

Existing Reclaimed Water Lines

- Town of Marana
- City of Tucson
- City of South Tucson



ATTACHMENT E



Cañada del Oro east of Thornydale Road
\$150,000 est.



Rillito River west of La Cañada Blvd.
\$300,000 est.



Santa Cruz River near St. Mary's Road
\$250,000 est.



Julian Wash near Paseo de Lupe Eckstrom



Pantano Wash north of 22nd Street
\$100,000 est.



Julian Wash between Kolb Road and Rita Ranch
\$300,000 est.

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map No	Total Miles	Asphalt Width Feet	DG Path (Miles)	DG Path Width Feet	Asphalt/DG Path Notes	Irrigation (Y or N)	Reclaimed Availability Miles	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E (See Legend Below)	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed (10%, 20%, etc.)	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width feet	Bridge Width feet
Santa Cruz River																			
Valencia to Drexel (west bank)	S1		-	-	-		N	1.5	Y		E		100'+/-			Restoration/ recreational			
Valencia to Drexel (east bank)	S1		-	-	-		N	1.5	Y		E		100'+/-			Restoration/ recreational			
Drexel to Irvington (west bank)	S2	2.17	12	1.30			Y-Potable	0.6	Y		Currently D/ E. Re-evaluate after growth but likely to achieve C and possibly B along pathway spine.	Newly constructed	100'+/-			Restoration/ recreational. Provide dense canopied nodes each 1/4 mile.			
Drexel to Irvington (east bank)	S2	1.95	12	0.92	8		Y-Potable	0.6	Y	Y	Currently D/ E. Re-evaluate after growth but likely to achieve predominantly C and possibly B along pathway spine.	Newly constructed	100'+/-	2		Restoration/ recreational. Provide dense canopied nodes each 1/4 mile.			
Irvington to Ajo Way (west bank)	S3	1.17	10	-	8		Y-Potable	0	Y	Y	C (consistent veg, but sporadic canopy over pathway). Portion N half = D.		S: 100' to 500'. N: 30' to 50'	3		Enhance existing nodes. Provide dense canopied nodes each 1/4 mile.	Reclaimed at Irvington		
Irvington to Ajo Way (east bank)	S3	1.61	10	0.30		Soft path from Ajo south 1/3 of the way	Y-Potable	0	N				S: often 300' to 500'. N: 50' to 100'	3		Enhance existing nodes. Provide dense canopied nodes each 1/4 mile.	Reclaimed at Irvington		
Ajo Way to 29th Street (west bank)(Paseo project)	S4	-	-	-	8		N	0	N		E. Following evaluation based on 50% plans: Mostly A & B due to proposed restoration and large ROW. Small D areas. Node at Silverlake = A.	Paseo in planning. 50% plans reviewed.	Large areas over 100'. Some pinch points.	3		In design	Reclaimed at 29th St.		
Ajo Way to 29th Street (east bank)(Paseo project)	S4	-	-	-		Soft path from 22nd north about half way	N	0	Y		E. Following evaluation based on 50% plans: B, C & D throughout. North bank Julian & node at Silverlake = Restoration A.	Paseo in planning. 50% plans reviewed. No pathway Ajo to Julian.	Large areas over 100'.	1 (parking trailhead)		In design	Reclaimed at 29th St.		
29th Street to 22nd Street (east bank)	S5	1.15	12	0.52	-		Y-Potable	0	Y		B/ C		100' to over 300'	1		Add canopy to nodes	Reclaimed at 29th St.		
29th Street to 22nd Street (west bank)	S5	1.15	12	0.58	8		Y-Potable	0	Y				Generally 30' to 50'. Never 100'	0		Provide seating at 29th and 22nd in canopy area. Increase C to B.	Tucson Water: Just South of Broadway Road about a 1/4 mile, there is a reclaimed main that crosses the Santa Cruz. The park could be connected at this location on both sides of the Santa Cruz.		
22nd Street to Congress (east bank)	S6	2.00	12	0.66	8		Y-Potable	0	Y		B/ C		South and midway have areas well over 100'. Remaining generally 60'.	3		Many areas where canopy could be placed on both sides of trail to increase comfort. Increase canopy at seating nodes.	COT reclaim fed from Cushing St. with meter on south edge of new bridge to Mission Lane.		
22nd Street to Congress (west bank)	S6	2.03	12	0.78	8		Y-Potable & Reclaimed	0	Y				Narrowest 80'. Typically 300' or more.	1		Possible restoration/ enhance large node at Mission Rd/ 22nd St, especially at art wall. Other large ROW located on landfill- harder to develop. Where space on both sides path, increase canopy density to high.	COT reclaim meter at Alameda with controller at Garden of Getsamanes. Reclaim line runs from St. Mary's - Mission Lane.		
Congress to St. Mary's (east bank)	S7	0.67	12	-	8		Y-Reclaimed	0	Y				South 1/2 averages > 100'. N 1/2 70'.	1		Enhance node/ park at Congress.	COT reclaim fed from Cushing St. with meter on south edge of new bridge		
Congress to St. Mary's (west bank)	S7	0.75	12	-	-		Y-Reclaimed	0	Y				South 1/2 and northmost averages > 100'. Remaining 50' to 70'.	2		Enhance adjacent node at Congress	COT reclaim meter at Alameda with controller at Garden. Reclaim line runs from St. Mary's - Mission Lane.		
St. Mary's to Speedway (east bank)	S8	0.49	12	-	-		Y-Reclaimed	0	Y				Varies	2		Enhance 2 High nodes; create seating. Provide additional canopy in Medium areas.	COT reclaim fed from Cushing St. with meter on south edge of new bridge		
St. Mary's to Speedway (west bank)	S8	0.54	12	-	-		Y-Reclaimed	0	Y				Varies	4		Enhance canopy in area adjacent to park and at nodes.	Reclaimed-756 & 757 N. Riverside - 2 Meters supply Speedway to St. Mary's (west bank).		
Speedway to Grant (east bank)	S9	1.46	12	-	-		Y-Reclaimed	0	Y	Y			Averages from 30' to 80'. Over 100' slightly north of Speedway	2		Upgrade canopy and improve seating at nodes. Entire length needs additional canopy.	Reclaimed-830 & 831 W Speedway- 2 Meters supply Grant to Speedway (both banks)		
Speedway to Grant (west bank)	S9	2.66	12	0.28	-	Soft path north of Speedway about 1/4 way	Y-Reclaimed	0	Y	Y			Generally over 100'	2		Much of northern pathway can be shaded on both sides- upgrade this area and nodes.	Reclaimed-830 & 831 W Speedway- 2 Meters supply Grant to Speedway (both banks)		

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width
Grant Road to Sweetwater (east bank)	S10	0.14	-	-	-		N	0			E. C next to 360 l.f. of path		Averages 80'	0		Wonderful opp'ty to interface with Sweetwater Wetlands. Develop per Landscape Standards.	Tucson Water - approximately 1/2 mile north of Grant, west of I-10, a reclaimed line crosses the Santa Cruz. The park could be connected at this location, on both sides of the Santa Cruz.		
Grant Road to Sweetwater (west bank)	S10	3.36	12	-	-		Y-Potable	0	Y		E/ D	New asphalt-only project. Plans for veg?	Varies from path width to > 100'	0		North has ROW for path width only but potential to interface with adjacent municipal parcels including golf. Canopy could be installed on adjacent parcel. Potential node & 100' ROW south of Ft. Lowell (although portion of path at top of berm) and at Grant. Follow Standards to develop.			
Sweetwater to Camino del Cerro (east bank)	S11		-	-	-		N	0			E	Part of future Bond project	Generally path width	0		Very limited ROW. Possibly partner with adjacent properties.			
NOTE: from this location to Ina Road, all projects are subject to the overlay of El Corazón de los Tres Ríos del Norte (El Corazón), a multi-project initiative to create a model multi-faceted recreation zone. The urban-model Anza Trail will be developed on the Santa Cruz west bank and the River Park Divided Urban Path system will be developed on the east bank. The Corazón paths will be linked to all existing and proposed pathway systems. The interim paths may be abandoned.																			
Sweetwater to Camino del Cerro (west bank)(El Corazón)	S11	1.41	12	0.03	-		N	0			E/ D	New asphalt-only project. Veg included in El Corazon.	ROW of municipal land typically exceeds 100'	0		Very good opportunity to share resources with adjacent existing Columbus Park and Silverbell Golf.	Reclaimed at Columbus Park		
Camino Del Cerro to Ina (Future El Corazón Anza Trail) (west bank)	S12	4.71	12	-	-		N	0.6			E	El Corazon		0		Follow Anza Trail Guidelines and Landscape Standards	Tucson Water: For portions of this near the county's new Water Campus, and on the east side of the Santa Cruz, the county could easily connect the park to the Water Campus effluent, and permit the park for Class B+ water use. If the County wanted to also provide water to the west side of the Santa Cruz, a river crossing (and 404 permit) would be required.		
Camino del Cerro to Ina (Interceptor alignment/ interim) (east bank)	S12	5.11	14	-	-	8 foot path from Ted Walker to Ina	N	2.5			E/ D. Small area of C near Ina			0		This path will receive minimal improvements based upon the time line of El Corazón improvements. The ultimate River Park pathway will follow the Santa Cruz bank.	Tucson Water: For portions of this near the county's new Water Campus, and on the east side of the Santa Cruz, the county could easily connect the park to the Water Campus effluent, and permit the park for Class B+ water use. If the County wanted to also provide water to the west side of the Santa Cruz, a river crossing (and 404 permit) would be required.		
Temp ADOT paths Sunset to CDO Wash	S12	?	12	-	-		N	3.5			E/ D			0		These temporary links cross ADOT property to provide connectivity to the Interceptor alignment/ interim pathway	Tucson Water: County could permit this portion of the park for class B+ water use, and supply this park with water from Ina Rd.PCRWRD and/ or the new Water Campus.		
Camino del Cerro to Ina (Future El Corazon River Park) (east bank)	S12	?	12	-	-						E	El Corazon		0		Develop in conjunction with el Corazón. Follow Landscape Standards.			
Ina to Cortaro (west bank)	S13	2.67	14	1.24	-		Y-Potable	0	Y	Y	E/ D	Asphalt-only project	Marana. Unknown	1	O	Unknown or no ROW- possible opportunity to interface with Silverbell District Park near Cortaro. Enhance node at Cortaro. If ROW available, optimal area for riparian restoration. Provide overlooks to riparian/ flowing water.	Tucson Water: A new main over 5 miles long would be required to supply water to this park.		
Ina to Cortaro (east bank)	S13	-	14	-	-		N	0			E		Marana. Unknown	0		Unknown or no ROW. If ROW available, optimal area for riparian restoration. Provide overlooks to observe riparian/ flowing water.	Tucson Water: A new main over 5 miles long would be required to supply water to this park.		
Cortaro to Twin Peaks (west bank)	S14	2.70	14	-	-	Good room to install soft path	Y-Potable	2.3	Y	N	E/ D		Marana. Unknown	1	25%	Enhance node near Redrock Dr. Opportunity to share landscape with Wade McLean Park. Follow Landscape Standards to upgrade.	Reclaimed at Camino de Oeste		

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width	
Cortaro to Twin Peaks (east bank)	S14	2.00	14	-	-		N	2.3			E/ D. Some C (includes an area of borrowed landscape from golf course)		Marana. Appears very narrow	0		Follow Landscape Standards to upgrade. Very good oppty to shift asphalt path or add dg path right up to mature canopy from north edge golf course to Tiffany (north end)				
Twin Peaks to N. boundary Continental Ranch (west bank)	S15	1.74	14	-	-	Good room to install soft path	N	3.9			E/ D. Very small area of C at north end of subdivision.		Marana. Possibly averaging 100' or more if adjacent subdivision partners.	2		Enhance node at Idle Wild Dr. and north end of subdivision. Possible nodes at side drainageways entering river.	Reclaimed Water line at Thornydale, but it is at capacity			
N. boundary Continental Ranch to Avra Valley Road (west bank)	S15	-	14	-	-	Good room to install soft path	Y-Potable		Y-Potable	N			Marana. South half municipal-potentially 100's of feet available. North half private.	0	20%	Possible riparian restoration/wetlands on municipal land				
Twin Peaks to Avra Valley Road (east bank)	S15	0.22	-	-	-	1100' pathway from Twin Peaks to north	N	3.9			E		Marana. Unknown.	0		Follow Landscape Standards to upgrade.	Reclaimed Water line at Thornydale, but it is at capacity			
Avra Valley Road to Tangerine (west bank)	S16	-	-	-	-		N	7	N	N	E		Marana. Mostly private or State land.	0		Follow Landscape Standards to upgrade.	Reclaimed Water line at Thornydale, but it is at capacity			
Avra Valley Road to Tangerine (east bank)	S16	-	-	-	-		N	7	N	N	E		Marana. Unknown.	0		Follow Landscape Standards to upgrade.	Reclaimed Water line at Thornydale, but it is at capacity			
Tangerine to Sanders Road (west bank)	S17	-	-	-	-		N	7	N	N	E		Marana. Unknown.			Provide overlooks to observe riparian/ flowing water.	Reclaimed Water line at Thornydale, but it is at capacity			
Tangerine to Sanders Road (east bank)	S17	1.85	14	-	-	New asphalt; Good room to install soft path	Y-Potable	7	Y	N	E. Mostly D. From north of N Heritage Park Drive to Sanders= very young C.		Marana. Possibly large areas of ROW for riparian restoration or recreation.			Enhance node near White Ave. Interface with Gladden Farms Neighborhood Park and Marana Heritage Park. At north end, assess value of new plantings. Provide overlooks to observe riparian/ flowing water.	Reclaimed Water line at Thornydale, but it is at capacity			
Santa Cruz River Total		45.71	336.00	6.61	56.00			51.20												
Rillito River																				
Craycroft to Swan (north bank)		2.10	10	1.00	8		Y-Reclaimed		Y		C/ D. At Craycroft=B. Trees north of pathway provide minimal shade.		50' to 80'. At Craycroft node to 300'	1		Enhance Craycroft trailhead with additional canopy & seating. Create level B node at Swan and increase density of C canopy. ROW restrictions prevent increasing C canopy to B.	Reclaimed-3800 N Alvernon-2 Meters supply Dodge to Craycroft (north bank)			
Craycroft to Swan - (south bank)	R1	1.93	12, 13	1.70	8		Y-Reclaimed		Y				1/3 minimal to 50'. 2/3 over 300'	1		Possible riparian restoration or recreation. At wide ROW, relocate asphalt pathway to allow canopy on both sides. Increase C canopy to B. Enhance equestrian trailhead at Swan from C to B canopy.	Reclaimed-3400 N Alvernon-1 Meter supplies Alvernon to Craycroft including the Swan Wetlands (30 ac. ft)- (south bank)			
Swan to Columbus (north bank)	R2	1.16	10, 12	0.58	8		Y-Reclaimed		Y		C. Node of B at Swan. DG paths mostly D. Trees north of pathway provide minimal shade.		100' min to frequently up to 300'			Many opportunities to increase canopy density wherever pathway has space on both sides.	Reclaimed-3800 N. Alvernon-1 Meter supplies Dodge to Craycroft (north bank)			
Swan to Columbus (south bank)	R2	1.15	12, 13	0.99	8		Y-Reclaimed		Y				Varies from < 30' to >300'			Possible recreational and/ or restoration opportunity. Some opportunities to realign pathway to allow canopy density on both sides.	Reclaimed-3400 N. Alvernon-1 Meter supplies Alvernon to Craycroft including the Swan Wetlands (30 ac.ft)- (south bank)			
Columbus to Alvernon (north bank)	R3	0.91	11, 12	0.21	-		Y-Reclaimed						Over 100' near Columbus, center segment adjacent to Mehl Park, 30' near Alvernon.			Some opportunities to realign pathway to allow canopy density on both sides. Interface with Mehl Park. Increase D canopy to C minimum.	Reclaimed-3800 N. Alvernon-2 Meters supply Dodge to Craycroft (north bank)			

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow augmentation)	Reclaimed Water Comments	Underpass Width	Bridge Width
Columbus to Alvernon (south bank)	R3	1.62	11, 12	0.55	8		Y-Reclaimed	0	Y				80' along residential to potential acquisition of 300'.			Many opportunities for improvements. Possible restoration in floodplain near Columbus, recreational opportunity by existing parking/ trailhead. Potential node improvement at Alvernon.	Reclaimed-3400 N. Alvernon-1 Meter supplies Alvernon to Craycroft including the Swan Wetlands (30 ac.ft)-(south bank)		
Alvernon to Dodge (north bank)	R4	0.43	11	0.06	-		Y-Reclaimed	0	Y		B & C w/ small D at Alvernon		Up to 200'			Primarily D with ample space to upgrade to a C or B node.	Reclaimed-3800 N. Alvernon-2 Meters supply Dodge to Craycroft (north bank)		
Alvernon to Dodge - (south bank)	R4	0.31	-	0.20	-		N	0	N	N			Average 50'			Existing veg has potential. Follow landscape Standards. Partner with adjacent church property?	No irrigation		
Dodge to Country Club (north bank)	R5	2.76	11, 12	1.11	8		Y-Reclaimed	0	Y		E		Average 30' to 60'. "Borrowed" ROW to over 1000' at Brandi Fenton Park.			Upgrades needed, especially along Brandi Fenton Pak to compliment lush park area. Remainder to B or minimal C designation.	Reclaimed-3616 N. Edith-1 Meter supplies Dodge to Country Club (North Bank)		
Dodge to Country Club (south bank)	R5	0.64	-	0.64	-		N	0	N	N			No access to 50' average.			Bring to B or minimal C canopy designation.	No irrigation		
Country Club to Hacienda del Sol - (north bank)	R6	0.46	12	0.09	-		N	0	N	N			Averages below 50' to path access only.	0		Upgrades needed but existing narrow ROW steeply sloped- no canopy room. Enhance small area of C to 50% density.	No irrigation		
Country Club to Hacienda del Sol (south bank)	R6	0.68	12	0.29	8		Y-Reclaimed	0	Y	Y	D.		60' to path width only	0		Upgrades needed from D to dense C canopy	Reclaimed-Irrigation goes 80 yards east of Cactus Blvd.		
Hacienda del Sol to Campbell (north bank)	R7	1.22	12	0.47	8		Y-Reclaimed	0	Y	Y			Average 50' and some areas asphalt path only.	1		Upgrades needed from D and C canopy to dense C (pathway at top bank so only one side available for planting).	Reclaimed-1500 E. River - 2 Meters supply Stone to Hacienda del Sol (north bank)		
Hacienda del Sol to Campbell (south bank)	R7	1.69	12	0.77	8	Soft path goes 1/2 mile west of Hacienda del Sol	Y-Reclaimed	0	Y	Y	Mostly C. Some D		Average ROW 75' to 80'	2		ROW adequate for dual paths. No room to realign asphalt except at Rio Vista Park. Focus upgrades here and at Campbell node. Increase density of C canopy.	Reclaimed-1321 E. Prospect Lane - 2 Meters supply Stone to Cactus Blvd. (South Bank)		
Note: both banks Campbell to 1st Avenue have potential to become a Premium high canopy segment. The foundation for consistent dense canopy is there. In this segment do not recommend adding dg path where not existing. It would destroy the foundation.																			
Campbell to Mountain (north bank)	R8	0.63	10	-	-		Y-Reclaimed	0	Y	Y			At Campbell path width only. Then averages 50' with > 100' at Mountain			The B canopy in this area is ideal for development to linear nodal park. Increase canopy density B to A and focus on denser canopy at numerous nodes. Upgrade density of C canopy.	Reclaimed-1500 E. River - 2 Meters supply Stone to Hacienda del Sol (north bank)		
Campbell to Mountain - (south bank)	R8	0.94	8	0.38	4, 8		Y-Reclaimed	0	Y	Y			60' to 90'			Improve node at Campbell. Possible node near Mountain/ ped bridge. Enhance C canopy	Reclaimed-1321 E Prospect Lane - 2 Meters supply Stone to Cactus Blvd. (south bank)		
Mountain to First Avenue (north bank)	R9	0.89	10	-	-		Y-Reclaimed	0	Y	Y			Av < 50' with ~ 200' node potential at Mountain			The B canopy in this area is ideal for development to linear nodal park. Increase canopy density B to A and focus on denser canopy at Mountain and 1st Ave nodes. Upgrade density of C canopy. Interface with Rillito Downs parcel.	Reclaimed-1500 E. River-2 Meters supply Stone to Hacienda del Sol (north bank)		
Mountain to First Avenue (south bank)	R9	1.16	11, 12	0.51	8		Y-Reclaimed	0	Y	Y			East half 100', west half 85'			Opportunities to enhance mini-nodes and develop linear nodal park in 100' ROW.	Reclaimed-1321 E. Prospect Lane - 2 Meters supply Stone to Cactus Blvd. (south bank)		
First Avenue to Stone (north bank)	R10	0.62	10	-	-		Y-Reclaimed	0	Y	Y			20' to 50'			Good opportunities to upgrade C canopy to B due to planting area on both sides pathway.	Reclaimed-1500 e River-2 Meters supply Stone to Hacienda del Sol (north bank)		
First Avenue to Stone (south bank)	R10	0.55	13	-	-		Y-Reclaimed	0	Y	Y			20' to 50'			Increase density of D. Good opportunities to upgrade eastern C canopy to D due to available planting area on both sides pathway. Near Stone when C has pathway against top of bank, upgrade canopy density. Partner with SunTran.	Reclaimed-1321 E. Prospect Lane - 2 Meters supply Stone to Cactus Blvd. (south bank)		
						Hard path narrows to 9 near Stone		0			East 1/2 B. West 1/2 C.								

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augmentation)	Reclaimed Water Comments	Underpass Width	Bridge Width	
Stone to Oracle (north bank)	R11	0.63	10	-	-		Y-Reclaimed		Y	Y			20' to 80'	Scattered minor nodes		In C area at Stone, add canopy on north side of pathway and create node. In B area, increase canopy density on south side pathway. Upgrade D area at Oracle.	Reclaimed - 4840 & 4841 N. La Canada - 2 Meters supply La Canada to Stone including Children's Memorial Park (18 ac. ft.)			
Tucson Mall; Stone to Oracle (south bank)	R11	0.20	-	-	-	12' hard path 1,000 feet east of Oracle	Y-Reclaimed	0	Y	Y	E at Tucson Mall. Existing area near Oracle=B.		0' at Mall; 40' to 80' near Oracle.			Tucson Mall good location for another cantilever path like at Lowes Speedway. Upgraded seating amenities will result in Premium A designation here.	Reclaimed - 4760 N. Flowing Wells - 2 Meters supply La Canada to Stone (south bank)			
Oracle to La Canada (north bank)	R12	1.12	10	-	-		Y-Reclaimed	0	Y	Y			Small central area at 50' with Children's Memorial Park adjacent. Majority at 80' to 90'	2		Oppty to create node at Oracle. Plenty of space in C area to upgrade canopy density on south side pathway to B. focus west of Memorial Park. Increase density canopy at Children's Memorial Park. Improve node at La Canada.	Reclaimed - 4840 & 4841 N. La Canada - 2 Meters supply La Canada to Stone including Children's Memorial Park (18 ac. ft.)			
Oracle Road to La Canada (south bank)	R12	1.18	13	-	-		Y-Reclaimed	0	Y	Y			Small area toward La Canada over 100'. Remaining from path width to 60' average.	1		Focus: provide trees to produce canopy in D area. Upgrade node at Oracle. Area of wide ROW can support dg path. Create node at La Canada.	Reclaimed - 4760 N. Flowing Wells - 2 Meters supply La Canada to Stone (south bank)			
La Canada to La Cholla (north bank)	R13	2.95	10	1.23	-	Intermittent sections of soft path	Y-Reclaimed	0	Y	Y			Average 80' except near midpoint and drainage over 200'. Narrows at La Cholla.	minor		Focus: node at La Cholla. Many areas of C can easily be enhanced to B where room on south side of path. Enhance all nodes, except only on fringe of "Stonehenge", with canopy.	Reclaimed - 4845 N. Flowing Wells - 3 Meters supply La Canada to Shannon (north bank) and line crosses over the La Cholla bridge to supply the south bank to Camino de la Tierra including Flowing Wells and Curtis Parks - looped system (39 ac. ft.)			
La Canada to La Cholla (south bank)	R13	2.08	12	0.79	-	Intermittent sections of soft path	Y-Reclaimed	0	Y	Y			Majority averages over 100' with narrowing as low as 60' at west and east ends.	5		Focus: enhance trailhead near Shamrock Pl with amenities and more canopy, and create node at La Cholla. Create node at La Canada. Many opportunities to enhance C canopy on south side of pathway.	Reclaimed-4765 N.Flowing Wells - 3 Meters supply La Canada to Shannon - looped system (south bank)			
La Cholla to I-10 - (north bank)	R14	3.68	12	1.21	8	Soft path CDLT to La Cholla	Y/N	0	Y	Y			Minimal width except near Camino de la Tierra.			Focus: nodes at La Cholla, Shannon and trailhead at I-10; and upgrade of D to C. Enhance the few areas of C to B where room on south side of path. In some areas can create premium soft path experience due to ample room.	No irrigation at CDLT to RR (north bank); non potable-Latimore Well; Reclaimed - 4845 N. Flowing Wells - 3 Meters supply La Canada to I-10			
La Cholla to I-10 (south bank)	R14	2.64	11, 12	0.03	-		Y/N	0	Y	Y			Many areas of > 200' in conjunction with Flowing Wells Park.			Enhance access at La Cholla in conjunction with Curtis Park, and enhance west node at Camino de la Tierra. Enhance D to C at Curtis Park. Great opportunities to enhance C to B canopy throughout due to space on south side of asphalt pathway (dg as well)	Potable - CDLT to I-10; Reclaimed - 3 Meters 4765 N. Flowing Wells - La Canada-Shannon looped system; line cross over the bridge			
Rillito Total		36.33	187.00	12.81	88.00															
Pantano Wash													<i>Not provided</i>							
Harrison Connection: Julian to Valencia	P1	2.72					N									Follow Landscape Standards. Work with existing vegetation along Rita Road and native vegetation along Frost Drive alignment.	Reclaimed at Civano			
Harrison Connection: Valencia to Irvington	P1.5	3.05	-				N									Follow Landscape Standards. Work with existing native vegetation.	Tucson Water: Irvington and Harrison: A reclaimed main crosses Harrison Road at Irvington Road. A connection to the reclaimed system could be made at this location.			

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width
Harrison Connection: Irvington to Sellarole to Pantano (interim on-street)	P2		-				N	0			E					No improvements	Tucson Water: Irvington and Harrison: A reclaimed main crosses Harrison Road at Irvington Road. A connection to the reclaimed system could be made at this location.		
Harrison to Sellarole (north/east bank)	P2	0.67	-				N	1.2			E					Follow Landscape Standards	Irvington and Harrison: A reclaimed main crosses Harrison Road at Irvington Road. A connection to the reclaimed system could be made at this location. Harrison about 1 mile south of Irvington: Houghton Reclaimed Booster Station is about 1/2 mile east of the location of the greenway. A connection to the reclaimed water system could be made at this location.		
Harrison to Sellarole (south/ west bank)	P2	-	-				N	1.2			E					Follow Landscape Standards			
Sellarole Road to Golf Links (east bank)(south half)	P3	0.61	12	0.23			Y - reclaim	0	Y	Y	E/ D	NEED PLANS				Upgrade: at minimum provide dense canopied nodes with bench each 1/4 mile.			
Sellarole Road to Golf Links (west bank)(south half)	P3	-	12	-	6, 8		N	0			E					Follow Landscape Standards			
Sellarole Road to Golf Links (east bank)(north half)	P3	1.44	12	0.34	6,8		Y - reclaim	0	Y	Y	E/ D. Node @ GLinks = C.					Upgrade 2 nodes. At minimum provide new dense canopied nodes with bench each 1/4 mile.	Reclaimed at Golf Links		
Sellarole Road to Golf Links (west bank)(north half)	P3	0.56	12	-	6	Paths on west for 2,200 feet south of GL	N	0	Y	N	South = E. North = 1/2 C. 1/2 B.					Increase density of C whenever possible, but provide dense canopied nodes with bench each 1/4 mile.	Reclaimed at Golf Links		
Golf Links to 22nd Street (east bank)	P4	2.52	12	1.10	6, 8		Y - reclaim	0	Y		Sparse C for asphalt pathway; dense C for some areas of dg.					Increase trailhead at Golf Links and 22nd to Canopy B. Provide dense canopied nodes with bench each 1/4 mile- focus on areas when canopy can be provided on both sides pathway. Improve canopy at existing nodes near Thaxton Dr. and Estes Lane. Dg path has borrowed good canopy from roadway landscaping.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano river area. 404 permits would be required.		
Golf Links to 22nd Street (west bank)	P4	-	-	-	-		N	0			E					Follow Landscape Standards	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano river area. 404 permits would be required.		
22nd Street to Kenyon (east bank)	P5	1.15	12	0.54	8		Y - reclaim	0	Y		C. All native vegetation- path winds amongst sporadic canopy.	Fairly new installation- NEED PLANS				Wide ROW, creating great potential to enhance canopy to an A designation and provide habitat and/ or passive recreational activates (picnicking, seating areas). Increase canopy at all existing nodes. Focus on 22nd St node.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
22nd Street to Kenyon (west bank)	P5	-	-	-	-		N	0			E					Follow Landscape Standards	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
Kenyon to Broadway (east bank)	P6	-	-	-	-		N	0			E					Follow Landscape Standards	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
Kenyon to Broadway (west bank)	P6	0.15	8	-	-	8-foot paved path for 1/8 mile south of Broadway	N	0			E. Along 1/8 mi path=C					Increase canopy whenever possible. Little opportunity for nodes unless partner with private property.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
Broadway Underpasses (both banks)	P6	-	-	-	-		N	0											
Broadway to 5th Street Wash (east bank)	P7	0.86	12	0.38	8	Paths only from Broadway to 5th Street Wash.	N	0	Y		Small piece of B at Broadway; C at Wash. Dg path has more B.					Increase canopy at Broadway and Center park nodes.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width
Broadway to 5th Street Wash (west bank)	P7	-	12		-		N	0	N		C at Broadway (existing adjacent vegetation). Remaining = D.					Follow Landscape Standards for undeveloped area. Little opportunity; increase canopy whenever possible.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
5th Street Wash to Speedway (east bank)	P8	0.22	-		-		N	0			C for 700' south of Speedway. Remaining = E.					Follow Landscape Standards for undeveloped area. Increase amenities (benches) and C canopy to B at Speedway node.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
5th Street Wash to Speedway (west bank)	P8	0.33	-		-		N	0			C for 1000' south of Speedway. Remaining = E					Follow Landscape Standards for undeveloped area. Increase canopy whenever possible. Little opportunity for nodes unless partner with private property.	Tucson Water: A reclaimed main parallels the Pantano River from Speedway Blvd. all the way to 22nd Street. This main is mostly in the Pantano River area. 404 permits would be required.		
Speedway to Tanque Verde (east bank)	P9	1.26	12		-		Y-Reclaimed				E	NEED PLANS.				New path, no landscape installed. Evaluate after installation?			
Speedway to Tanque Verde (west bank)	P9	1.27	12		-		Y-Reclaimed		Y		E. At Lowes=B. Small commercial = C. Remaining = E.	NEED PLANS.				If possible, at Speedway, partner with commercial to create node/ entry statement/ trailhead. At Lowes, shade trees, ramadas and art create High Value. But at north end, need connection to new path which continues to Tanque Verde. Enhance C by partnering with commercial. No landscape installed north of commercial. Evaluate after installation?			
Tanque Verde to Confluence (east/north bank)	P10	0.20	-		-		N	0			E					Follow Landscape Standards.			
Tanque Verde to Craycroft (south/west bank)	P10	0.21	-		-		N	0			E					Follow Landscape Standards.			
Pantano Total		17.22	128.00		2.59	22.00													
Julian Wash/Tucson Diversion Channel																			
Rita Road to Kolb Road - (north bank)	J1	5.95	12	2.98	8		N	4			D	New construction. CHECK PLANS		8 including basin overlook, and trailhead.		Focus: retain desert quality. Enhance canopy at all nodes.	Reclaimed at Civano		
Kolb to Wilmot (south bank)	J2	1.35	12	-	-		N	4.3	N	N	E			0		Follow Landscape Standards. Enhance native plant system context. At minimum provide new dense canopied nodes with bench each 1/4 mile.	Reclaimed at Irvington		
Wilmot Road to Valencia @ Thomas Jay Park (south bank)	J3	2.09	10, 12	1.09	8		N	4	Y		C where path exists. Remainder = E.			0		Provide heavy canopied nodes every 1/4 mile. Enhance existing mini-node near Avenida Arrabal. Interface with Thomas Jay Park and Sunnyside school. Follow Landscape Standards for new construction.	Reclaimed at Palo Verde & Ajo		
Valencia to Interstate 10/ Drexel	J4	-	12		8		N	4	N	N	E			0		Follow Landscape Standards. To highest degree possible, integrate and encourage existing native trees.	Reclaimed at Palo Verde & Ajo		
Interstate 10/Drexel to Palo Verde Road (south bank)	J5	-	-	-	-		N	2.6			E			0		Follow Landscape Standards. Interface with County park and Sunnyside school at Alvernon. To highest degree possible, integrate and encourage existing native trees.	Reclaimed at Drexel/Campbell or Palo Verde/Ajo		
Palo Verde Road to Country Club	J6	-	-	-	-		N	0			E			0		Follow Landscape Standards. To highest degree possible, integrate and encourage existing native trees.			
Country Club to KERF Outlet	J7	-	12	-	-		Y-Reclaimed				E					Follow Landscape Standards. To highest degree possible, integrate and encourage existing native trees.			

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augmentation)	Reclaimed Water Comments	Underpass Width	Bridge Width
KERP Outlet to Campbell (west bank)	J8	0.61	10	-	-		N	0			E. No veg					Follow Landscape Standards.			
Campbell to Park/Ajo (north bank)	J9	0.80	10, 12	-	-		N	0			E.					Follow Landscape Standards. To highest degree possible, integrate off-site (private) canopy. Partner with private property to increase canopy where solitary trees exist and possibly carve out islands in adjacent parking to support canopy and/or nodes.			
Park/Ajo to UPRR	J10	0.40	10	-	-		N	0.4			E					Follow Landscape Standards. Partner with Golden Eagle for employee benefit/ potential node at UPRR.	Reclaimed at Lundy & Ajo		
UPRR to 44th Street (along east and north side of VA Hospital)	J11	0.62	10	-	-		N	0			E. No veg					Follow Landscape Standards.			
44th Street to 10th Avenue (north bank)	J11	1.01	10, 12	-	-		N	0			B. C at north and south. D along 44th St.			0		Enhance density of C canopy. Partner with commercial at 44th Street to enhance their parking lot islands and landscape border.			
44th Street to 10th Avenue (south bank)	J11	0.31	10, 12	-	-		N	0			B. C at north			2		Enhance canopy at nodes and density of C canopy.			
10th Avenue to 12th Avenue (north bank)	J11	0.18	12	-	-		N	0			B. Some A at 10th Ave.	CHECK PLANS		2		Recently improved- nice density. Enhance selectively.			
10th Avenue to 12th Avenue (south bank)	J11	0.15	12	-	-		N	0			B	CHECK PLANS		1		Recently improved- nice density. Enhance selectively.			
12th Avenue to end Heritage Park Loop (north bank)	J12	0.13	12	-	-		N	0.2			C.	CHECK PLANS		0		Recently improved. Enhance C so both sides of trail have canopy.			
12th Avenue to end Heritage Park Loop (south bank)	J12	0.15	12	-	-		N	0.2			At 12th node=A/B. Remaining = B.	CHECK PLANS		2		Recently improved- nice density. Enhance selectively.			
End Heritage Park Loop to Santa Cruz confluence (north bank)	J12	-	-	-	-		N	?			E			0		Follow Landscape Standards. Anticipate C to I-19 then B/ A to Santa Cruz due to large ROW and restoration potential.			
End Heritage Park Loop to Santa Cruz confluence (south bank)	J12	-	-	-	-		N	?			E			0		Follow Landscape Standards.	Tucson Water: Reclaimed mains runs east of I-19. Connections are more than 1/2 mile away from the park, and would have to cross I-19 to get to the park. If water was needed in the park on the west side of the Santa Cruz, river crossings (404 permits) would also likely to be required. Best location for connection to the reclaimed system is at the Mission Manor Booster Station south of Drexel Road.		
Julian Wash/Tucson Diversion Channel Total		13.75	126.00	4.07	24.00														
Cañada del Oro Wash																			
Tangerine Road to Oracle Road (west bank)	C1	-	-	-	-		N	Check with TOV			E								
Tangerine Road to Oracle Road (east bank)	C1	1.54	12	-	-	On east bank, technically becomes Big Wash at Oracle. This multi-use trail was installed by the Market Place Mall, and its maintenance is overseen by their management company.	Y-Potable	Check with TOV			C. At north & south= D. Small area anticipated to become B toward south. Areas of vegetation on north side pathway will provide little canopy/ shade. Canopy consistent but young - one side of path only.	Very recent; young vegetation				Create entry node at Tangerine. No additional space for dg path and few opportunities for more canopy depth. If possible, create high canopy nodes at wide locations ideally at every 1/4 mile. Improve node at south end Innovation Market Dr. Upgrade D canopy to C although path segment at top of berm- no space. Possible trailhead at Oracle.			
Oracle Road to First Avenue (north/ west bank)	C2	-	-	-	-		N	Check with TOV			E					Follow Landscape Standards			
Oracle Road to First Avenue (south/ east bank)	C2	0.81	0,12	-	-	No path 1st Ave to Steam Pump Ranch (funded and under design)	N	Check with TOV			North 1/2 C w/ small D northmost. South 1/2= E.					Upgrade D canopy to C and improve some C to B to create nodes at 1/4 mile min. Provide node at N. 1st Ave.			
First Avenue to Pusch View Lane (north bank)	C3	-	-	-	-		N	Check with TOV			E					Follow Landscape Standards			

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-built Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width	
First Avenue to Pusch View Lane (south bank)	C3	0.67	12	-	-		N	Check with TOV								Increase density of C canopy. Provide large node/pull path from bank to allow canopy on both sides at widened ROW mid-way (by Target).				
Pusch View Lane to La Canada (north bank)	C4	-	-	-	-		N	Check with TOV								C. Canopy on one side only.				
Pusch View Lane to La Canada (south bank)	C4	2.49	10,12	-	-		N	Check with TOV								Follow Landscape Standards				
La Canada to La Cholla (north bank)	C5	-	-	-	-		N	Check with Metro Water	N	N						Possible trailhead/ node at Pusch View. Entire ROW appears narrow because fencing/ post & cable adjacent to path, but MapGuide measures 50' average with some to 100'. Adjacent golf course land is OV- partner to increase canopy density at 1/4 mile min.				
La Canada to La Cholla (south bank)	C5	-	-	-	-		N	Check with Metro Water	N	N						Follow Landscape Standards. Work with existing native vegetation.				
La Cholla to Magee (south bank)	C6	1.50	10, 12	-	-	Paved stops at Hardy, does not reach La Cholla	Y-Potable	0.1	Y	Y	La Cholla to Hardy = E. Hardy to Magee: Central 1/2= C. Rest= D.					Follow Landscape Standards	Reclaimed Water line connection made to COT 24" Thornydale main and run down Magee Road to CDO River Park and NW YMCA Park at Mona Lisa Road under Metro Water authority. Line not fully connected to either park due to capacity issues. Reclaimed water not available in spring and summer months. Please note that the reclaimed flow to these sites can not be guaranteed, due to the over commitment of the 24-inch reclaimed main. For firm supply of water a pipeline over 4 miles long, and booster pumping system would be required for the CDO RP, NW YMCA, ARTHUR Pack Park, Mountain View H.S., Pima College NW Campus, and potentially other neighborhood parks, and school grounds.			
Magee to Ina (north bank)	C7	-	10,12		8		N	Check with Metro Water	N	N						Follow Landscape Standards				
Magee to Ina (south bank)	C7	1.64	-	0.34	-	D.G. goes only 1,000 feet north of Ina	Y-Potable	0.1	Y	Y	The general projection from plans: 1/2 C. 1/2 D. Small B at north and south.	Pima County W.O. 5CDOPK reviewed. Indicates canopy on both sides asphalt pathway to north.				Enhance C canopy at Magee. Along pathway increase density of C or upgrade to B. Possibly partner with homeowners adjacent to D (and C) to increase canopy on their property and provide "borrowed" shade for path.	Reclaimed Water line connection made to COT 24" Thornydale main and run down Magee Road to CDO River Park and NW YMCA Park at Mona Lisa Road under Metro Water authority. Line not fully connected to either park due to capacity issues. Reclaimed water not available in spring and summer months. Please note that the reclaimed flow to these sites can not be guaranteed, due to the over commitment of the 24-inch reclaimed main. For firm supply of water a pipeline over 4 miles long, and booster pumping system would be required for the CDO RP, NW YMCA, ARTHUR Pack Park, Mountain View H.S., Pima College NW Campus, and potentially other neighborhood parks, and school grounds.			
Ina to Thornydale (north/ west bank)	C8	-	-	-	-		N	1.4	N	N						Follow Landscape Standards				

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map_No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width
Ina to Thornydale (south/ east bank)	C8	0.86	12	0.34	8		Y-Potable	0.1	Y	Y							Reclaimed Water line connection made to COT 24" Thornydale main and run down Magee Road to CDO River Park and NW YMCA Park at Mona Lisa Road under Metro Water authority. Line not fully connected to either park due to capacity issues. Reclaimed water not available in spring and summer months. Please note that the reclaimed flow to these sites can not be guaranteed, due to the over commitment of the 24-inch reclaimed main. For firm supply of water a pipeline over 4 miles long, and booster pumping system would be required for the CDO RP, NW YMCA, ARTHUR Pack Park, Mountain View H.S., Pima College NW Campus, and potentially other neighborhood parks, and school grounds.		
Thornydale to I-10 (north bank)	C8	-	-	-	-		N	1.4	N	N	E	Pima County W.O. 5CDOPK reviewed.				Follow Landscape Standards			
Thornydale to I-10 (south bank)	C8	-	-	-	-		N	1.4	N	N	E					Follow Landscape Standards			
Cañada del Oro Total		9.51	36.00	0.68	16.00			4.50											
Tanque Verde Wash																			
Pantano Road to Sabino Canyon Road - (north bank)	T1						N				E		Private ownership			Riparian restoration if ROW acquired.			
Sabino Canyon Road to Craycroft Rd- (north bank)	T2						N				E		Primarily private			Riparian restoration if ROW acquired.			
Tanque Verde Wash Total																			
Existing Underpasses and Bridges																			
Santa Cruz River (west bank)																			
Irvington Road U/P																		10	
Silverlake Road U/P																		9.75	
Old West Branch Pedestrian Bridge																		13	
Starr Pass Boulevard U/P																		8	
Congress Boulevard U/P																		7.167	
St. Mary's Road U/P																		7.5	
Speedway Boulevard U/P																		8.5	
Short Ped Bridge btw. Grant and Speedway (approx. 3000' upstream from Grant)																		11.5	
Grant Road U/P																		8.5	
Speedway Wash/Silvercroft Wash Pedestrian Bridge																		11.25	
Camino De Oeste Wash Pedestrian Bridge																		11.5	
Trails End Wash Pedestrian Bridge																		11.5	
Roger Wash Pedestrian Bridge																		11.5	
Sweetwater Wash Pedestrian Bridge																		11.5	
Cortaro Road U/P																		12	
First channel north of Cortaro Road																		12	
Second channel north of Cortaro Road																		12	
Santa Cruz River (east bank)																			
Irvington Road U/P																		5.25	
Irvington Wash Pedestrian Bridge (east bank about ½ mile downstream of Irvington)																		12	
Wyoming Wash Pedestrian Bridge (east bank of SCR downstream of Irvington)																		12	
Silverlake Road U/P																		11.5	
Starr Pass Boulevard U/P																		10	
Old Julian Wash Pedestrian Bridge [300' downstream of Simpson Street (wood plank structure)]																		11.5	
Cushing Street Wash - Pedestrian Bridge north of new Cushing Street Bridge																		11	
Arroyo Chico Pedestrian Bridge																		8	
St. Mary's Road U/P																		6	
Speedway Boulevard U/P (overhead pipe)																		9.75	
Bronx Wash Pedestrian Bridge																		12	
Unnamed Wash Pedestrian Bridge (2475' downstream from Bronx Wash)																		12	
Grant Road U/P																		8	
Santa Cruz Total																			
Rillito River (north bank)																			
Craycroft Wash Pedestrian Bridge																		11.5	
Swan Road U/P																		9.167	
Valley View Wash Bridge																		11.5	
Binghamton Wash Bridge (approximately 600' east of Alvernon Way)																		11.5	
Alvernon Way U/P																		9	
Hacienda Del Sol Wash Bridge																		11.5	
Country Club Road Pedestrian Bridge (spanning the Rillito River)																		11.5	
Camino Real Wash Bridge																		11.5	
Unknown Wash Bridge (east side of St. Phillips Plaza, approximately 500' east of Campbell Ave.)																		11.5	
Campbell Avenue U/P																		10.833	
Campbell Wash Bridge																		8	
Mountain Avenue Pedestrian Bridge (spanning the Rillito River)																		8.667	

Urban Loop Project Status

Reach/Project (Upstream to Downstream)	Map No	Total Miles	Asphalt Width	DG Path	DG Path Width	Asphalt/DG Path Notes	Irrigation	Reclaimed Availability	Irrigation Operational	Irrigation As-builts Available	Landscaping: Existing Vegetation Designation (A)(B)(C)(D)E	In Planning or Recent Construction	Average ROW**	# Hardscape Nodes	Additional Canopy Needed	Proposed Priority Landscaping (follow Landscape Standards to augment)	Reclaimed Water Comments	Underpass Width	Bridge Width
Via Entrada Wash Bridge																			8
1 st Avenue U/P																		7.667	7.833
Race Track Wash Bridge																			
Stone Avenue U/P																		11.5	8
Friendly Village Wash Bridge																		8.333	8
Oracle Road U/P																		8.583	8
Pima Wash Bridge																			
Flowing Wells Rd./La Canada Dr. U/P																		8.583	8
Roller Coaster Wash Bridge																			11.5
Casas Adobes Wash Bridge																		11.833	11.5
La Cholla Boulevard U/P																			11.5
Nanini Wash Bridge																			11.5
Pegler Wash Bridge																			11.5
Rillito River (south bank)																			
Arroyo Del Fuerte Wash Bridge (west side of St. Gregory's)																			11.5
Alamo Wash Bridge																			13.667
Swan Road U/P																		9.167	11.5
Lazy Creek/Creekside Wash Bridge																			11.5
Alvernon Wash Bridge																			11.5
Alvernon Way U/P																		8.083	11.5
Christmas Wash Bridge																			11.5
Campbell Avenue U/P																		11.417	8
Mountain Avenue Wash Bridge Unpaved																			12
Mountain Avenue Wash Bridge Paved																			12
1 st Avenue U/P																		7.5	7.667
Oracle Road U/P																		7.667	7.583
Flowing Wells Rd./La Canada Dr. U/P																		7.583	12.5
La Cholla Boulevard U/P																			12.5
Hodges Wash Bridge (approximately 1500' d/s from La Cholla)																			11.5
Flatcar Wash Pedestrian Bridge																			10.5
UPRR and I-10 U/P																		11.5	
Rillito Total																			
Julian Wash/Tucson Diversion Channel (one bank only)																			
Kolb Detention Basin inlet Pedestrian Bridge																			12
KERP inlet Pedestrian Bridge																			12
KERP outlet Pedestrian Bridge																			7.5
Court Complex Pedestrian Bridge (near KERP outlet)																		9.833	
Ajo Way U/P in channel (narrowest section)																		9.5	
I-10 U/P (north bank)																		9.5	
Kino Parkway U/P (north bank)																		10	
Benson HWY U/P (north bank)																			10
Pedestrian Bridge near 6 th Avenue and 40 th Street (on the VA Hospital campus)																			12
Pedestrian Bridge near 9 th Avenue and 44 th Street																			11.5
Pedestrian Bridge on the 11 th Avenue alignment at 39 th Street																			12
Pedestrian Bridge between 11th and 12th Avenue over street drainage channel on south bank																			
Pedestrian Bridge on the Sunset Villa Avenue alignment at 40 th Street (east of I-19)																			
Julian /Tucson Diversion Channel Total																			
Pantano Wash (west bank)																			
Tanque Verde Road U/P																		11.583	
Golf Links Road U/P																		6	
Pantano Wash (east bank)																			
Tanque Verde Road U/P																		10.333	
Speedway Boulevard U/P																		13.333	
22nd Street U/P																		15.583	
Rolling Hills Wash culvert bridge																			12
Golf Links Road U/P																		14.75	
Pantano Total																			
Cañada del Oro Wash (one bank only)																			
Oracle Road Pedestrian Bridge																			12
First Avenue U/P																		11.5	
Pusch View Lane U/P																		10	
Rooney Wash Pedestrian Bridge																			12
Oro Valley Country Club Pedestrian Bridge																			12
Logan's Crossing Pedestrian Bridge																			12
Hardy Road drainageway culvert crossing																			19
Magee Road U/P																		11.5	
Ethan Allen Place Pedestrian Bridge																			11.5
Camino De La Tierra culvert crossing																			12
Ina Road U/P																		12	
Cañada del Oro Total																			

